

GRAIN DEALERS JOURNAL

“—most profitable machine in our elevator”

THAT'S what the J. C. Mytinger Grain Company, Wichita Falls, Texas, say about the Carter Disc Separator. Here's their letter—read it and see why it would pay **you** to have this equipment in **your** elevator.

CARTER DISC SEPARATOR

"We have been very well pleased indeed with the results we have obtained from your Carter Disc Separator. We have found it's capacity to be from 250 to 300 bu. per hour. We are operating it with a 3 H. P. motor and find its power consumption to be very small indeed. On account of the sturdy character of construction of this machine it would seem to us it could reasonably be expected to remain in good operating condition for many years to come.

"We have our elevator so arranged that we have a large bin over the Carter machine with two bins below. By reason of this arrangement we can put a car of oaty wheat in the bin above the machine; start the machine and let it run with practically no attention until the bin above is empty. The two bins below receive the wheat and oats separately as the machine has separated them. The only attention the machine requires is the filling of the overhead bin and the emptying of the bins below with an occasional visit to the machine to prevent choking from sticks and any other large foreign material that might be in the wheat.

"We have been running this machine seven days and nights per week although we have only operated our elevator on a daylight basis. The night watchman has been able to look after the machine at night and the day watchman on Sundays so that we have had 100% efficiency on the part of the machine.

"We really feel that this machine is the most profitable one we have in our elevator. The separation made by this machine is really wonderful as it takes every grain of oats out of wheat and a great deal of our wheat has come in here this season with oats dockage ranging from 1% to 12%. Yours very truly,"

J. C. MYTINGER GRAIN COMPANY,
BY J. C. MYTINGER.

The Armour Grain of Chicago have 8 Carter Disc Separators; Bartlett Frazier Co. (Soo Line Elevator) of Minneapolis, 4 machines; Pennsylvania R. R., Baltimore, 2 machines; Midland Linseed Products, Minneapolis, Minn., and Edgewater, N. J., 4 machines; Industrial Commission of North Dakota, 12 machines; Port Commission Elevator, Norfolk, Va., 2 machines. These and many other elevators have found the Carter Disc Separator to be the most efficient machine of its kind—and a most profitable investment.

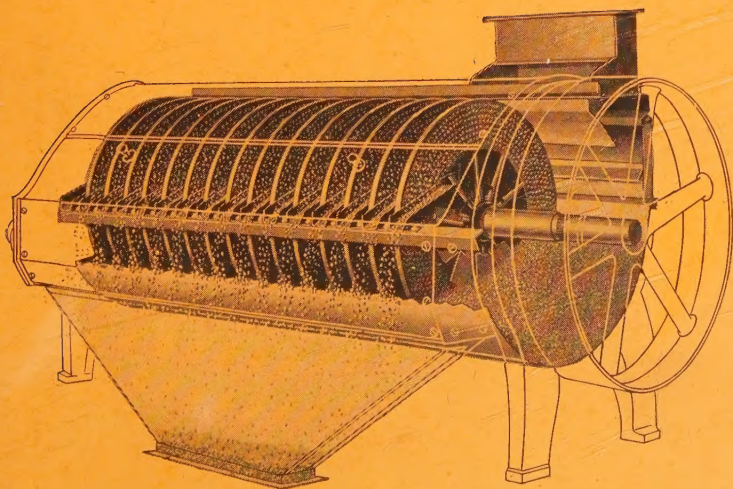
Write for our 30-day free trial proposition.

Carter-Mayhew Mfg. Co.

611 Fifth Ave. So.

Minneapolis, Minn.

In Canada, Manufactured and sold by
THE STRONG-SCOTT MFG. CO., LTD., Winnipeg



WEBSTER MARINE LEGS

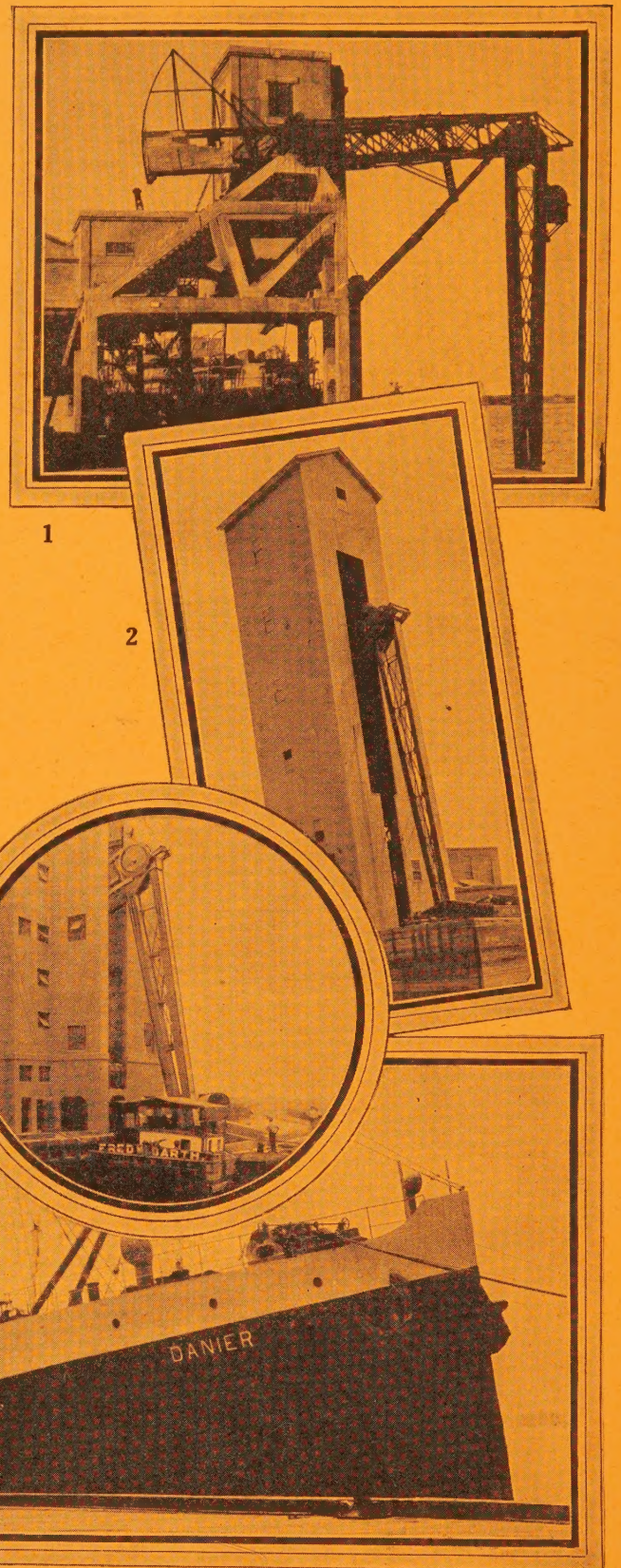
are the most successful form of installation for efficiently handling the grain from the ship hold to the elevator with the greatest facility. They are sturdily built and mechanically correct in design, thus insuring years of reliable and dependable service.

The extensive operation of Webster Marine Legs in some of the largest grain elevators in the United States and Canada proves their adaptability.

Webster Grain Handling equipment not only includes marine legs, but a complete line of equipment: Belt Conveyors; Trippers; Elevators; Buckets; Power Transmission Machinery; Loading Hoppers; Power Shovels; Spouting, etc.

Let Webster Engineers give you the benefit of their experience.

- (1) Sociedade Anonyma Grandes Moinhos, Pernambuco, Brazil
- (2) Corn Products Refining Co., Edgewater, N. J.
- (3) New York Barge Canal Elevator, Gowanus Bay, Brooklyn, N. Y.
- (4) Board of Commissioners, Port of New Orleans, New Orleans, La.



THE WEBSTER MFG. COMPANY

4500-4560 CORTLAND ST., CHICAGO

Factories-Tiffin, O. and Chicago - Sales Offices in Principal Cities

Directory of the Grain Trade

In Organized Markets Only Members of the Local Grain Exchange Will Be Listed

HAVING YOUR name in this directory will introduce you to many old and new firms during the year, whom you do not know or could not meet in any other way. Many new concerns are looking for connections, seeking an outlet or an inlet, possibly in your territory. It is certain that they turn to this recognized Directory, and act upon the suggestions it gives them.

AMARILLO, TEXAS.

Early Grain & Elevtr. Co., wholesale grain.*
Stone & Co., Lester, wholesale grain.*

ATCHISON, KANS.

Moore-Lawless Grain Co., consgts., futures, pvt. wires.*

ATLANTA, GA.

Commercial Exchange Members.

Hayza & Sims, wholesale brokers, grain, hay.*

BALTIMORE, MD.

Chamber of Commerce Members.

Baltimore Grain Co., grain receivers, exporters.*
Dennis & Co., grain merchants.*
England & Co., Inc., Chas., grain, hay.*
Fahey & Co., John T., grain receivers and exporters.*
Gill & Fisher, receivers and shippers.*
Hammond, Snyder & Co., Inc., receivers, exporters.*
Hax & Co., G. A., grain, hay, seeds.*
Jones & Co., H. C., receivers, shippers, exporters.*
Lederer Bros., grain receivers.*
Robinson & Jackson, grain commission merchants.*

BOSTON, MASS.

Chamber of Commerce Members.

Bezaquin, Matthew D., grain brokerage commission.*

BUFFALO, N. Y.

Corn Exchange Members.

Anchor Grain Co., Inc., grain and feed.
Armour Grain Co., grain merchants.*
Burns Bros. Grain Corp., grain commission.*
Davis, Inc., A. C., grain.*
Harold, A. W., grain, barley a specialty.
McConnell Grain Corporation, commission merchants.*
McKillen, Inc., J. G., receivers and shippers.*
Pratt & Co., receivers, shippers of grain.*
Ratcliffe, S. M., commission merchant.*
Seymour-Wood Grain Co., consignments.*
Sunset Grain & Feed Co., grain & feed.*
Townsend Ward Co., The, consignments.*

CAIRO, ILL.

Board of Trade Members.

Cairo Grain Com. Co., consignments.*
Holiday Elevator Co., grain dealers.*
Hastings Co., Samuel, receivers & shippers.*
Hastings-Stout Co., grain merchants.*
Lynch Grain Co., grain dealers.*
Thistlewood & Co., grain and hay.*

CEDAR RAPIDS, IOWA.

Gifford Grain Co., grain and grain products.*
King Wilder Grain Co., grain shippers.*
Murrel Grain Co., Ray, recvrs. and shprs.*

CHATTANOOGA, TENN.

Chattanooga Feed Co., mfrs. feed; shprs. gr., hay, feed.

CHICAGO, ILL.

Board of Trade Members.

Anderson & Co., W. P., grain commission merchants.*
Armour Grain Co., grain buyers.*
Bailey & Co., E. W., grain commission merchants.*
Bartlett-Frazier Co., grain merchants.*
Brennan & Co., John E., grain commission merchants.*
Carhart Code Harwood Co., grain commission.*
Dole & Co., J. H., grain and seeds.*
Frecman & Co., Henry H., grain, hay, straw.*
Gerstenberg & Co., commission merchants.*
Harris, Winthrop & Co., grain commission.*
Hitch & Carder, commission merchants.*
Holt & Co., Lowell, commission, grain and seeds.*
Lamson Bros. & Co., consignments solicited.*
McKenna & Dickey, commission merchants.*
Mumford & Co., W. R., grain, hay, millstuffs.*
Norris Grain Co., grain merchants.*
Pope & Eckhardt Co., grain and seeds.*
Rang & Co., Henry, grain merchants.*
Requa Bros., wheat a specialty.*
Rosenbaum Brothers, grain merchants.*
Rosenbaum Grain Co., J., commission merchants.*
Rumsey & Co., grain commission.*
Sawyers Grain Co., consignments.*
Schiffin & Co., P. H., commission.*
Shaffer Grain Co., J. C., grain merchants.*
Somers, Jones & Co., grain and field seeds.*
Uplike Grain Co., consignments.*

CINCINNATI, O.

Grain & Hay Exchange Members.

Early & Daniel Co., grain, hay, feed.*

CLEVELAND, O.

Grain & Hay Exchange Members.

Bailey, E. I., grain and millfeed.*
Cleveland Grain & Milling Co., The, recvrs. & shprs.*
Gates Elevator Co., The, receivers and shippers.*
Sheets Elevator Co., The, grain, hay, straw.*
Shepard, Clark & Co., grain merchants.*
Strauss & Co., H. M., receivers, shippers hay & grain.*

COLUMBUS, OHIO.

Smith-Sayles Grain Co., The, buyers and shippers.*

DAVENPORT, IOWA.

Davenport Elevator Co., receivers and shippers.*

DECATUR, ALA.

Lyle-Taylor Grain Co., whlse. grain, hay, feeds.

DECATUR, ILL.

Baldwin & Co., H. I., grain dealers.*

DENVER, COLO.

Grain Exchange Members.

Ady & Crowe Merc. Co., The, grain and hay.*
Conley-Ross Grain Co., The, grain and beans.*
Crescent Flour Mills Co., The, merchant millers.*
Crowell Elevator Co., wholesale grain.*
Denver Elevator, wholesale grain, flour, millfeed.*
Gallagher Grain Co., grain merchants.*
Houlton Grain Co., wholesale grain.*
Kellogg Grain Co., O. M., receivers and shippers.*
McCaull-Dunsmore Co., wholesaler and commission.*
Moore-Lawless Grain Co., private wires to all markets.*
Phelps Grain Co., T. D., wholesale grain.*
Rocky Mountain Grain Co., export and domestic grain.*
Summit Grain Co., wheat, corn, oats, rye, barley.*

DES MOINES, IOWA.

Board of Trade Members.

Des Moines Elevtr. & Gr. Co., oats a specialty.*
Iowa Corn Products Co., receivers and shippers.*
Lockwood, Lee, broker.*
Marshall Hall Grain Co., grain commission.*
Rang & Co., Henry, grain commission.*
Taylor & Patton Co., corn and oats.*

DETROIT, MICH.

Board of Trade Members.

Dumont, Roberts & Co., receivers, shippers.*
Huston, C. E., grain and hay.*
Lientenberg & Son, oats, corn, hay, straw.*
Simmons & Co., F. J., grain and hay.*
Swift Grain Co., consign or ask for bid.*

FORT WORTH, TEX.

Grain and Cotton Exchange Members.

Dorsey Grain Co., merchants—commission, consignments.*
Transit Grain & Com. Co., consignments, brokerage.*

GREAT FALLS, MONTANA.

Barkemeyer Grain & Seed Co., grain dealers.*

GREENVILLE, O.

Grubbs Grain Co., E. A., track buyers.*

HAMBURG, IOWA.

Sullivan & McBride S. & G. Co., red cob fodder corn.*

HASTINGS, NEBR.

Koehler-Twisdale Elevator Co., grain dealers.*

HATTIESBURG, MISS.

Merchants Grocery Co., whlse. grocers, grain, fd., fr.*

HOUSTON, TEX.

Gulf Grain Co., grain, hay, millfeed.*
Rothschild Co., S., grain, c/s products, rice b/p.*

HUTCHINSON, KANS.

Board of Trade Members.

Central Grain & Laboratories Co., The, buyers for mills.*

INDIANAPOLIS, IND.

Board of Trade Members.

Anderson & Mercer, grain commission & consignments.*
Boyd Grain Co., Bert A., strictly brokerage & com.*
Cleveland Grain & Milling Co., grain commission.*
Hill Grain Co., The Lew, grain merchants.*
Steinhart Grain Co., commission and brokerage.*
Witt, Frank A., grain commission and brokerage.*

JACKSON, MICH.

Bartlett & Co., J. E., salvage grain buyers & sellers

JACKSON, MISS.

Royal Feed & Mfg. Co., mixed feed mfrs.

KANSAS CITY, MO.

Board of Trade Members.

Christopher & Co., B. C., kafir, feterita, milo.*
Croydsdale Grain Co., commission merchants.*
Davis Grain Co., A. C., grain commission.*
Denton Kuhn Grain Co., consignments.*
Ernst Davis Grain Co., commissions.*
Federal Grain Co., receivers, shippers.*
Frisco Elevators Co., grain merchants.*
Goffe & Carkener, grain commission.*
Hall-Baker Grain Co., consignments.*
Lichtig & Co., H., kafir, milo, screenings.*
Logan Bros. Grain Co., receivers and shippers.*
Miller Grain Co., S. H., consignments.*
Moore-Lawless Grain Co., grain receivers.*
Moore-Seaver Grain Co., grain receivers & exporters.*
Norris Grain Co., grain merchants and exporters.*
Roeben Grain Co., E. E., consignments.*
Ry. Mt. Grain & Com. Co., consignments.*
Root Grain Co., consignments and futures.*
Scoular Bishop Grain Co., receivers and shippers.*
Shannon Grain Co., consignments.*
Simonds, Shields, Lonsdale Grain Co., grain.*
Smith-Weekes Bkg. Co., grain broker.*
Terminal Elevators, grain merchants.*
Thresher Grain Co., R. J., grain commission.*
Vanderslice-Lynds Co., grain merchants.*
Watkins Grain Co., consignments.*
Wilser Grain Co., consignments.*

LAWTON, OKLA.

Western Grain Co., hay, grain, seeds, mill pds.

LIBERAL, KANS.

Vickers Grain & Seed Co., grain and field seeds.

LINCOLN, NEBR.

Grain Exchange Members.

Lincoln Grain Co., grain merchants.*

LITTLE ROCK, ARK.

Grain Exchange Members.

Caple & Stockton, hay, grain, feed.*
Farmer Co., E. L., brokers, grain & mill feed.*
Gordy Co., C. L., grain brok., hay, grain, millfeed.*

LOUISVILLE, KY.

Board of Trade Members.

Bingham-Hewett Grain Co., receivers-shippers of grain.*
Callahan & Sons, receivers and shippers of grain.*
Kentucky Public Elevator Co., storers and shippers.*
Verhoeff & Co., H., receivers and shippers.*
Zorn & Co., S., receivers and shippers.*

LYNCHBURG, VA.

Moon-Taylor Co., grain and hay brokers.

M'KINNEY, TEXAS.

Reinhart & Company, wheat, corn, oats, maize.*

MEMPHIS, TENN.

Merchants Exchange Members.

Browne, Walter M., broker and com., consignments.*
Buxton, E. E., broker & commission merchant.*
Davis & Andrews Co., grain, mixed feed.*
U. S. Feed Co., grain, hay, mill feed.*

MIDDLETOWN, CONN.

Meech & Stoddard, Inc., grain, feed, hay, flour.*

(Continued on next page.)

*Member Grain Dealers National Association.

Directory of the Grain Trade

In Organized Markets Only Members of the Local Grain Exchange Will Be Listed

MILWAUKEE, WIS.

Chamber of Commerce Members.

Bacon Co., E. P., grain and seed.*
Bell Co., W. M., commission merchants.*
Bush Grain Co., grain merchants.
Cargill Grain Co., grain and seeds.
Donahue-Stratton Co., dlsr. grain and feed.*
Flanley Grain Co., consignments solicited.
Franke Grain Co., feeds, grain, hay.
Fraser-Smith Co., commission merchants.
Froedtert Gr. & Maltng Co., grain commission.
Kamm Co., P. C., grain shippers.*
La Bude Feed & Grain Co., grain and feed.
Lamson Bros. & Co., grain commission.
Milwaukee Grain Commission Co., grain commission.
Rang & Co., Henry, commission merchants.
Runkel & Dadmun, grain commission merchants.*
Taylor & Bourneque Co., grain merchants.*
Udpike Grain Co., consignments solicited.*

MERIDIAN, MISS.

Board of Trade Members.

Lyon & Co., A. J. whsle. gro., grain, feed.
Threefoot Bros. & Co., whsle. grain, feed, flr., gro.*

MINNEAPOLIS, MINN.

Chamber of Commerce Members.

Cargill Commission Co., grain commission.*
Cereal Grading Co., grain merchants.*
Hankinson & Co., H. L., grain commission.*
Malmquist & Co., C. A., receivers & shippers.*
Marfield Grain Co., grain commission.*
McCevill Dismore Co., consignments solicited.*
Seidl, Frank J., all grains and feeds.*
Sheffield Elevator Co., shippers of grain.*
Sterling Grain Co., receivers and shippers, all grains.*
Van Dusen-Harrington Co., grain merchants.*
Zimmerman, Otto A., grain and feed.

NEW CASTLE, PA.

Hamilton Co., grain, feed, flour, hay, potatoes.

NEW ORLEANS, LA.

Board of Trade Members.

Anderson & Jackson, Inc., exporters of grain.*
Fox Co., O. B., exporters.*
Nathan, Edw., suocr. to Nathan & Fettis, fwdg. agt.*

NEW YORK CITY.

Produce Exchange Members.

Jones & Co., M. B., buyers—quote us.*
Knight & Company, commission merchants.*
Muller & Co., Inc., W. H., recvrs., buyers, exporters.*
Therrien, A. F., broker.

NORFOLK, VA.

Moon-Taylor Co., grain and hay brokers.

OKLAHOMA CITY, OKLA.

Grain Exchange Members.

Cherokee Grain Co., grain merchants.*
Lang Grain Co., J. H., prompt and efficient service.*
Marshall Grain Co., grain, feed, seeds.*
Moore, George L., grain merchant.
Okla. City M. & E. Co., grain merchants, millers.*
Perkins Grain Co., W. L., brokers.*
Scannell-Winters Grain Co., grain and feed.*
Stinnett Grain Co., grain merchants.*
Stowers Grain Co., W. B., commission merchants.*
Vandenburgh, Jesse, milling wheat.*
Vandever Grain Co., wholesale grain.
White Grain Co.*

OMAHA, NEBR.

Grain Exchange Members.

Butler Welsh Grain Co., grain merchants.*
Crowell Elevator Co., receivers, shippers.*
Roberts Grain Co., Geo. A., grain merchants.*
Stockham Grain Co., E., commission merchants.*
Taylor Grain Co., commission and brokers.
Trans-Mississippi Grain Co., receivers and shippers.*
United Grain Co., grain commission.
Udpike Grain Co., consignments.*

OTTAWA, KANS.

Ross Milling Co., The, millers, hard wheat flour.

PEORIA, ILL.

Board of Trade Members.

Bartlett Co., S. C., grain commission.
Bowen Grain Co., H. D., receivers & shippers.
Cole Grain Co., Geo. W., receivers and shippers.*
Dewey & Sons, W. W., grain commission.*
Feltman Grain Co., C. H., grain commission.
Harrison, Ward & Co., receivers & shippers.*
Luke Grain Co., grain commission.*
Mies, P. B. & C. O., grain commission.*
Turner Hudnut Co., receivers and shippers.*
Tyng Grain Company, receivers and shippers.*

PHILADELPHIA, PA.

Commercial Exchange Members.

Delp Grain Co., E. E., grain and millfeeds.*
Dunwoody Co., Ezl., flour, grain, feed.*
Richardson Bros., grain, flour, millfeeds.*
Richardson, Geo. M., grain and feeds.*
Rogers & Co., E. L., hay, straw, grain, feed.*
Stites, A. Judson, grain and millfeed.*

PONTIAC, ILL.

Balbach, Paul A., grain buyer, all markets.

RICHMOND, VA.

Grain Exchange Members.

Beveridge & Co., S. T., grain, hay, feeds, seeds.

ROCHESTER, N. Y.

Dailey Bros., Inc., receivers and shippers.*

SALINA, KANS.

Board of Trade Members.

Bossemeyer Grain Co., The Paul, grain merchants.*
Freeman-Faith Grain Co., receivers and shippers.
Hayes Grain Co., Always in the market.
Rickel, E. L., grain receiver and shipper.
Service Grain Co., grain, feed, grain products.*
Weber Flour Mills Corp., millers, exporters, grain dlsr.*

ST. JOSEPH, MO.

Grain Exchange Members.

A. J. Elevator Co., The, wheat, corn, oats.*
Burton-Simmons Grain Co., grain commission.*
Gordon Grain Co., grain commission.*
Marshall Hall Grain Co., consignments solicited.*
Washer Grain Co., W. S., consignments.*
Yancey Grain Co., commission merchants.

SAN ANTONIO, TEXAS

King, Douglas W., wheat, corn-brok., Texas R. Oats.*

ST. LOUIS, MO.

Merchants Exchange Members.

Aunan Burg G. & M. Co., flour, grain, millfeed.*
Dreyer Commission Co., feedingstuffs, grain, seeds.*
Graham & Martin Grain Co., grain commission.*
Marshall Hall Grain Co., grain commissions.*
Langenberg Bros. Grain Co., grain commission.*
Morton & Co., grain commission.*
Nanson Commission Co., grain commission.*
Picker & Beardsley Com. Co., grain and grass seed.*
Turner Grain Co., grain commission.*

SIOUX CITY, IOWA.

Board of Trade Members.

Button Co., L. C., grain commission.*
McCaull Dismore Co., commission.*
Western Terminal Elevator Co., receivers and shippers.*

SIOUX FALLS, S. D.

Burke Grain Co., grain merchants.

SUPERIOR, NEBR.

Bossemeyer Bros. Terminal Elevator.*
Elliott & Myers, grain merchants.*
Superior Terminal Elvtr. Co., grain, corn products.

TOLEDO, OHIO.

Produce Exchange Members.

De Vore & Co., H. W., grain and seeds.*
King & Co., C. A., grain and seeds.*
Southworth & Co., grain and seeds.*
Wickenhisser & Co., John, grain receivers, shippers.*

TOPEKA, KANS.

Derby Grain Co., wheat, corn, oats, and millfeed.*
Golden Belt Grain & Elvtr. Co., The, recvrs. & shprs.*
Topeka Grain Co., wheat, corn, oats, mill & ctn. feed.*

WASHINGTON, D. C.

Wilkins-Rogers Mlg. Co., Inc., receivers and shippers.*

WICHITA, KANS.

Board of Trade Members.

Baker-Evans Grain Co., milling and export wheat.*
Hipple Grain Co., consignments.
Kansas Flour Mills Co., receivers and shippers.
Linton-Christy Grain Co., strictly brokerage & comm.
Raymond Grain Co., consignments.
Simonds-Shields-Lonsdale Gr. Co., exporters.
Strong Trading Co., wholesale grain and feed.*
Terminal Elevators, exporters.

WICHITA FALLS, TEXAS.

Mytinger Grain Co., J. C., grain, feed, seeds.*

WINCHESTER, IND.

Goodrich Bros. Hay & Grain Co., whlrs. gr. and seeds.*

*Member Grain Dealers National Association.

Chamber of Commerce
Members

BALTIMORE

Chamber of Commerce
Members

Receivers, Shippers, Exporters

H. C. JONES & CO.
BALTIMORE
Grain and Hay
Consignments or Direct Purchases

CONSIGNMENTS A SPECIALTY

JOHN T. FAHEY & CO.
Commission Merchants
Grain Receivers and Shippers
In the Market every day
Remember us on Rye BALTIMORE, MD.

GILL & FISHER
Grain Receivers and Shippers
BALTIMORE

Chas. England & Co., Inc.
GRAIN—HAY—SEEDS
Commission Merchants
308-310 CHAMBER OF COMMERCE
BALTIMORE

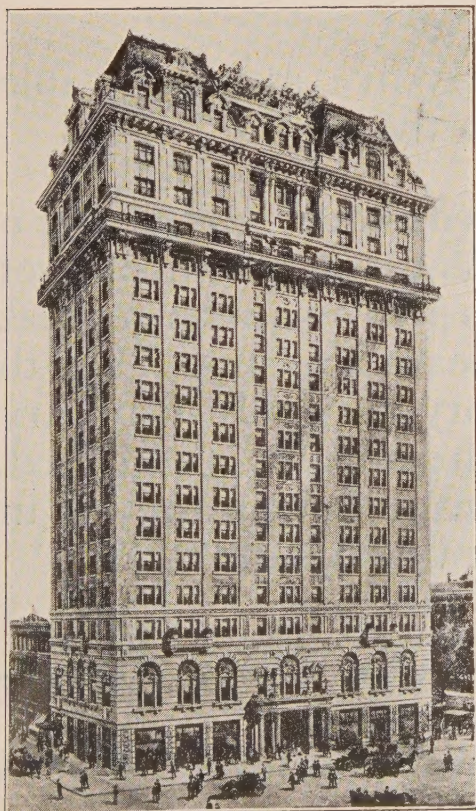
Baltimore Grain Co.
Consignments
Baltimore Maryland

Established 1900
G. A. HAX & CO.
COMMISSION
Grain and Hay
BALTIMORE MARYLAND

MEMPHIS

Memphis is now the undisputed claimant to the title of "Mixed Feed Center of the United States." The manufacture of mixed feeds in Memphis has grown until the annual output now is in excess of 250,000 tons. This alone should suggest to your mind that Memphis is a large buyer of grain and other ingredients going to manufacture feeds of all kinds.

Memphis also buys grain for reshipping and milling purposes. The demand is constant, and grain shippers from many states help to fill the bins of Memphis elevators. Have you a reliable connection in this market? If not, it is your own loss.



Memphis Merchants Exchange Members:

J. E. Tate & Co.
Wholesale Grain Dealers

Scruggs Robinson Co.
Broker, Com. Merchant, Consignments

Denyven & Co.
Broker & Commission

Marshall Grain Co.
Brokerage and Consignments

L. P. Cook
Receiver and Shipper

Lee D. Jones
Grain Dealer

Walter M. Browne
Broker and Commission, Consignments

Heckle Bros.
Grain—Flour—Feed

Royal Feed & Milling Co.
Mixed Feed Mfrs.

J. L. Nessly Co.
Broker, Com., Alfalfa Meal, Molasses

Edgar-Morgan Co.
Mixed Feed Mfrs., Grain

Gardner-Greer Grain Co.
Whlse. Grain and Hay

Superior Feed Co.
Mfrs. Mixed Feed, Grain Receivers

The Quaker Oats Co.
Feed Mfrs.

John Wade & Sons, Inc.
Grain, Feed, Flour

U. S. Feed Co.
Grain, Hay and Millfeed

Pease & Dwyer Co.
Grain, Mixed Feed

Cereal Byproducts Co.
"Everything for Mixed Feeds"

T. B. Jones & Co.
Wholesale Grain and Feed

Clark Burkle & Co.
Grain and Hay

Embrey E. Anderson
Grain, Hay, Ear Corn a specialty

B. H. Williamson & Co.
Grain, Feed, Alfalfa Meal

E. W. Wyatt
Broker Grain and Mill Feed

Sessum Grain Co.
Grain, Mixed Feed

Davis & Andrews Co.
Grain, Mixed Feed

Internat'l Sugar Feed No. 2 Co.
Mfrs. Sweet Mixed Feed.

NEW OR

Second Port

The Terminal Market

Robt. McMillan
 Public Grain Elevator
 J. T. Gibbons
 J. S. Waterman & Co.
 Milam-Morgan Co.
 James Thomas & Co.
 C. B. Fox Co., Inc.
 H. J. Landry
 E. Nathan
 Langenberg Grain Co.
 James M. Rogers
 Anderson & Jackson, Inc.
 C. S. Leach
 Chas. M. Rodd & Co.
 The W. L. Richeson Co., Inc.
 Strachan Shipping Co.
 Chas. R. Matthews & Bro.
 Fenner & Beane
 Steinhardt & Co.
 The Central Elevator &
 Warehouse Co.
 Mississippi Shipping Co., Inc.

Remember These Facts

Elevator capacity of New Orleans is 7,000,000 bus.

The Port is served by 12 railroads.

Total trackage within the city limits---200 miles.

Forty-five steamship lines run into the port.

In considering the large export business in grain conducted at this port do not forget that New Orleans also supplies all the territory naturally tributary to it with grain and mixed feed. This demand is constant and urgent, but well handled.

—By These Firms—

LEANS

-U. S. A.

et's Market

Exports Past Three Years

| | | |
|----------------|-------------|------|
| Wheat | 118,171,446 | bus. |
| Corn | 16,062,397 | " |
| Oats | 7,293,456 | " |
| Barley | 14,046,489 | " |

From These Terminals

| | |
|--------------|---------------|
| Chicago | Wichita |
| Minneapolis | Hutchinson |
| St. Louis | Oklahoma City |
| Kansas City | Des Moines |
| Omaha | San Francisco |
| Milwaukee | Cairo |
| Cedar Rapids | Portland |
| Peoria | Duluth |

Thru The Second Port

New Orleans

The Terminal Market's
Market

To These Countries

| | |
|------------------------------|----------|
| England | Belgium |
| France | Holland |
| Italy | Ireland |
| Spain | Scotland |
| and South American Countries | |

Board of Trade
Members**CHICAGO**Board of Trade
Members

Harris, Winthrop & Co.

11 Wall Street, New York
The Rookery, Chicago

Grain Commission

MEMBERSNew York Stock Exchange
New York Cotton Exchange
New York Produce ExchangeNew York Coffee Exchange
Chicago Board of Trade
Chicago Stock ExchangeKansas City Board of Trade
Minneapolis Chamber of Commerce
Winnipeg Grain ExchangeFor Best Results
CONSIGN**ReQua Brothers**Board of Trade, CHICAGO
W. M. TIMBERLAKE, Mgr. Cash GrainCONSIGNMENTS
SOLICITEDWE PLACE
GRAIN TO ARRIVE

Chicago Grain & Salvage Co.

DEALERS IN

SALVAGE GRAIN

GRAIN, FEEDS, Etc.

WRITE OR WIRE

930 Postal Telegraph Bldg.

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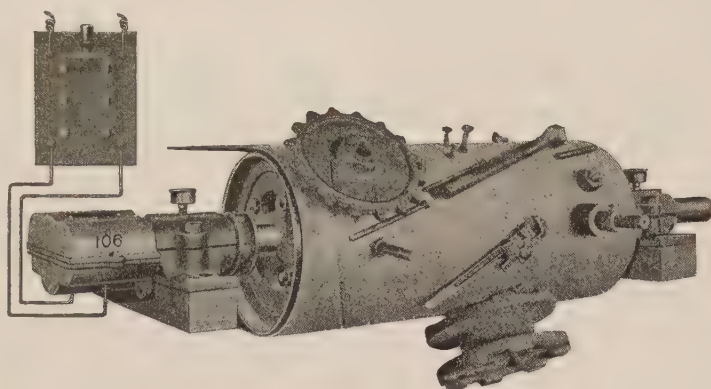
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Grain Scale Book Size of page, 10½x15¼ inches. This indexed receiving book is designed to meet the needs of dealers who want something better than the ordinary. Separate pages are given to each farmer and all are indexed so that their accounts may be instantly located. If so desired, it can be used as a grain journal, the two "L. F." columns providing for posting both debits and credits and entering the numbers of the ledger pages.

The book contains 252 numbered pages of high grade heavy linen ledger paper, each ruled for 41 wagon loads, thus having room for 10,322 loads, in addition to a 28-page index. Together with "Grain Shipping Ledger" it forms a complete set of books of high grade.

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Grain Dealers Journal

309 So. La Salle St.

Chicago, Ill.

Next Year— Build to *Keep Water Out!*

WATER in grain elevators has taken terrible toll through the years—money will never measure the harm it has done. Ruined grain, rusted equipment, molded and rotted conveyor and elevator belts—all represent loss due to water in elevator and scale pits, boots and sumps; water that had no business there; water that could have been kept out.

Next year, *you keep it out!* Start right now with your plans; and see that your cement specifications for new work call for “Medusa Waterproofed Cement” or that they provide for the addition of 8 pounds Medusa Integral Waterproofing (Powder or Paste) to every barrel of plain portland cement you use.

It's perfectly simple—and *it works.* The Medusa Waterproofing in the cement coats the pores or voids in the mass with a lining that *keeps moisture back.* That's all there is to it—and it lasts as long as the concrete itself. The extra cost for the Waterproofing is something less than 75 cents per barrel of cement, at the mill.

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*Manufacturers of Medusa White Portland Cement (Plain and Waterproofed);
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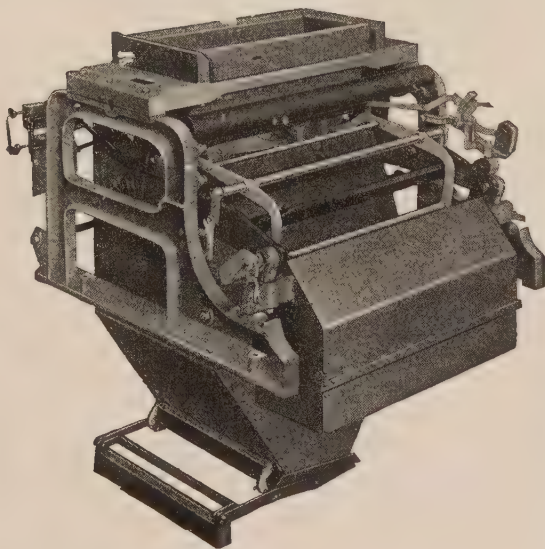
MEDUSA

WATERPROOFING

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Claims For Losses In Transit Can Be Collected, If the Shipper Can Prove His "Weights"



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Will give you full service in this respect. Grain is balanced with standard test weights on a Richardson Equal Arm Beam. No accuracy could be closer.

Every requirement of the I. C. C. is carried out in the design and construction of the machine.

Thoroughly tested by years of actual service. Requires only five minutes of your time to properly inspect and test. Don't experiment with less satisfactory scales that can't give you the full service of a Richardson.

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Any length, diameter or gauge of steel with round or square head. Fully guaranteed. Each spout treated with heavy coat of high-grade black asphaltum paint to prevent rust and insure lasting durability.

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Draw a line through the supplies wanted, and write us regarding your contemplated improvements or changes. We will place you in communication with reputable firms specializing in what you need, to the end that you will receive information regarding the latest and best.

Information Bureau

Grain Dealers Journal, 309 So. La Salle St., Chicago



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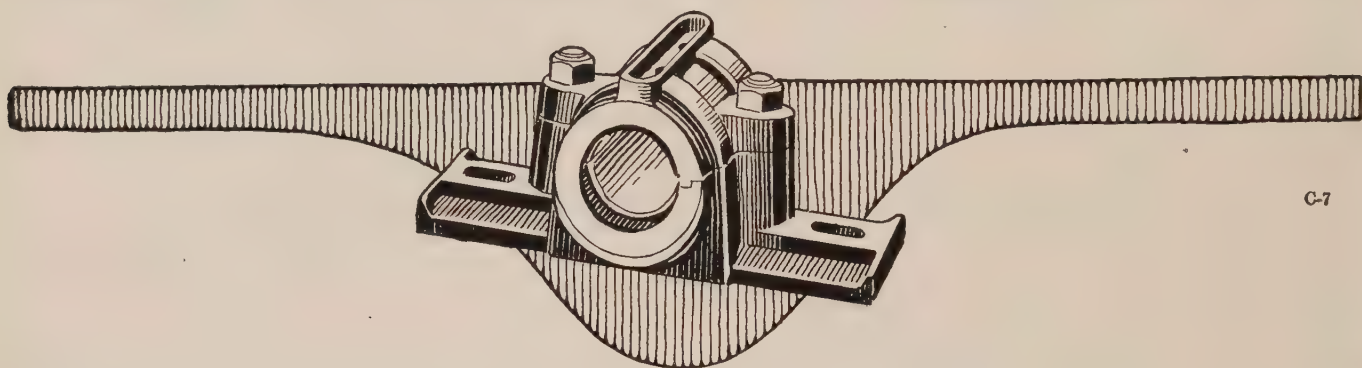
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This book contains 250 leaves. Each of the 125 original leaves bears four scale tickets, is machine perforated, printed on white bond, size of tickets 3x6½ inches. The 125 duplicates are printed on manila but not perforated. Check bound at top of tickets with hinge top cover, 500 tickets in each book arranged horizontally. Size of book 7½x12 inches, each book supplied with 5 sheets of carbon.

The printing is crosswise ticket and has spaces for the following record: "Owner, Hauler, Grain, Grade and Dockage, Gross, Tare, Net, Total Dockage, Net Pounds, Bushels, Price and Amount, Storage Ticket No., Station Ticket No., and Date, Weigher, Name of Firm or Buyer."

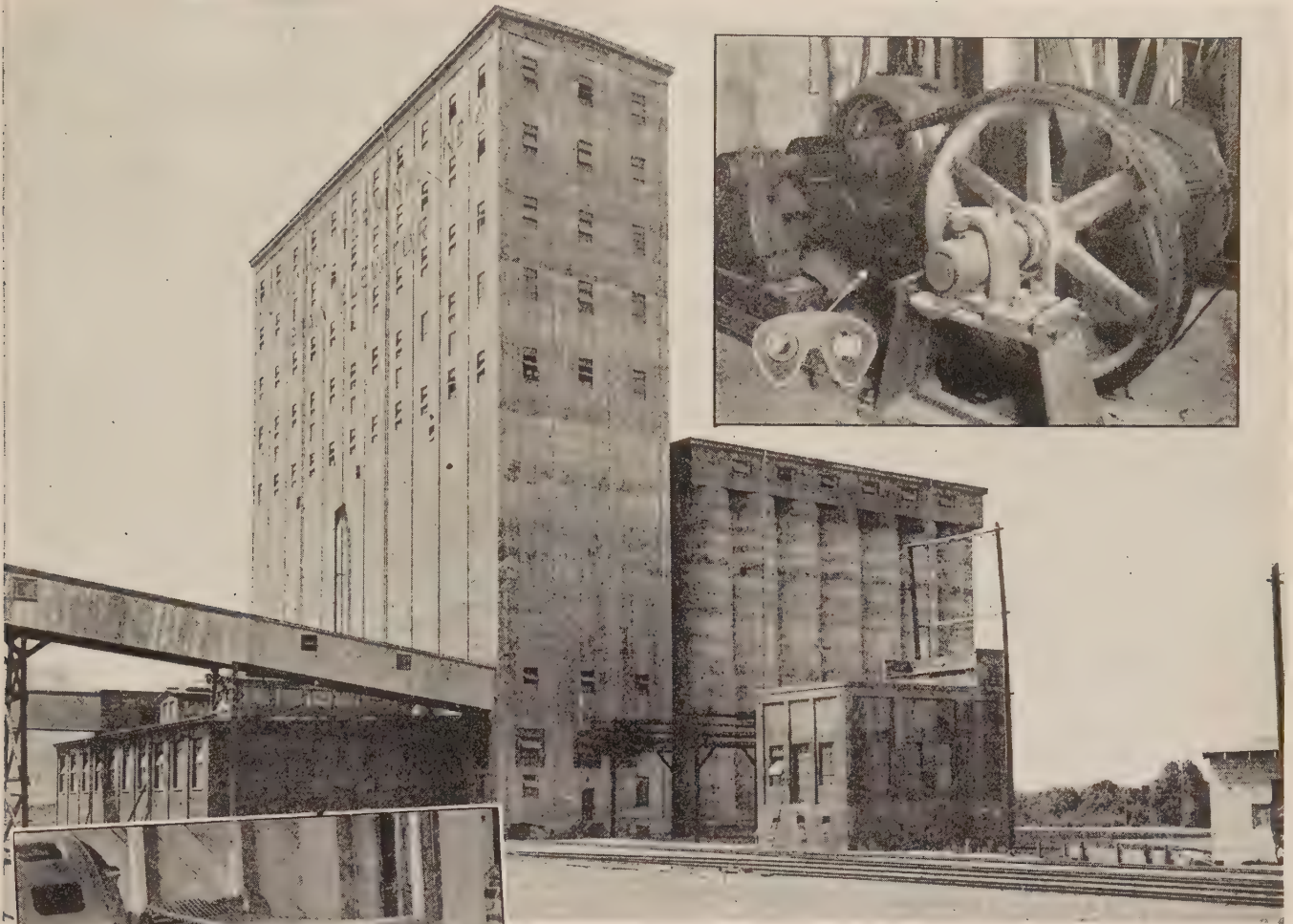
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Grain Dealers Journal

309 So. La Salle St.

Chicago, Ill.



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(CASINGS REMOVED FOR PHOTOGRAPHING)

ALL of the main drives in the Portland, Ore., municipal elevator are Link-Belt Silent Chain. They were installed in 1919, and have been a source of great satisfaction to the operators of this elevator.

Link-Belt Silent Chain is the efficient drive for grain handling plants. It eliminates slip; saves power; is compact, long-lived, reliable, safe; and is proving its superiority by its daily performance. It has earned an enviable reputation for efficiency in power transmission. Every drive is guaranteed, every installa-

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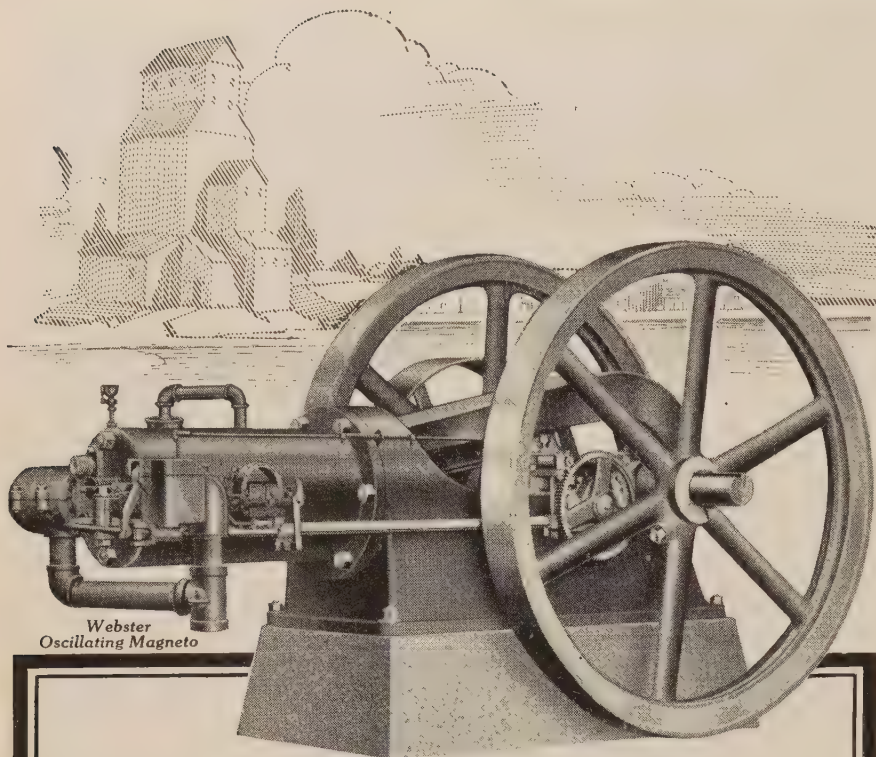
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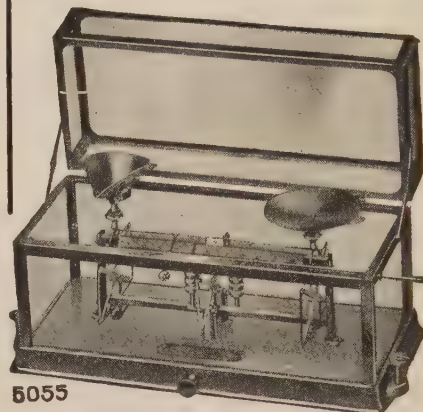


The Worthington nameplate on any piece of machinery is a guarantee of quality. The product has been on the market 80 years.

In addition to the industrial type of engine, Worthington manufactures a complete line of hopper cooled engines $1\frac{1}{2}$ to 15 H. P. for farm and general service, and also oil and gas engines up to 4000 H. P. for every service.

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I have the Grain Dealers Journal habit and renew regularly. It is like a letter from home.—H. Fike, mgr., Burbank Equity Exchange Co.

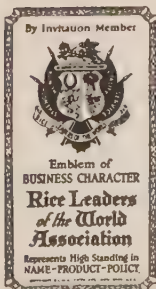


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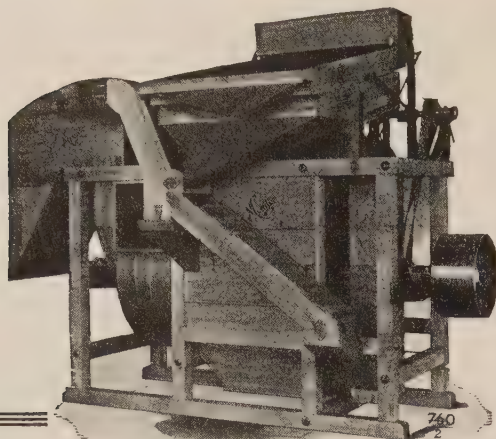
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when if your clipper was fitted to give the grain screen cleaning and two air cleanings without clipping, it would mean real money.

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309 South La Salle Street

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The Morris Automatic Grain Drier receives your grain in any state of wetness and dries and cools it to exactly the degree you require. And it dries your grain evenly throughout. None is parched or made brittle. No overheating. No baked smell. Every kernel receives the same, absolutely uniform exposure. It's worth a great deal to any grain man to know that The Morris does this job right, without constant attention.

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Everything for Every Mill and Elevator



Gruendler Feed Mixer and Grinder

America's
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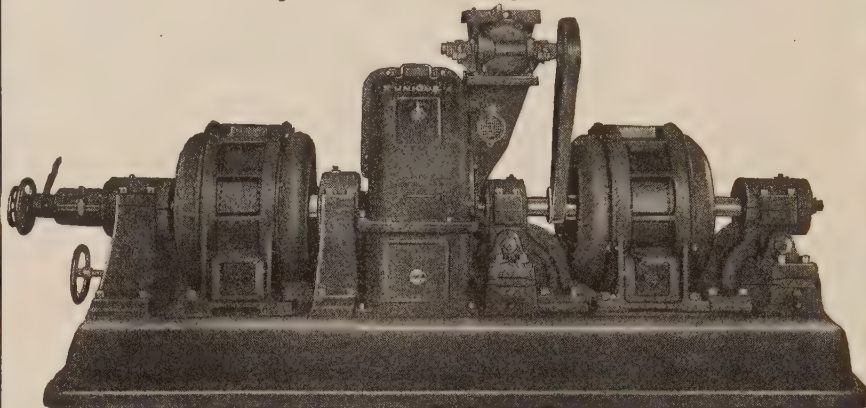
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Gruendler Feed Grinders have been serving the field for 50 years. All parts are easily adjustable and accessible. Made of best steel throughout. All machines are guaranteed for 2 years against defects in material or workmanship. Unbreakable.

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(Belt Driven If Preferred)

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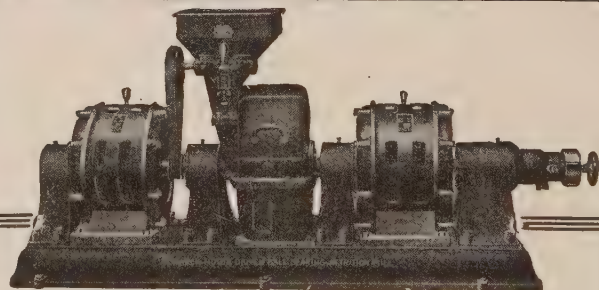
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BALL BEARING
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DIRECT HEAT OR STEAM CONTINUOUS FLOW MADE IN ALL SIZES
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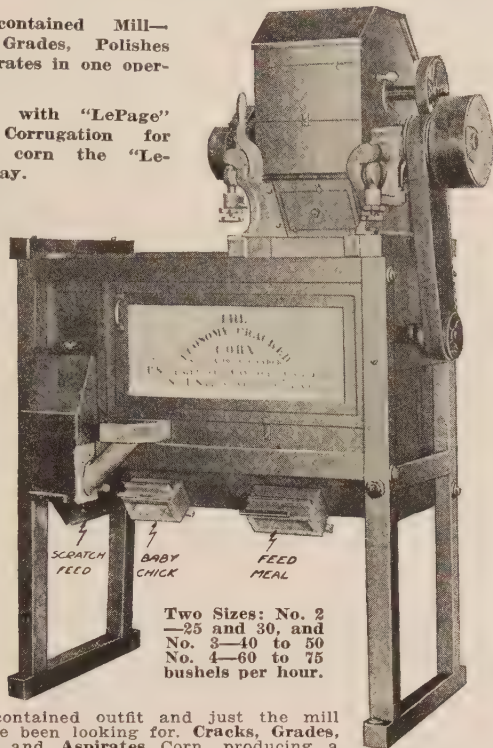


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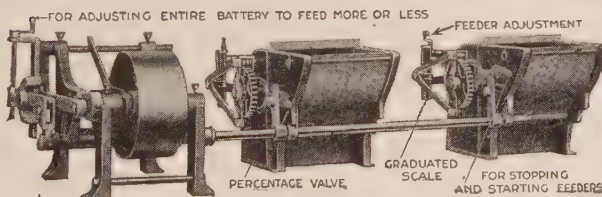
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A mill at little cost that will yield large profit.

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For mixing two or more ingredients in a running stream
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Draver Feeders with One Belt Master Drive are
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The most improved and economical method for
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The MASTER DRIVE is an exclusive Draver Feature
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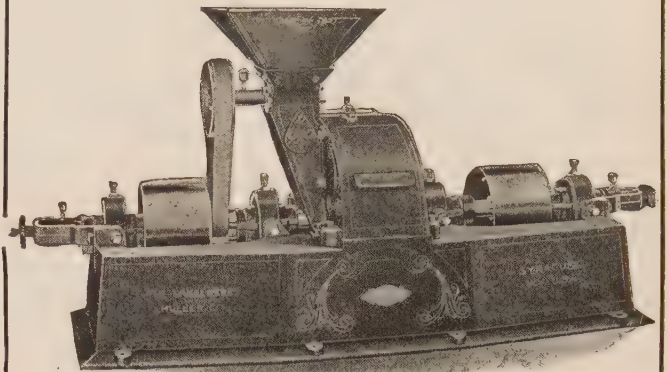
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The "HALSTED" HAS NO EQUAL



No Seal Rings
Scientific

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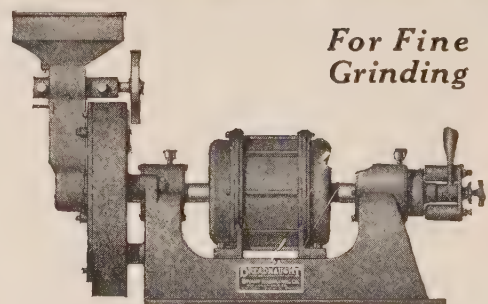
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Grinding Plates alike on both sides, and being re-
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Highest efficiency at the smallest expense.

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For Fine
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A powerful, efficient feed grinder for installa-
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with "Shear-Cut" plates and ball bearings.
Especially adapted to cutting oat hulls and
other feed difficult to reduce to fineness.

In ordinary service the DREADNAUGHT
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Feed Grinders
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Feed Screens
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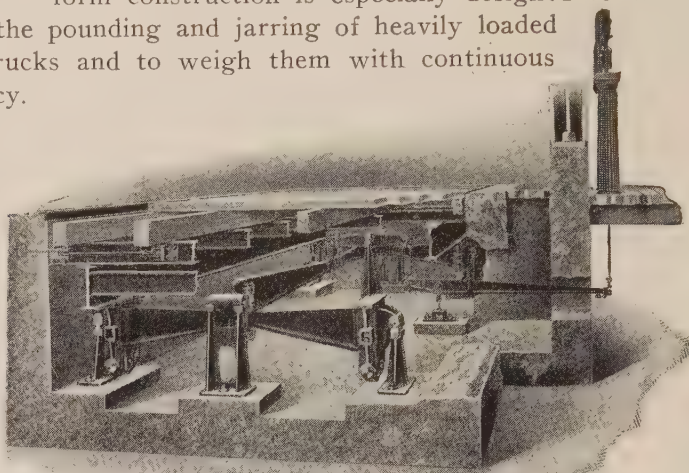
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ACCOUNTBOOKS FOR GRAIN DEALERS

GRAIN RECEIVING BOOK Form 12 AA is designed to facilitate keeping a record of weights and number of bushels in wagon loads of grain received.

Each page is 8¼x14 inches, and at top of the 11 columns are printed Date, Name, Kind of Grain, Gross, Tare, Net, Bushels, Pounds, Price, Amount and Remarks.

Each page has spaces for 40 wagon loads and each book has 160 pages or spaces for records of 6,500 loads. The book is well printed, ruled on ledger paper, and substantially bound in full heavy canvas covers. Weighs 2½ lbs.

GRAIN SHIPPING BOOK Form 14 AA is designed to facilitate recording sales, shipments and returns from the shipments made. Its use will save much time and book work. The pages are 10¼x16½ inches, used double. The left-hand pages are ruled for information regarding **Sales and Shipments**; the right-hand pages for **Returns**. Under **Sales** the column headings are Date, Amount Sold, Price, Grain, Terms. Under **Shipments** are Date, Car Number and Initial. Our Weights in Bushels, Grade, Route, Rate. Under **Returns** are Destination, Grade, Difference, Bushels Over, Short, Gross Proceeds, Freight, Over, Short, Commissions, Other Charges, Total Charges, Net Proceeds, Drafts, Remarks.

The book contains 76 double pages, with room for records of 2,250 cars, is printed on linen ledger, well bound in heavy canvas covers with keratol corners. Weighs 4 lbs.

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GRAIN DEALERS JOURNAL

309 So. La Salle Street, Chicago.

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WHEN EVERY AGENCY FOR
SAFETY OPERATION SAYS
"DO AWAY WITH DUST"
IT'S TIME THAT YOU LIS-
TENED TO REASON—



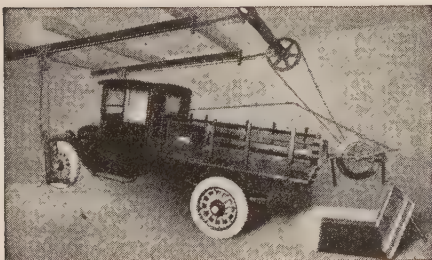
The hundreds
of thousands
of dollars lost
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explosions
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in the country.

Don't be the next
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309 So. La Salle St. Chicago, Ill.

Dust Collectors



alone do not prevent explosions in Elevators, but

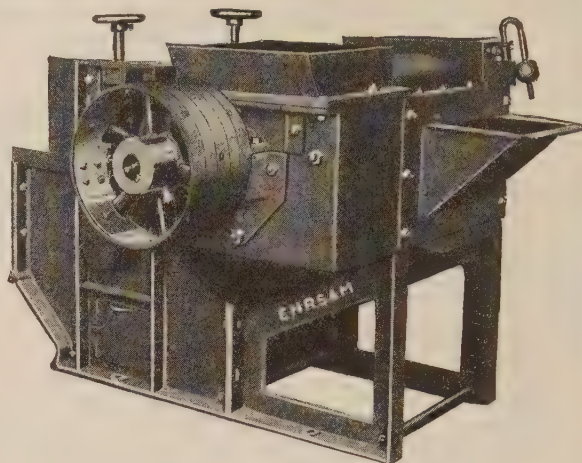
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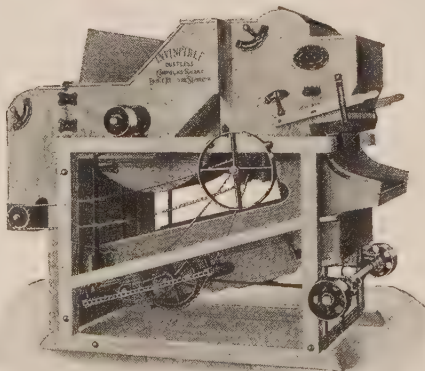
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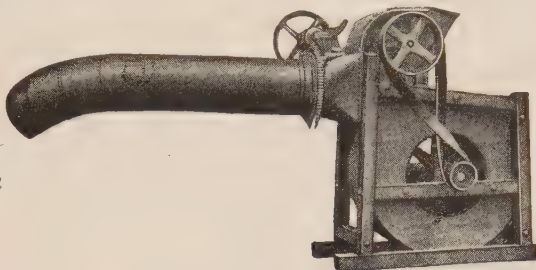
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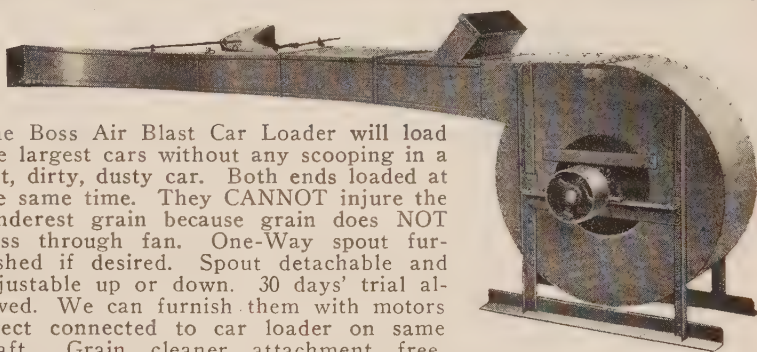
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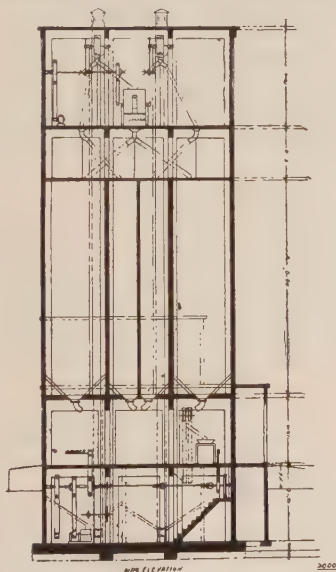
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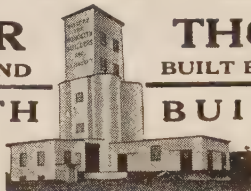
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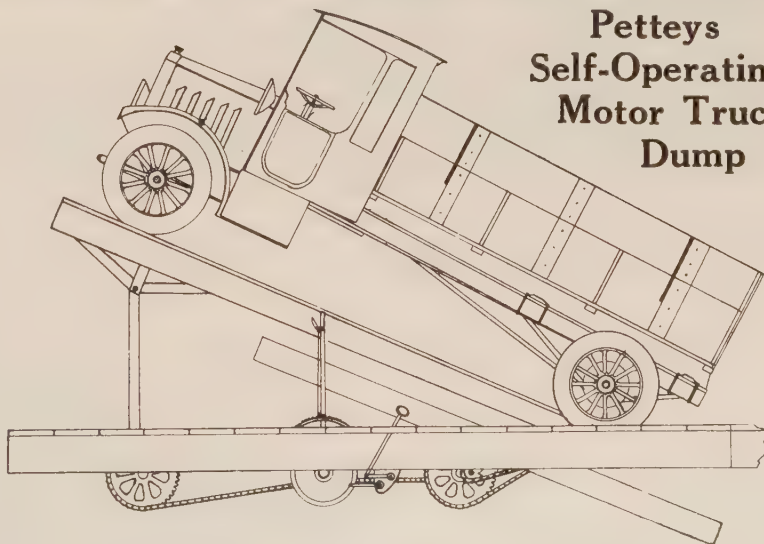
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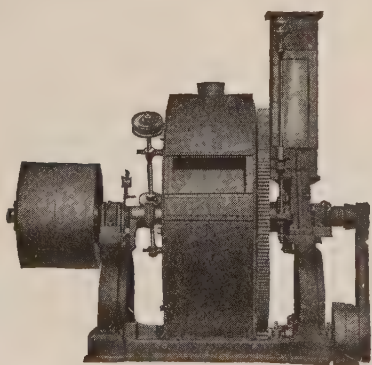
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MISSOURI—We will sell a good 10,000 bu. elevator in northwest for \$7,000, where conditions are good and elevator in good shape. Write 49T16, Grain Dealers Journal, Chicago.

IOWA—Grain, coal and feed business for sale; new 37,000 bu. concrete elvtr., 10,000 bu. wood. Ship 300 to 400,000 bu. grain annually. Storage for 900 tons coal. Address 49S17, Grain Dealers Journal, Chicago, Ill.

INDIANA—Three good elevators located in Northeastern Indiana; good corn, oats, wheat and rye section. Good established business. Coal and retail business. No competition in towns. Only interested parties need apply. Address 49T1, Grain Dealers Journal, Chicago, Ill.

ELEVATORS FOR SALE OR TRADE.

SOUTH DAKOTA—For sale or trade, 25m capacity elevator; flour house and coal sheds; operating daily; station will handle 500,000; crop just moving; only three houses here; good territory. 49T7, Grain Dealers Journal, Chicago.

ELEVATOR FOR SALE OR LEASE.

NORTH FLORIDA—For sale or lease, grain elevator and feed mill. Fully equipped; also warehouse and dwelling. Good grain country. Red clay section. Will lease with buying privilege. Address Jefferson Co. Products Co., Monticello, Fla.

ELEVATOR BROKERS.

EXCHANGE of owners, mills, hotels; elevators a specialty. Langford, Saunemin, Ill.

JOHN A RICE **ELMER N. SMITH**
Elevator Brokers, Frankfort, Indiana.

CLAYBAUGH-McCOMAS
Offices

Frankfort, Ind. Indianapolis, Ind
223 B. of T. Bldg. 601 Board of Trade
If you want to buy, sell or trade an elevator write us at either address.

ALWAYS HAVE ELEVATORS For Sale in the Grain Belt of Illinois and Eastern Indiana. If you are in the market write me fully and I will try to satisfy you.

JAMES M. MAGUIRE,
432 Postal Tel. Bldg., Chicago, Ill.

FLOUR FOR SALE.

MIXED CARS of flour and mill feeds in 100 pound sacks are our specialties. We are now manufacturing a full line of corn goods, cracked corn, feed meal, corn and oats chop, Ohio Farm feed, shelled corn and standard oats in connection with our flouring mill. Would like to send you a trial to convince you of the superiority of our products. Ansted & Burk Co., Springfield, Ohio.

ELEVATORS WANTED.

WANTED—Elevator and side lines in Iowa. Address 49U12 Grain Dealers Journal, Chicago.

WANTED to lease elevator in central Illinois. Address 49U22 Grain Dealers Journal, Chicago.

WILL TRADE 160 acres choice Hancock county, Iowa, land for good Iowa or Illinois elevator. Address Box 106, Ft. Dodge, Iowa.

WILL TRADE for grain elevator 200 acres improved farm in the Red River Valley, North Dakota. Address Farmer, 1122 Flour Exchange, Minneapolis, Minn.

THE WAY inquiries are coming in from my ad may make it necessary to insert another in the Journal stating that the elevator has been sold.—F. C. H.

WANTED—Elevator doing business of 250,000 bus. annually, central Illinois, located at competitive rate point, good live town. Address 49U21 Grain Dealers Journal, Chicago, Ill.

WILL TRADE for grain elevator a quarter section of land in North Dakota with one hundred acres under cultivation, good crop. Address Farmer, 1122 Flour Exchange Building, Minneapolis, Minn.

FOR SALE OR TRADE.

3 story, 3 apartment solid brick building. A-1 condition, So. Chicago. For elevator. Price \$12,000. Address E. A. White, owner, 104 Wash Ave., Terre Haute, Ind.

ELEVATOR & MILL FOR SALE

NEW PLANT FOR SALE.

300,000 bu. elevator, 12,000 bu. cereal mill, fire proof buildings, modern equipment transit privileges, two private spurs on Illinois Central Railroad, priced to sell. Address C. E. Beyer, 1504 Southern Bldg., Louisville, Ky.

THE WANTED—FOR SALE DEPARTMENT of the Grain Dealers Journal is a market place where buyer and seller, employer and employee, and those offering investments can meet to their mutual advantage and profit, and it will pay every subscriber to give these columns a close study twice each month, because of the constantly changing variety of opportunities seeking your consideration.

MICHIGAN—A large flouring mill and elevator in one of the finest farming sections with very small competition. Capacity of mill 60 barrels per day. Capacity of elevator 22,000 bushels. Well equipped in every way. Four thousand dollars worth of new feed grinding machinery, capacity 125 bushels per hour. New electric motors all around; large new warehouses and coal sheds costing over \$6,000. Mill is open for inspection every day and can be handled for a small amount of money. Call or write J. A. Jewell, Receiver, Romeo, Mich.

OHIO—GRAIN ELEVATOR AND FLOUR MILL at Kennard, Champaign County, Ohio, of J. L. Hewling, at auction on Nov. 9, 1922, at 2 P. M. Elevator fully equipped with modern machinery, scales, cribs, bins, dumps, etc. Located on Erie R. R. in fine farming community. Established business. Mill equipped with American Midget Marvel Mill of 25 bbl. capacity, complete with packer, dust collector, elevators, magnets, automatic scale, scourer and separator. Had less than one year's use. An excellent opportunity to get an elevator and mill at your own price. Elevator and mill will be offered separately. Address communications to Bessie L. Hewling, Cable, O., R. D. 3.

BUSINESS OPPORTUNITIES.

DO YOU WANT a good business for \$10,000 where the overhead is low and in a terminal market where the chances to grow are good? Write 49T15, Grain Dealers Journal, Chicago, Ill.

EASTERN KANSAS—Prosperous grain, coal and feed business for sale. Modern ironclad elevator. Best reasons for selling. Address 49S7, Grain Dealers Journal, Chicago, Ill.

MINNESOTA — An electric power money making feed mill and two dwellings situated on six large lots in the village of Verndale. Am obliged to sell on account of ill health. A first class opportunity for younger man. Inquire Hans Oehlert, Verndale, Minn.

SOUTHEASTERN KANSAS—A fine location for an elevator; on the Mo. Pacific Ry.; 7—25 ft. lots. Private switch. Large quantity of wheat shipped out of here each year. Old elevator burned. Address Box 97, Elk City, Kans.

IDAHO—A grain, seed and coal business in one of the best irrigated seed-growing sections in Idaho. A growing concern established in 1909 doing a wholesale and retail business. Brick store building with modern fixtures, uptown, warehouse with seed cleaners and feed grinder and large coal yards and sheds on R. R. spur with 250 ft. of trackage. Would make a splendid branch for a larger seed house or an opportunity to place that son of yours in a good business. Reason for selling wish to concentrate all our efforts on other interests. It will take \$40,000.00 to swing the deal. Address 49U2 Grain Dealers Journal, Chicago, Ill.

MILL FOR SALE.

MODERN 100-BARREL MILL for sale; large territory, Rocky Mountain section; plenty of wheat. Feed mill in connection. Cheap power. Address 48B4, Grain Dealers Journal, Chicago.

FOR SALE.

Whole or part interest in modern feed milling and mixing plant of fair capacity in large eastern Pennsylvania city; transit facilities, own siding and room for expansion. Write 49Q15, Grain Dealers Journal, Chicago, Ill.

ADDRESS WANTED.

ADDRESS wanted of Lindley C. Binford, formerly in grain business at Haviland, Kansas. Last known address Topeka, Kans. Address 49T20, Grain Dealers Journal, Chicago, Ill.

OFFICE APPLIANCES.

SAFES—Large stock of new and used safes on hand. Protect your valuable papers. Prices reasonable. The Howe Scale Co. of Ill., 512-514 St. Charles St., St. Louis, Mo.

WOOL WANTED.

ELEVATOR OPERATORS who have wool to ship will find it to their advantage to tell the 6700 regular readers what they have to offer. An advertisement in this column will cost you but 25c per type line per insertion.

WANT ADS WORK WONDERS.

They sell elevators, find help and partners. secure machines and engines which you want. sell those for which you have no further use. and perform a myriad of kindred services for shrewd people who use them regularly. READ and USE THEM.

SITUATIONS WANTED.

WANTED—Position as manager country elevator. 10 years experience. Can furnish No. 1 reference. J. L. Lininger, Syracuse, Nebr.

WANTED—Position by young man, six years experience as manager. Knows grain accounting thoroughly. Address 49U18 Grain Dealers Journal, Chicago, Ill.

MARRIED MAN wants position as manager of grain and implement company; have had 13 years' exp. in grain business and 17 years in implement business; middle aged. Address 49Q12, Grain Dealers Journal, Chicago, Ill.

WANTED—Position as manager and grain buyer for independent or farmers elevator. Have had several years practical experience. Good bookkeeper. Employed steady since 1912. Address 49U23 Grain Dealers Journal, Chicago

WANTED—Position as manager of country elvtr. Experienced in side lines. Managing elvtr. at present, wish to make change. Will consider position in brokerage office with opportunity to learn business. Prefer Ohio. Address 49U6 Grain Dealers Journal, Chicago, Ill.

EXPERIENCED grain inspector wants position either as Inspector or with terminal elevator co. Well qualified to operate elevator or manage Inspect. Dept. A1 references. Married and 32 years old. Address 49R18, Grain Dealers Journal, Chicago, Ill.

COMPETENT GRAIN MAN wants position as supt. or foreman of large elevator or a position with a grain firm that will bring a future. Ten years experience; 35 years of age and married. Can furnish best of references. Address 49T26, Grain Dealers Journal, Chicago, Ill.

POSITION wanted as terminal elevator superintendent or location as licensed grain inspector in any large market by man with 17 years experience in four terminal markets. Familiar with grading and scientific mixing of all types of grain. Executive ability. Best of references. Age 36. At present employed. Address 49U3 Grain Dealers Journal, Chicago, Ill.

SAMPLE ENVELOPES.

SAMPLE ENVELOPES—SPEAR SAFETY—for mailing samples of grain, feed and seed. Made of very heavy manila for strength and durability and to withstand hard usage. Special folding method for closing envelope. Have a limited supply to sell at \$2.50 the hundred f. o. b. Chicago. Sample mailed on request. Grain Dealers Journal, 309 So. La Salle St., Chicago, Ill.

HELP WANTED.

WANTED—Manager of elevator experienced in hay and grain. Address Ohlman Co-op. Co., Ohlman, Ill.

WANTED—Elevator man for one man house. Eastern central Illinois. Address 49T24, Grain Dealers Journal, Chicago, Ill.

WANTED—Experienced manager for independent country grain elevator. Address, with ref., 49U19 Grain Dealers Journal, Chicago.

WANTED—Married man experienced in grain and lumber to assist in managing small Northern Illinois Station. Address Holcomb Lumber Co., Sycamore, Ill.

HIGH POWERED man wanted to act as secretary and sales manager of one of the prominent grain machinery manufacturing companies. Must have ability and experience. All replies strictly confidential. Address 49S13, Grain Dealers Journal, Chicago, Ill.

HELP WANTED. WITH INVESTMENT.

WANTED—Competent grain man, terminal elevator experience, to buy active interest in established business. Address 49U11 Grain Dealers Journal, Chicago, Ill.

WANTED — Experienced country elevator manager with sufficient capital to buy interest in established business. Give references and experience. Address 49U10 Grain Dealers Journal, Chicago, Ill.

MISCELLANEOUS.

BAG MENDING—Want address of Michigan firm equipped to mend bags. Address 49U9 Grain Dealers Journal, Chicago, Ill.



EDGAR Improved Seal
The number section is protected by a safety clip

THE MIDLAND FLOUR MILLING CO.,
Kansas City, Mo.

"We have found your seals very satisfactory. We believe it is the only seal that meets the requirements of the Interstate Commerce Commission to the extent that it cannot be manipulated."

Ask for samples and prices today

The Edgar Steel Seal & Mfg. Co.
LAWRENCE, KANSAS

Duplicating Wagon Load Receiving Book

| Hauler | Gross | Tare | Net | |
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This book is designed to facilitate the work of country buyers during the busy season when each farmer is hauling a number of loads at a time. The above illustration shows the left hand side of the sheet which remains in the book. The outer half has the same rulings, but is printed on the other side of the sheet, so that when sheet is folded back on itself, and a sheet of carbon is inserted, an exact duplicate will be made of each entry. Each page has room for 33 loads and is machine perforated down the middle so outer half may be torn out and given to the farmer or sent to headquarters of line company.

The book is 12x12 inches, check bound with canvas back, contains 225 leaves ruled both sides, and nine sheets of carbon.

Order Form 66. Price \$3.00. Weight. 4 lbs.

Grain Dealers Journal 309 So. La Salle St. Chicago, Ill.

BARGAIN SALE ON SHELF-WORN BOOKS.

Two Universal Grain Codes, shelf worn from being used as samples. They are slightly soiled. Leather bound, containing 13,745 code words as well as the latest supplement for U. S. Standards for wheat, corn and oats. Order "Bargain Universal." Price while they last, \$2.00 per copy.

One No. 4—Storage Receipts, for keeping record of grain stored for farmers. Contains 47 receipts and 47 stubs. Prevent misunderstandings, avoid disputes, law suits and losses by using receipts. Get this book; it's a bargain. Hurry! Sale price, 35c. Include postage to your postoffice. Order "No. 4 Special."

One Decimal Grain Value Book for converting pounds into dollars and cents for any quantity of grain from 10 to 100,000 pounds at any given market price. Table includes oats from 10c to 79c per bushel; corn, rye and flaxseed 10c to \$1.09 per bushel; wheat, clover and peas, 30c to \$1.59 per bushel; and barley and buckwheat 20c to \$1.49 per bu. This copy is damaged from use as a sample, so will sell for \$2.00 plus carriage charges. Regular price \$5.00. Order No. 36 "Special."

One Grain Receiving Register designed for recording wagon loads of grain as received. Headings read—Date, Name, Kind of Grain, Gross, Tare, Net, Bushels, Pounds, Price, Amount, and Remarks. Contains 160 pages of Linen Ledger paper, each of which is ruled for 41 entries, giving a total capacity of 6560 wagon loads. Can be used in three ways: enter loads in order received; devote different sections of the book to different kinds of grain; and thirdly give each patron a separate page. This book is well printed and substantially bound in full canvas. Regular price \$2.75; will sell this slightly soiled copy for \$2.00 plus postage and insurance—weight 3 lbs. Order No. 12 "Special."

All prices are f. o. b. Chicago, Illinois.

GRAIN DEALERS JOURNAL,
309 So. LaSalle St., Chicago, Ill.

Helpful Books FOR Carlot Grain Handlers

Clark's Decimal Grain Values: Saves time and money and prevents errors. It shows at a glance, or with simple addition, the cost of any quantity of grain from 10 to 100,000 pounds at any given market price and reduces pounds to bushels on the same page. Values are shown directly from pounds without reducing to bushels. Pounds shown in red figures and values in black; price being given at top and bottom of each page. Prices for oats range from 10 cents to 79 cents a bushel; for corn, rye and flaxseed, 10 cents to \$1.09 per bushel; for wheat, clover, peas and potatoes, 30 cents to \$1.59 per bushel; for barley and buckwheat, 20 cents to \$1.49 per bushel. Order Form 36. Price \$5.00.

Clark's Car Load Grain Tables: The eighth edition of Clark's Car Load Grain Tables is the most complete car load reduction table ever published. The tables show reductions by 50-pound breaks as follows:

| | |
|------------------------|--------------------|
| 20,000 to 107,950 lbs. | to 32 lb. bushels. |
| 20,000 to 74,950 lbs. | to 34 lb. bushels. |
| 20,000 to 96,950 lbs. | to 48 lb. bushels. |
| 20,000 to 118,950 lbs. | to 56 lb. bushels. |
| 20,000 to 118,950 lbs. | to 60 lb. bushels. |

Bushels are printed from bold faced type in black ink; pounds in red, on heavy ledger paper, sewed and reinforced with muslin, and bound in flexible keratol covers with marginal index. Weight 6 ounces. Price \$2.50.

Clark's Double Indexed Car Register: Is an index designed to afford ready reference to the entry or record of any car. Facing pages 11x16 inches of heavy ledger paper are each ruled into five columns, those on the left being numbered 0, 1, 2, 3 and 4, while columns on the right are numbered 5, 6, 7, 8 and 9. Each column is ruled into three distinct divisions with the following sub-headings: "initial," "Car No." and "Record." The marginal index figure represents the right hand or unit figure of the number entered, while the column heading is the second or tens figure. The required number can be instantly found if properly entered. Form No. 40, with space for 12,000 cars, \$2.00. Form No. 42, with space for 21,600 cars, \$3.00.

GRAIN DEALERS JOURNAL,
309 So. LaSalle St., Chicago, Ill.

MACHINES FOR SALE.

BOSS CAR LOADER for sale; one No. 8. Address A. H. Richner, Crawfordville, Ind.

CLIPPER CLEANER—Hand power—for sale or will trade in on one of larger capacity. J. C. Jordan, Colfax, Ind.

THE SMALL EXPENSE incurred through the placing of an advertisement in the Grain Dealers Journal is more than compensated for by the results obtained.

FOR SALE at Southern South Dakota point, machinery and complete equipment of 100 barrel mill, including 75 horse power Corliss engine, all in A-No. 1 condition. \$1,500.00 if taken at once. Address Miller, 1122 Flour Exchange Building, Minneapolis, Minn.

DURABLE WIRE ROPE for sale, for car shovels; cast or plow steel. Manila rope, buckets and everything in elevator supplies.

PULLEYS—1,000—for sale. ALL sizes. solid cast iron, wood and steel split. Standard Mill Supply Co., Kansas City, Mo.

BARGAINS.

1 No. 5 Williams Mill. Ground less than 100 tons of hay.

1 Double Giddings Packer

1 Dust Collector.

All for No. 5 Williams Mill.

1 Corn Sheller.

2 Jumbo Hay Balers.

Address Dwight Hamlin, Inc., Arsenal Station, Pittsburgh, Pa.

ELECTRIC MOTORS FOR SALE.

1 75 H. P.

1 50 H. P.

1 40 H. P.

all complete with starters. Also

2 66"x18' Boilers.

1 60"x110' Smokestack.

1 500 H. P. Allis Engine.

2 No. 3½ Iron Prince Scourers.

Write for description and price.

EDGAR-MORGAN COMPANY,
Memphis, Tenn.

REAL BARGAINS.

Prompt Attention. Quick Shipments. When in need of elevator or mill machinery, notify us. We are headquarters for power and transmission equipment, and have on hand several well-known makes of motors, boilers, engines, etc.

Send us list of all your wants. We can supply you with full line of machinery for elevators, flour, corn and cereal mills. Complete equipments for modern mills of all kinds, molasses, stock, and poultry feed plants, plans, specifications, flow sheets, etc., our specialty.

Write us without delay

Geo. J. Noth, Mgr.,

9 S. Clinton St.

Chicago, Ill.

MACHINES WANTED.

WANTED—Small chicken feed mixing equipment for mixing scratch feed and baby chick feed. Do not need grinder. Send full description, blue prints if possible, and name lowest price. J. C. Mytinger Grain Company, Wichita Falls, Texas.

SCALES FOR SALE.

FOR SALE—Special ball bearing 10 ton Howe auto truck and wagon scale. New, never used. Address John Kinsella, Colfax, Ill.

DYNAMOS—MOTORS.

FAIRBANKS-MORSE. Motor for sale. 25 H. P. 3 phase, 60 cycle, 40% overload with overload relay switches and starter complete. Run one year and guaranteed good as new. Price \$285. Address J. M. Bergeson, Ashton, Ill.

TWENTY HORSE WAGNER electric motor for sale at half price. One twenty horse-power single phase, sixty cycle, 110/220 volt, 1165 RPM with sliding rails and pulley, complete with rheostat, \$225. Address O. A. Talbott & Co., Laclede, Mo.

OIL & GAS ENGINES FOR SALE

FOR SALE—15 h.p. Fairbanks Z engine, good condition; reasonable. Address P. O. Box 103, Bloomington, Illinois.

FOR SALE—Two new 16 horse-power Stover heavy duty kerosene engines. Less than wholesale price. No trades. L. Box 748, Eldorado, Kan.

FOR SALE—One 22 H. P. Gasoline engine in first class shape. will guarantee it to run satisfactory; have installed electric motor, reason for selling engine. Address 49U8 Grain Dealers Journal, Chicago, Ill.

BAGS—BAGGING—BURLAP.

BURLAP BAGS OF EVERY KIND FOR SALE; new or second-hand, plain or printed with your brand; seamless Cotton Grain Bags; Sample Bags; Burlap, Cotton Sheetting or Paper for Car Lining, etc. Wanted: Second-hand bags, best prices paid.

WM. ROSS & CO., 409 N. Peoria St., Chicago.

DYNAMOS AND MOTORS WANTED—Buyers of this equipment are reached in largest numbers and at the least expense through the use of the "DYNAMOS-MOTORS" columns of the Grain Dealers Journal,—the medium for power bargains.

KEEP POSTED

GRAIN DEALERS JOURNAL

309 So. La Salle St., Chicago, Ill.

Gentlemen:—In order to keep us posted regarding what is going on in the grain trade outside our office, please send us the Grain Dealers Journal on the 10th and 25th of each month. Enclosed find Two Dollars for one year.

Name of Firm.....

Capacity of Elevator.....

Post Office.....

..... bus.

State.....

SEEDS FOR SALE—WANTED

SEEDS FOR SALE—WANTED

SEED WHEAT WANTED.

Mediterranean Soft Wheat wanted in car lots for Northern and Central Texas. List of dealers wanting seed will be given shippers who have choice seed to offer. Address H. B. Dorsey, Secy., Texas Grain Dealers Ass'n, Fort Worth, Texas.

RUDY PATRICK SEED CO.

BUYERS AND SELLERS
Alfalfa, Blue Grass, Clover,
Timothy, Sudan, Millet and Cane.
KANSAS CITY, MO.

L. Teweles Seed Co.

MILWAUKEE, WIS.

Grass and Field Seeds

North American Seed Co.

WHOLESALE GRASS & FIELD SEEDS

Milwaukee, Wisc.

"THE HOUSE OF QUALITY"

We Buy:-
Timothy
Red Clover
Alsike Clover
Sweet Clover
Millets

Special Varieties Seed Grain

Sample Bags Furnished Free

NORTHROP KING & CO.
MINNEAPOLIS --- MINN.

MINNEAPOLIS SEED COMPANY

MINNEAPOLIS, MINN.

WE ARE BUYERS AND SELLERS

TIMOTHY—CLOVERS—MILLETS

Grass Seeds and Seed Grains

Send samples for bids

Ask for samples and prices

RED OATS!

WANT TO BUY
WANT TO SELL

SHERMAN - TEXAS
PITTMAN & HARRISON CO.

ALFALFA

TIMOTHY RED TOP

Agricultural Seed Company

Main & O'Fallon Sts.

St. Louis, Mo

OUREN SEED CO.

Council Bluffs, Iowa

Buy and Sell Red, White and Alsike
Clovers, White and Yellow Blossom
Sweet Clover, Alfalfa, Red Top, Blue
Grass and all Seed Grains

IMPORTERS

EXPORTERS

GRASS and
CLOVER SEED

Buyers and Sellers of Timothy, Red Clover,
Alsike, Alfalfa, White Clover, etc.

NUNGESSER-DICKINSON SEED CO.
New York, N. Y., U. S. A.

HEADQUARTERS

on all

Imported

Clover

Grass and Field

SEEDS

Julius Loewith, Inc.

150 Nassau Street New York, N. Y.

(Formerly Loewith, Larsen & Co.)

Field and Grass
Seed Trade Directory

ARNHEM, HOLLAND.

Wm. E. Busgers & Co., European fancy natural gr. sds.

ATCHISON, KANS.

Manglesdorf Seed Co., The, wholesale seeds.

BALTIMORE, MD.

Wm. G. Scarlett & Co., wholesale seed merchants.

BUFFALO, N. Y.

Stanford Seed Co., Inc., The, wholesale field seeds.
Whitney-Eckstein Seed Co., wholesale seeds.

CHICAGO, ILL.

Dickinson Co., The, Albert, seeds.
Illinois Seed Co., The, grass and field seeds.

CINCINNATI, OHIO.

McCullough's Sons, The, J. M., field and garden seeds

CONCORDIA, KANS.

Bowman Bros. Seed Co., field seeds.

COUNCIL BLUFFS, IOWA.

Ouren Seed Co., wholesale seeds and grain.

CRAWFORDSVILLE, IND.

Crabbs Reynolds Taylor Co., grass and field seeds.
Crawfordsville Seed Co., seed merchants.

KANSAS CITY, MO.

Peppard Seed Co., J. G., wholesale seeds.
Tobin Seed Co., alfalfa—bluegrass.

LOUISVILLE, KY.

Hardin, Hamilton & Lewman, grain and field seeds
Louisville Seed Co., clover and grasses.

MILWAUKEE, WIS.

Courteen Seed Co., field seeds.
North American Seed Co., wholesale grass & field seeds.
Teweles Seed Co., L., seed merchants.

MINNEAPOLIS, MINN.

Dickinson Co., The, Albert, seeds.
Minneapolis Seed Co., seed merchants.

NEW YORK, N. Y.

Julius Loewith, Inc., grass and field seeds.
Nungesser-Dickinson Seed Co., wholesale seed merchants

PHILADELPHIA, PA.

Philadelphia Seed Co., Inc., The, wholesale field seeds.

ST. LOUIS, MO.

Agricultural Seed Co., cow peas.
Mangelsdorf, Ed. F. & Bro., wholesale field seeds.

ST. PAUL, MINN.

Jameson Hevener Co., shippers of field seeds.

TOLEDO, OHIO.

Churchill Grain & Seed Co., field seed, popcorn
Flower Co., The, S. W., seed merchants.
Hirsch, Henry, wholesale field seed.
Toledo Field Seed Co., The, clover, timothy.

Kraus & Apfelbaum, Fort Wayne, Ind.

Indiana's Pioneer Grain, Seed and Wool Merchants

Our "AA" Brand of Field Seeds are noted for
their Excellence. - They Get Results.

Ask the Dealers Who Sell Them

We are in the market for all kinds of field seeds

Mail Your Samples to

Kraus & Apfelbaum, Fort Wayne, Ind.

HAY WANTED.



SEEDS FOR SALE—WANTED

**THE
ILLINOIS SEED CO.**
CHICAGO, ILL.
WE BUY AND SELL
Field Seeds

*Ask for Prices
Mail Samples for Bids*

The S. W. Flower Co.
WHOLESALE
**FIELD SEED
MERCHANTS**
SPECIALTIES
RED CLOVER, TIMOTHY
ALSIKE
**TOLEDO
OHIO**

J. G. PEPPARD SEED COMPANY
Buyers **SEEDS** Sellers
Correspondence Invited Kansas City, Mo

The J. M. McCullough's Sons Co.
BUYERS — SELLERS
Field and Garden Seeds
CINCINNATI - - OHIO

The Mangelsdorf Seed Co.
Sweet Clover, Alfalfa,
Soudan Grass, Millet, Rape.
ATCHISON KANSAS

WHITNEY-ECKSTEIN SEED CO.
Wholesale Seed Merchants
BUFFALO, N. Y.
CORRESPONDENCE INVITED

HENRY HIRSCH
WHOLESALE FIELD SEEDS
CLOVER — ALSIKE — TIMOTHY — ALFALFA
Our Specialty
All Other Field Seeds
TOLEDO - - OHIO

The Toledo Field Seed Co.
Clover and Timothy Seed
Consignments solicited Send us your samples
TOLEDO, OHIO



CRAWFORDSVILLE SEED CO.
FIELD SEEDS
CRAWFORDSVILLE, INDIANA

**Missouri Grown Blue Grass
Kansas Grown Alfalfa, New Crop**
We are now prepared to accept orders for both
TOBIN SEED CO.
KANSAS CITY, MO.

**COURTEEN
SEED CO.** Milwaukee,
Wisconsin
Clover, Timothy, Grass Seed
Grain Bags

SEED

*We Buy
and Sell
all Varieties
of Grass
and Field
Seeds*

The Albert Dickinson Co.
MINNEAPOLIS CHICAGO

The value of an "ad" is not measured by what it costs, but by what it pays the advertiser.

A. E. REYNOLDS, President
T. C. CRABBS, Secretary-Treasurer

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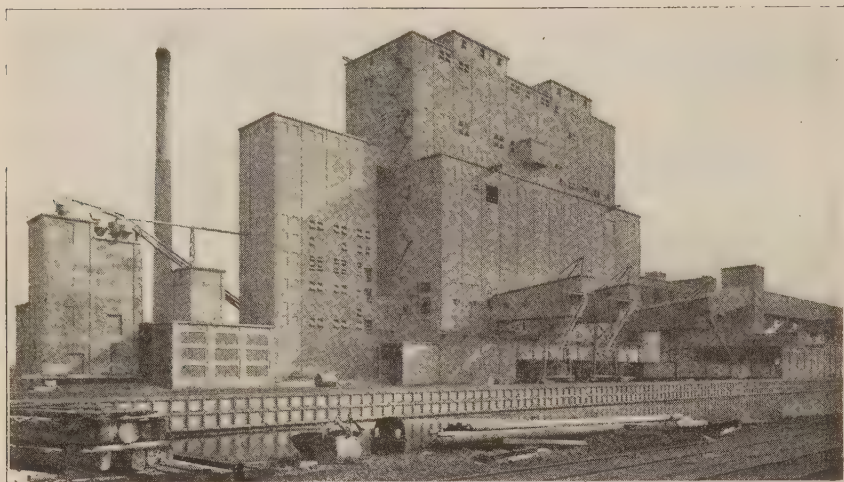
Crabbs Reynolds Taylor Co.
CRAWFORDSVILLE, IND.

**Country Shippers of Grain from Indiana's
Most Fertile Corn Belt**

TRACK BUYERS OF CAR LOT GRAIN

Specialists Dealing in Clover, Timothy
and allied Field Seeds—car lots or less

An old, well established firm, operating a chain
of elevators and seed cleaning plant, furnishing
a service which is at your command.



The Northern Central Elevator at Canton, Baltimore, Md., designed and erected by James Stewart & Co., is equipped with 19 Falk Herringbone Head Drive Gears for reducing speed of motors.

Our new Herringbone Gear Bulletin No. 31 is just off the press. Send for your copy today.

The Choice of the Leaders

Every elevator operator knows that there are two classes of elevator builders—those who build for profit, and those who build for reputation.

Falk Herringbone Gears are hobbled on our special machines from solid steel blanks cast in our own foundry.

The leaders, whose reputation depends on the economical operation of the elevators they design, cannot afford to specify any equipment but the best.

Falk Herringbone Head Drive Gears are the choice of these builders. The absolute dependability of Falk Gears has been proved by years of first-class service in some of the world's largest elevators.

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HERRINGBONE GEARS

GRAIN DEALERS JOURNAL

309 South LaSalle Street, Chicago, Ill., U. S. A.
Charles S. Clark, Manager

Published on the 10th and 25th of each month in the interests of better business methods and improved handling facilities for progressive wholesale dealers in grain and field seeds.

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THE ADVERTISING value of the Grain Dealers Journal as a medium for reaching progressive grain dealers and elevator men is unquestioned. The character and number of advertisements in each number tell of its worth. If you would be classed with the leading firms catering to the wholesale grain trade, place your announcements in the Journal.

Advertisements of meritorious grain elevator machinery and supplies and of responsible firms who seek to serve grain dealers are solicited. We will not knowingly permit our pages to be used by irresponsible firms for advertising a fake or a swindle.

LETTERS on subjects of interest to those engaged in the grain trade, news items, reports on crops, grain movement, new grain firms, new grain elevators, contemplated improvements, grain receipts, shipments, and cars leaving grain in transit, are always welcome. Let us hear from you.

QUERIES for grain trade information not found in the Journal are invited. Address "Asked-Answered" department. The service is free.

CHICAGO, OCTOBER 10, 1922

THE SHIPPER who studies closely all freight tariff schedules and gets thoroughly posted on rates and conditions will invariably find his time profitably spent.

TERMINAL market inspectors report an unusual number of losses in delayed shipments of grain traceable direct to leaking roofs. It is evident that many shippers in their eagerness to gain bin room have hastily coopered the sides and floors, but overlooked the roofs of cars offered. Water does not improve either the grade or the keeping quality of grain, so it is well to keep it out of your shipments when possible.

THE MOVEMENT of grain from all surplus sections is greatly handicapped by a scarcity of cars. The same complaint comes from Kansas, Nebraska, Iowa, Illinois and the Dakotas. Elevators are filled to the roofs yet shipments are light because carriers have not a supply of good cars for transporting grain. The trouble started in the days of government operation of the railroads because new box cars were not purchased. Private operation has been so crippled by a lack of money that the car shortage has not been remedied. The shopmen's strike did not improve matters and it now seems certain that grain cars will continue to be scarce for another year unless the movement of other freight slows down. Recently the railroads have been unable to handle much more than 70% of the freight offered, so all lines have suffered delay.

IF YOU believe in confiscatory taxation which fosters wasteful methods, strangles enterprise, discourages industry and stifles initiative, read what President Clement said at New Orleans about taxation, then pass the message on to your representatives in Congress.

SEED WHEAT, especially soft winter wheat of good quality, is in much demand in Southwestern districts which were afflicted with a prolonged drought. Dealers in position to supply seed wheat suitable for Texas and southern Oklahoma should get in touch with the shippers associations of those states.

BURSTING ELEVATORS in different sections of the country reflect a great need for more grain tight box cars as well as more careful elevator builders. Every house should be so built as to withstand a maximum stress, then when grain receipts are heavy and cars scarce, the elevator man can take in the grain offered without fear of bringing the house down on his head.

MORTGAGED grain and grain covered by lease for rent of ground on which it was grown seems to be making more trouble for country grain buyers than usual, so it behooves all to exercise great care and refuse to pay tenants for grain until they know it is free from all liens. The Iowa plan of paying for tenants' grain with a check made payable jointly to the landlord and tenant protects the buyer and leaves it up to the banker to decide who shall receive the money.

GROUND RENTALS for elevator sites on railroad right-of-way will never be reduced to fair figure in any state until the elevator operators join hands and fight the extortion now being practiced by the railroad companies. In some states bills are being discussed which will provide for an equitable valuation of the ground occupied and call for its leasing on the basis of 6% annual return. Any change so reasonable as that would be accepted without objection by the grain elevator operators.

A FEDERAL COMMISSION that has predetermined in its own mind that decline in the market price of wheat on the resumption of future trading was due to the rules and practices of the exchanges, and which already has expressed a desire not to hear anything about supply and demand as affecting the price of wheat, can be expected to hand-pick all facts elicited to bolster up its own preconceived theory. After the taking of voluminous testimony there still will remain the controlling fact that the price declined because there were more buyers than sellers, both nationally and internationally. One reason there were more sellers than buyers is that would-be purchasers refrained from buying because of a belief that prices were too high. In fact, the Commission will find the reasons almost as numerous as the number of individuals on the long or short side of the market. If the Senators hoped the Commission's report on the reasons for the ups and downs of the market would enable the Congressmen to evolve a system to "beat the market" they are doomed to disappointment.

SEPARATE DURUM wheat crop reports will add much to the interest in the spring wheat statistics put out by the government. Grain dealers of the Northwest will be pleased to have the U. S. Dept. of Agriculture segregate its durum wheat crop figures as proposed.

ONE INDIANA candidate for the United States Senate has made a part of his platform the abolition of the railroad labor board and the repeal of paternalistic laws strangling industry and business. It is hoped other candidates for office will follow the example of this champion of freedom in industry.

A CANDIDATE for vice pres. of the Minneapolis Chamber of Commerce ran for office on the platform "Fight the Capper-Tincher Law to the last ditch" and was elected, which indicates that the rank and file of the grain trade are beginning to see this is no time to lie down and be clubbed to death by bureaucrats catering to the bolshevik clement in our citizenship.

HAY GRADES now in use may not be satisfactory to everyone, nor will they ever be so. The U. S. Dept. of Agri. may have sufficient power to draw up grades for hay, but how will the rules be enforced? If their enforcement calls for the employment of another army of clerks, samplers and inspectors we know many business men and taxpayers will throw a fit.

EXCESSIVE TAXATION of future trades is driving scalpers from wheat to the grains of lower value. A change of a cent in the market value of ten cent corn nets the trader more profit than the same change would in two dollar wheat. The trading tax is levied on units of hundred dollar value. So low priced grain when an active market prevails draws many shrewd scalpers from the wheat pits and thereby reduces the activity in wheat to the disadvantage of producers and consumers.

THE NEW Capper-Tincher Grain Futures Act is to be contested in the courts by the grain exchanges and the entire trade wishes them every success. This law and the continual agitation kept up by Congress through investigations has so discouraged speculation as to affect a marked decrease in the demand for farm products. Men with daring and initiative will not continue in a line of business which is handicapped by uncertainty and misgivings as a result of threatened legislation and investigation.

VAST CAPITAL controlled by residents in foreign countries now is tied up in gold in United States banks and when stability of business and credit is assured abroad these credits will flow out and greatly stimulate our exports of wheat. The former banking reserves of Europe are now largely concentrated in our Federal Reserve Banks in an amount greatly in excess of the needs of domestic business. As soon as exporters of grain are given more adequate guaranties of payment by importers on the Continent they will tap our banking resources, without any fanciful \$300,000,000 government revolving fund.

Violators of Section 5 Will Be Expelled.

No organization of business men can afford to condone wrongdoing or compromise with the wrongdoers, and the recent action of the Grain Dealers National Ass'n in voting to expel any member found guilty by the Secretary of Agriculture with violating Section 5 of the Grain Standards Act is designed to place the grain trade on a plane where the word of members will pass without question.

Section 5 authorizes the Secretary of Agriculture to publish his findings whenever he determines that any quantity of grain has been incorrectly certified as to grade or has been sold, offered for sale, or consigned for sale, under any name, description or designation which is false or misleading.

The publication of firms for dishonest or deceitful practices should be enough to place the rest of the trade on guard and to give those suffering by the dishonesty easy help to a successful suit for damages. Reputable business men cannot afford to enter into contracts with other dealers who are accustomed to indulging in dishonest practices and what is more, they will not take the chance if warned.

If the Association continues to increase its requirements for continued membership, such membership will soon come to be a badge for square dealing, and membership will be more desirable and more eagerly sought by those willing to deal fairly with their brother members of the trade.

The Spirit of the New Orleans Convention.

While the attendance at the 26th annual meeting of the National Association was not up to the standard in numbers the addresses were masterly and throughout all of them came the warning against *too much government in business and not enough business in government*. The dominating note in nearly every address was a denouncement of bureaucratic interference with business. The same sentiment finds frequent expression in the reports of the committees and the sentiment of the convention was crystallized in the report of the Committee on Resolutions.

The deep interest of all the delegates in the sentiments expressed and their desire to pass the message on to others was exhibited by the motion to print the able address of President Clement and send it to every lawmaker of the land, in the hope that the protest sounded by the President against governmental interference with business and burdensome taxation would open the eyes of some lawmakers and thereby discourage other burdensome legislation.

The address of Senator Stanley reflected the same sentiment. It is fortunate indeed that the organized grain trade has come to a full understanding of the great handicaps which are oppressing business generally and it is sincerely to be hoped that every grain dealer will read carefully the full report, then pass the message on to other men interested in the Nation's welfare and the freedom of business generally.

No business man who has a clear understanding of the proper relations between business and government can read our report of the meeting without giving enthusiastic en-

dorsement to convictions of every grain dealer who addressed the convention. But if business men and citizens generally are to obtain any relief they must pass the message on to others and keep up a continual agitation for a repeal of many of the laws recently enacted, as well as demand relief from further bureaucratic legislation.

The long list of bills pending in Congress as reported at the New Orleans meeting would prove very discouraging to the average business man were it not for the fact that the next session of Congress is a short session and little opportunity will be obtained to enact the ridiculous laws proposed by the impractical socialists now holding seats at Washington.

The Bureaucrats Perversion.

Grain dealers who peruse Dr. Taylor's New Orleans defence of the county agents and the various activities of the Department of Agriculture will not be deceived. All of the bureaucrats at Washington are obsessed with the idea that any extension of governmental activity which makes for the employment of a horde of clerks, inspectors and detectives is sure to result in a very great service to the public. The one great difficulty with all the over-bearing autocrats from Washington is that they find such thorough enjoyment in the admiration of their own halo they are unable to credit the average business man with possessing either efficiency or honesty. They approach every line of business and every business institution with suspicion, gumshoes and impertinent questionnaires.

County agents throughout the land have persisted in using the taxpayers' money in an effort to tear down existing grain marketing machinery in the hope of establishing one of their own conception. They have shown no interest in the efficiency of established dealers, they have ignored entirely the advantages derived by farmers in encouraging grain dealers to establish up-to-date facilities for placing their grain in prime condition for marketing at a premium. If all the grain producers instead of trying to divert grain from men who have invested their capital and their time in providing modern facilities for storing, cleaning and handling grain would on the contrary enter into understandings among themselves to give or sell all their grain to the local elevator which is best equipped to handle and market their products they would promote their own interests far more, as well as promote the improvement of their local market.

The Bureau of Markets may not have given county agents specific instructions to induce all producers to go around the local elevator man in finding a market, but that has been the pronounced attitude of the county agents as is clearly evidenced by the following notice sent out by the county agent at Marshall, Minn., recently:

CO-OPERATIVE EXTENSION WORK IN AGRICULTURE AND HOME ECONOMICS
STATE OF MINNESOTA.

College of Agriculture, University of Minnesota.
U. S. Department of Agriculture.
Farm Bureau Association and
Lyon County Commissioners Co-operating.
Agricultural Extension Service
County Agent Work.
Marshall, Minnesota.
September 25, 1922.

Dear Sir:

On Wednesday evening, September 27, 1922, there will be a meeting in Bot's Hall in Green

Valley, for the purpose of considering plans for farmers to load their grain and ship it out in carload lots, independent of the elevators.

If you are interested in getting a better price for your grain by co-operation, come out to this meeting and help get the thing started. Hoping that everybody will be there and express their ideas in regard to this matter, I am,

Very truly yours,
(Signed): LOUIS S. KELEHAN,
County Agent.

The agitator has conducted a constant campaign against the detested middleman not because he ever expected to displace the middleman or intended to present any substitute which would effect the slightest economy in the marketing of grain. If the Department of Agriculture was really interested in the farmers' grain being marketed more efficiently and more economically it would apply itself to the education of existing middlemen in the hope of helping them to greater efficiency and a better understanding of marketing.

Every line of business always will have sluggards who drift; men who are ignorant, incompetent and indifferent. They never read, study or strive to improve either their facilities or their abilities. Their smug satisfaction always has and always will make for waste and inefficiency. But thanks to the ambition of the average grain dealer, the number of sluggards in the grain trade is small and ever decreasing.

One great weakness of the meddlesome methods of the Agricultural Department has been that the county agents and other inspectors and spies sent out by the department have used no judgment or discrimination whatever. They have encouraged farmers to engage in business in competition with well-equipped elevators that were managed by real experts, men who have learned how to handle the farmers' grain for a pittance and still make a good living. However, perpetual surveillance of the grain business by the meddling autocrats of the Department of Agriculture does not encourage men to improve their facilities or their abilities to market grain. On the contrary, it has a tendency to drive many self-respecting grain merchants of long experience out of business.

The various activities of the Department of Agriculture in administering the Grain Futures Act, the Packers and Stockyards Act, and the Capper-Volsted Act, as well as the many other meddlesome activities of the department have not increased the efficiency of the lines of business supervised, neither have they protected or promoted the interests of either the producers or consumers. The only thing these activities have accomplished is to increase the cost of doing business, increase the great army of feeders at the public crib and multiply our taxes indefinitely. A much larger percentage of the business men are honest than the inspectors and detectives of the Department of Agriculture are willing to admit, and the percentage of dishonest men is not large enough to justify the employment of 1% of the present army of meddlers.

The enactment of laws requiring dealers to mark plainly goods sold and providing a heavy penalty for misbranding and misrepresentation would if enforced, relieve a vast army of government employees for some useful work. Every increase in the number of bureaucrats attached to the payroll of our civil service has come as a result of internal

scheming by a politician identified with the departments. Not one increase can be traced back to a demand or even a request on the part of the dear people whose interests the bureaucrats claim to be promoting.

Getting the Correct Freight Rate.

Grain shippers cannot depend upon railroad men for correct freight rates. Too often soliciting freight agents have quoted rates lower than the schedule rate in order to get the business and with the full knowledge that the shipper would be charged the full schedule rate at destination. No railroad official has authority to grant a rate lower than the schedule rate.

It has been hoped that the railway system of freight tariffs would be so simplified that every shipper could easily learn for himself the correct rate of freight between any points. But that day has not yet arrived. Hence, every shipper must run the chance of being charged more than he expects when he loads grain into cars for routing to a new point.

The safest way now open to the average shipper is to write the general freight agent as well as the Interstate Commerce Commission for the rate wanted. While these authorities may often disagree their decisions will at least assist the shipper in learning the schedule rate.

The Arbitration Record.

While the various arbitration committees of the Grain Dealers National Ass'n rendered but 49 decisions during the past year of association work it became necessary for the organization to expel eighteen members for refusing to arbitrate or refusing to pay arbitration awards. Sixteen of the 49 decisions were appealed during the year.

It is indeed unfortunate that it should become necessary for the association to expel any grain dealer for refusing to make good on his contracts. While business conditions have greatly weakened the financial standing of many firms it cannot be presumed that all of the eighteen were so badly crippled that they were not able to make good on their contracts. When members of the National Ass'n are able to carry out both the spirit and the letter of their contracts as interpreted by arbitration committees the natural presumption is that they are perfectly willing to do so. That the last year has brought distress and disaster to many firms which would have made good on their contracts had they been able to do so is clearly indicated by the association's membership record.

During the year the organization lost 471 members. In addition to the eighteen expelled, 170 retired from business, 193 resigned and 90 were dropped for non-payment of dues. The association's arbitration record as well as its membership record is but a reflection of bad business conditions and methods which are rapidly passing away. Many firms were forced to default on their contracts because other firms had already defaulted on contracts with them. The general business conditions are slowly improving, and no doubt will enable the association to report quite different results on both membership and arbitration at the next annual meeting.

Damages for Telegraph Blunders.

A decision rendered by the Interstate Commerce Commission last year bearing on the transmission of telegraph messages increased the liability of the telegraph companies to \$500 for errors made in unrepeatable messages and to \$5,000 for errors made in repeated messages. This decision did not cause the telegraph operators to exercise greater care in transmitting messages, but spurred on the legal departments of the telegraph companies to devise new excuses for denying liability for errors and immediately they incorporated in the fine print on the back of their message blanks a statement to the effect that, they would not be responsible for errors in obscure messages or messages sent in cipher.

Inasmuch as most of the grain dealers use cipher codes in transmitting messages so as to reduce the opportunity for error and insure privacy in their business transactions, the new clause on the back of the blanks fell with unusual force upon the grain dealers of the land. It was not the desire or the intent of the Interstate Commerce Commission to except from its regulations messages sent in cipher. It seems very likely that the commission will rule out the exemptions placed on the back of the blanks by the telegraph companies.

Accordingly Mr. Goemann, Chairman of the National Association's Transportation Committee, has called a conference of many different trade organizations in Chicago for October 17th to draft a request to the Interstate Commerce Commission to require telegraph companies to assume the liabilities fixed by the Commission on all "code messages written in English dictionary words." While such specific action on the part of the Interstate Commerce Commission would be right in line with its original ruling it would not necessarily inflict any great hardships on the telegraph companies, because they can improve the efficiency of their operators without much expense and prevent errors being made.

The present practice of the telegraph companies is ridiculous. If an English word happens to be used by the sender of a message to mean something other than it is interpreted to mean in the English dictionary then the telegraph company is not liable for errors in its spelling. For that matter there seems no good grounds for permitting the telegraph companies to digress from the copy given them by the sender of the message. The telegraph operators' sending units are the letters of the alphabet and the digits and if he makes an error in sending a word whether it be a dictionary word or not, it is his error.

While the telegraph company might be entitled to a slightly increased compensation for the sending of messages in non-dictionary words the liability should be just the same. Users of telegraphic service can never expect to have their messages correctly transmitted until the telegraph companies are required to follow copy or else pay for their blunders. The measure of liability fixed by the Interstate Commerce Commission is so reasonable that none of the telegraph companies have protested against it or attacked it in the courts or before the Commission.

The senders of telegraph messages need cipher codes in order to reduce the high tolls assessed by the telegraph companies for the

service rendered, and it is not fair either to the business of the telegraph company or of their patrons that the users of any class of cipher words should be denied the same care in transmitting messages as is given any other patron.

No doubt the sending of messages in non-dictionary words has a tendency to slow up the service and users of such words would no doubt recognize the fairness of a slight increase in toll for transmitting such messages. But for transmitting ordinary dictionary words there should be no extra charge even though those words happen to appear in some cipher code book.

Grain Exchanges Will Fight Bureaucratic Capper-Tincher Law.

Chicago, Oct. 10.—Directors of the Chicago Board of Trade tonight voted to test the constitutionality of the Grain Futures Act which placed control of the grain exchanges in the hands of the Secretary of Agriculture.

The new law, recently enacted by Congress to become effective Nov. 1, was intended to replace the former law declared unconstitutional by the supreme court. Action against the previous law was not taken by the Board of Trade, but by individual members. In the supreme court's decision in the former act, Chief Justice Taft stated that the suit should have been filed by the board itself, and such a course is being followed in the present case.

The attorney of the Board of Trade, Henry S. Robbins, has given an opinion that the new act is unconstitutional upon the same grounds on which he had obtained a decision of the supreme court holding the former Capper-Tincher Act unconstitutional.

Robert McDougal, president of the Board of Trade, in announcing the decision of the directors, stated that the action was taken "not only in the interest of the grain trade and of the farmers who are being misled as to their own best interests, but as a patriotic duty to combat the drift of the government toward bureaucracy." An additional reason, he said, is that so much doubt exists as to the validity of the new act it is deemed best that in the interests of all concerned the validity of the act be determined once for all.

The legislative committee of all the grain exchanges at a meeting yesterday concurred in the decision to test the constitutionality of the act.

The action of the Chicago Board of Trade comes as a result of serious consideration of the effects of the recent legislation known as the Grain Futures Act. All of the grain exchanges of the country which afford facilities for future trading are of the opinion that the constitutionality of the Grain Futures Act must be determined at the earliest possible date in order to dispel the doubt and uncertainty hanging over the markets at present which has a decided tendency to restrict the volume of trading to the detriment of both producer and consumer.

The congressional attack upon grain exchanges started in January of 1921 after the general price deflation which carried grain and all other commodity prices to low levels.

Leaders of the grain trade have declared repeatedly that constant threats of legislation have destroyed the speculative demand and so narrowed the market at the heavy crop-moving and hedging season that the farmer has suffered.

The Grain Dealers National Ass'n, the largest organization of its kind, recently went on record in favor of repeal of the "insidious legislation" or a test of its constitutionality.

A number of large industries will watch with keenest interest the outcome of the present legal battle because of laws now in effect or likely to be passed which provide further governmental supervision of various lines of business.

Asked— Answered

[Readers who fall to find information desired on any subject of interest to grain dealers should send us their query for free publication here. The experience of your brother dealers is worth consulting. Replies to queries are solicited.]

How Long Must Shipper Guarantee Grade?

Grain Dealers Journal: We shipped a car of corn to Omaha on consignment and the car was inspected on arrival, grading No. 3 yellow and was sold for such. Five days later the car was set out at the terminal elevator which called for a re-inspection. It then graded sample heating, with a discount of two cents per bushel. How long is the shipper responsible for the grade of his grain after inspection and sale, according to the Omaha Grain Exchange rules? How long is shipper responsible at other terminal markets?—M. H. Kocinsky.

Relief from Poor Switching Service?

Grain Dealers Journal: Is there anyone to whom we can appeal for relief from the railway company for poor switching service of loaded cars to our siding or moving loaded cars from our siding? We have had loaded cars in the yards three or four days before they are set at our siding for unloading and we have also had loaded cars on our tracks two or three days after they are billed out before they are moved.—Ben B. Hunt, Hunt Bros. Milling Co., Pleasant Hill, Mo.

Ans: Local agents and switching crews are sometimes without power or authority to improve the service, and the attention of officials higher up should be called to the existing conditions. The division superintendent of the railroad company may be able to remedy the poor switching service, if informed.

If by reason of delayed switching the shipper or receiver of freight suffers loss by grain going out of condition or decline in market price the railroad company is liable for damages that are collectible by suit.

Rate on Millfeed Lower Than on Bran?

Grain Dealers Journal: We understand certain railroads have put in a tariff making a rate on millfeed from 2 to 4c per hundred less than the rate on bran.

Will the Journal advise us as to just what is understood as millfeed? Is there any reason why wheat bran is not millfeed just the same as wheat shorts or wheat millrun or mixed shorts, bran or millrun? Is there any reason why either of the three standard wheat feeds, such as bran, shorts or millrun, should not be called millfeed either singly or in combination?

Our reason for writing this letter is to try to get information to enlighten us so we will not violate a law and be subject to a fine. If we want to ship a car of wheat bran and by billing it as millfeed it can be carried 4c per cwt. cheaper, we want to do so if it can be done without violating a law. Any information will be appreciated.—Strong Trading Co., Wichita, Kan.

Ans: The rates taken by bran, millstuff, shorts, shipstuffs, etc., are given under Rule 235-M in Western Trunk Line Circ. 1-P, Supplement No. 35, effective Nov. 1, 1922.

Altho bran is a product of the mill and comes under the broad classification of a mill feed, the railroad companies have made it specific, and bran means no mixture, and as millfeed is understood to be a mixture, bran can not be billed as millfeed.

The millers do not bill bran as millfeed.

St. Louis, Mo.—We are believers in higher prices for all grain during the spring months. Europe needs our surplus and will find some way to finance its purchases.—J. B. Taylor Grain Co.

Federal Hearings on Speculation and Manipulation.

Complying with a resolution adopted by the Senate the Federal Trade Commission has begun an investigation into the fluctuations of cash and future grain prices since July 15, 1920.

The first hearings were held at New York Oct. 6 and 7, followed by several sessions at Chicago beginning Oct. 10.

At New York J. Leonard Replogle was asked what led him to buy wheat when, as he said, he knew no more about wheat than a hog knows about Sunday. J. Berlovitz, dealer in hides and wool at Lafayette, Ind., had told him wheat was a good buy. At Chicago Oct. 10 Berlovitz was the first witness called and stated how he happened to suggest the purchase of wheat, his travels abroad having led him to believe there would be a big demand. The deal resulted in a profit of \$100,000 and a subsequent loss of about the same to Mr. Replogle.

Julius H. Barnes told the history of the May, 1921, price movement. Jesse Livermore denied that there was manipulation of the wheat market.

Among those attending the Chicago hearing were John R. Mauff, J. J. Fones, L. F. Gates, Lowell Hoit, John Hill, Jr., and Samuel Mincer. Before they testified, an attorney representing the grain men informed the commissioners that he desired to have it in the record that the Commission had no authority to compel witnesses to testify in this inquiry, that their statements were voluntary.

The attorney made the point that a sale of grain on the Chicago Board of Trade was a local and not an interstate transaction.

In opening the Chicago hearing the Commissioners stated they did not want to hear so much about supply and demand; but more about the rules of the Board of Trade, particularly the rule providing for emergency delivery of grain in cars.

Conference on Proposed Tariffs.

At Chicago, Ill., Oct. 16, in the Transportation department of the Board of Trade, will be held a conference to discuss the proposed tariffs of the carriers covering loss and damage of grain in transit under Docket 9009.

Henry L. Goemann, chairman of the Transportation Com'te of the G. D. N. A., invites all traffic managers and others interested to attend.

Conference on Telegraph Liability.

Exchanges in all markets are invited to send a delegate to a meeting to be held in Chicago, Ill., Oct. 17, in the com'te room of the Board of Trade on telegraph liability.

Henry L. Goemann, of the G. D. N. A. Transportation Com'te, states that the conference is to be held for the purpose of complaining to the Interstate Commerce Commission regarding the limitation of liability of telegraph companies in that it does not cover code messages. The Commission will be asked to amend the rules so that the telegraph companies will be held liable for errors in transmitting code messages made up of words in the English dictionary. An effort will also be made for a reduction in charges and a revision of the rules on long distance telephone messages.

THE WHEAT crop of France is estimated at 235,380,000 bus. according to the U. S. Dept. of Agriculture, or a decrease of 88,090,000 bus. from 1921. Germany's wheat yield is estimated at 69,670,000 bus. or a reduction of 38,130,000 bus. from last year. France's rye crop is estimated at 37,600,000 bus. compared with 44,392,000 last year; barley, 39,540,000 bus.; compared with 244,455,000 bus. last year. Germany's rye crop is estimated at 210,580,000 bus.; compared with 267,648,000 last year; oats 284,600,000 bus.; compared with 344,812,000 bus. last year.

Suit Against Opening of State Protein Laboratories in Kansas.

Henry Reimer, a farmer of Wyandotte county, Kan., has filed suit in the district court to restrain the state grain inspectors from opening protein laboratories in Kansas City, Hutchinson and Wichita at a cost of \$10,000.

He claims that the expense will result in higher fees for inspecting and weighing grain in Kansas and that the laboratories will not be self-sustaining on a charge of 50 cents per test, as private laboratories charge 75 cents.

Hearing on Natural Shrinkage of Ex-Lake Export Grain.

A hearing will be held at Washington Oct. 23 by the Interstate Commerce Commission on Curlett's Supplement 20 to I. C. C. A. 25 and Supplement 24 to I. C. C. A15, which has been suspended until Jan. 23 in response to complaint by the Chicago Board of Trade and others. The change in Rule 7 contemplates the removal of the words, "unless shortage is occasioned by accident or defective cars," which if done would allow carrier to subtract from all claims, 1/8 of one per cent for shrinkage, even tho car was lost thru accident. The rule applies to ex-lake export grain only.

The proposed rule is:

"The carriers named as initial lines in this tariff will only be responsible for quantity of wheat, corn, rye, barley or oats (received for transportation under this tariff) and delivered to elevators located at the railroad terminals at the seaboard, when the shortage is in excess of one-eighth of one per cent of the contract lot. When in excess of one-eighth of one per cent, the carriers will only assume the shortage over and above one-eighth of one per cent of the contract lot, unless shortage is occasioned by accident or defective cars."

"The above applies on grain in merchantable condition. When not in merchantable condition, the carriers will not be responsible for any shortage unless occasioned by accident or defective cars."

Dealers in Attendance at New Orleans.

[Continued from page 196.]

Sager, J. H. Dole & Co.; L. T. Sayre, J. Rosenbaum Grain Co.; and Adolph Kempner.

Illinois shippers included L. A. Tripp, Assumption; J. P. Wilson, Beardstown; Frank Ware, Butler; H. I. Baldwin and H. J. Kapp of H. I. Baldwin & Co., Decatur; E. M. Wayne, Delavan; A. H. Hillmer, Freeport; F. G. Horner, Lawrenceville; B. B. Bishops, Sheldon, and C. E. Graves, Weston.

From Nebraska came Frank H. Brown, J. A. Linderholm of Crowell Elevator Co., J. R. Schmitt, C. D. Sturtevant and C. H. Wright, all of Omaha; O. F. Frisbie and D. W. Killeen, Schuyler.

Iowa was represented by J. R. Murrell, S. W. Wilder and C. A. Davis, all of Cedar Rapids; Geo. Moulton, Fondra; Frank Todd, Merrill; M. King, Paul Larson, S. P. Mason and H. P. Trusler, all of Sioux City.

From Indiana came E. Hutchinson, Arlington; A. E. Reynolds, Crabbs, Reynolds, Taylor Co., Crawfordsville; H. Thompson, Kokomo; O. N. Thomas, Marion; H. W. Reimann, Shelbyville; J. Vining Taylor, Winchester; and C. T. Martin, Wolcott.

Ohio was represented by F. J. Beasley, Athens; J. H. Motz, Brice; E. C. Eikenberry, Camden; P. C. Miller, Celina; F. E. Watkins, Cleveland; E. O. Teegarden, Duvall; R. Raabe, pres. National Hay Ass'n, Ft. Jennings; O. P. Hall of E. A. Grubbs Grain Co., Greenville; H. L. Goemann, Mansfield; and A. L. Elliott, Toledo.

From Texas came T. E. Connolly, Clarendon; C. M. Carter, G. E. Cranz, C. S. Dorsey, E. M. Rogers, B. K. Smith, O. S. Tillery, all of Fort Worth; J. S. Fordtran, Julius W. Jockusch, J. F. Ryder, of Galveston; W. R. Archer, O. Cranston, D. J. Green and J. E. Bishop of Houston; E. W. Crouch, McGregor; C. P. Dorsey, Rockwell; and B. E. Clement, Waco.

Missouri was represented by W. B. Lincoln, C. K. Davis, C. L. Fontaine, F. B. Godfrey, Simonds-Shields-Lonsdale Grain Co.; E. M. Hibbs of E. C. Christopher Co., Fred Hoose, F. R. Warrick, S. L. Weekes and C. M. Woodward, all of Kansas City. From St. Louis came J. O. Ballard, A. H. Beardsley, of Picker & Beardsley Com. Co.; J. P. Berger, T. A. Bryant, John Dower, W. J. Edwards, John H. Herron, F. M. McClelland, A. L. McDonald, A. E. Mann, Herman Miller, D. S. Mullally of Langenberg Bros. Grain Co., Philip Rothrock, L. F. Schultz and C. A. Wilson. From St. Joseph came W. W. Simmons, of Button-Simmons Grain Co.; from Sikeston, J. S. Kevill; and W. K. Hunter came from Versailles.

Ohio Marketing Scheme Blows Up

By TRAVELER.

The Ohio agitators who had their hearts set on getting control of the Union Elevator at Cleveland have given up the fight and abandoned their attempts to organize a \$300,000 grain marketing corporation. Like the U. S. G. G., Inc., the Union Co-operative Elevator Co., sinks under the burden of a great mass of liabilities.

The plan of the original promoters seems to have been to unload the old, out-of-date Union Elevator at Cleveland onto the farmers for \$175,000. The Union Co-operative Elevator Co., was organized and the Ohio Farm Bureau Federation thru its bulletins and *News* worked earnestly to unload the stock on the farmers and the co-operative elevators of the state as is evidenced by the following letter:

A Cheerful Appeal.

Columbus, Ohio, June 14, 1922.

Dear Folks:

We are up against it and are willing to admit it. Our Grain Marketing plans are not progressing as we had hoped they would by this time, and we want your personal opinion as to what we should do.

There has come into the office in actual cash and notes \$26,080, and we know of subscriptions that are ready to be paid that will make the total much more. But we are a long way from \$75,000, and here it is June 15th. Further, if only \$75,000 is forthcoming we here at headquarters are absolutely against going ahead with the Co-operative Union Elevator. We are not going to be a party to another start inadequately financed, and unless the major part of the \$200,000 is immediately forthcoming, we had better not attempt to start. We have done our best to get the information to you folks. We are convinced that the plan can be made one of the most profitable marketing enterprises that has yet been undertaken. Experienced grain men say it is possible, but we just can't seem to get you leaders back of it. Folks who have been right with us on other plans are now backing up in the breeching.

What's the matter? Don't you believe in it? Don't you understand the plan? Haven't you faith that we can make it go after we have it financed? Can't you look ahead and see the tremendous benefit it will be, or just what is the trouble?

If you leaders and farmers don't want it, believe us, we have no disposition to try to force it down your throats. We thought it was a necessary piece of marketing machinery to try to get the producers of grain a fair price for their products and to get together to purchase and manufacture our feeds. Think! only one share to an elevator and one share to a township would pretty nearly do it, and mark my words—you'll be driven into some such plan before long. If this plan goes down, it will not only put us back five years, but will be a blow to the whole co-operative movement and to every co-operative elevator.

You say no money. Other stocks are selling, and did you ever borrow to buy feed, to buy implements or to plant a crop? Won't you borrow to help get a better price for your products?

It's up to you to tell us what to do. We can only carry out your wishes. We enclose a return envelope and suggest that you answer right now on the back of this letter. Don't lay it down until you have answered. WE WANT YOUR HELP AS WE HAVE NEVER WANTED IT BEFORE. Won't you write us?

Very truly yours,

M. D. LINCOLN,

Executive Secretary.

According to Mr. Lincoln the Federation had collected only \$26,080 on June 14, but the other promoters had unloaded a lot of stock. However the Union Co-operative Elevtr. Co., never exercised its option on the Union Elevator at Cleveland and a Creditors' Committee has been appointed to wind up its affairs and if possible raise enough money to pay off its debts.

The Union Co-operative Elevator Co. is said to owe the Fostoria Milling Co., about \$17,000 and nearly every co-operative grain shipping ass'n which had any business dealings with the Cleveland organization is said to have an unsatisfied balance on its books of \$50 to \$500.

It also develops that out of the total stock subscribed to the Union Co-operative Elevator Co., when they first started, something like \$18,000 was not paid in. Stock was subscribed by the local Co-operative elevators at from \$500 to \$1,000 per elevator. The Creditors Committee is now endeavoring to collect on this stock subscription with a view to getting in the money to pay off the indebtedness. Naturally the elevators are loath to pay these subscriptions in view of the fact that they know they can get nothing back, as the indebtedness will more than use up all of these subscriptions. The reports the Creditors Committee have received from the various elevators owing for stock are to the effect that as they are already badly in debt, to be obliged to pay for this stock would probably put them into bankruptcy so the chances are not very favorable for the Union Co-operative institution ever to pay out its indebtedness.

The Farm Bureau has given up entirely the idea of being able to refinance the Union, but the Farm Bureau has taken over the Sales Service Corporation, or whatever the Company was called that was formed by Robinson, Latchaw and others at Columbus some two years ago. This institution had also gone in debt. The Farm Bureau of Ohio under direction of M. B. Lincoln is endeavoring to perform the functions of this Sales Service Corporation and is buying fertilizer, twine, and various supplies, for the farmers elevators and also endeavoring to sell grain on a brokerage basis.

Harry W. Robinson, who was president and general manager of the Union Co-operative Elevator Co. also retained his position of Vice-President of the Union Elevator Co. owner of the plant used by the Union Co-Ops. and which belongs to the Teagle Estate. The Teagles are big people in the Standard Oil, Walter D. Teagle having been elected some years ago to fill the position of Vice-President of the Standard Oil after the death of H. H. Rodgers. It also develops that the Union Elevator Co. is again running as an independent concern under the management of Harry W. Robinson.

Mr. Schlosser who was a director and heavy stockholder in the Fostoria Mlg. Co. took the management of the plant last winter when Fred Smith was let out. He is now together with the other directors who are holding the bag for a big deficit, forming a new organization and will probably take over the plant and operate it as an independent concern, for the stockholders, that is, the farmer stockholders, are unwilling to put any more money into it and as a result, the co-operative end of the business has ceased to exist.

Orators and promoters, such as Robinson and Latchaw, who posing as friends of the farmers succeeded in pulling the wool over their eyes for so long, have lost all of their prestige with the farmers and it would be extremely difficult for them or any one else to start any new farmers organizations in Ohio.

Among farmers organizations to attempt to get out of business recently is the Gibsonburg Farmers Elevator.

CORN, beans and rice will be permitted to be imported free of duty for four months from July 24, 1922, into Honduras, due to failure of crops this year.

EXPORTS of grain from the United States for the week ending Sept. 28, were, wheat, 11,897,118 bus. compared with 7,455,754 last year; corn, 1,733,122 bus. compared with 2,997,393 last year.

Hearings on Proposed Hay Grades.

A hearing will be held in Washington, D. C., Oct. 12, on the proposed hay grades by the Department of Agriculture.

At a hearing held Sept. 27 in Chicago, the following hay dealers were present and offered suggestions and criticisms: R. L. Bishop, Almont, Mich.; John H. Devlin, Chicago; Geo. S. Bridge, Chicago; H. S. Button, Chicago; H. M. Mead, Thorp, Wis.; T. J. Hubbard, Lansing, Mich.; F. L. Young, Lansing, Mich.; S. W. Kemp, Spencerville, Ohio; G. R. Kell, Fort Wayne, Ind.; Rudolph Raabe, Fort Jennings, Ohio; and Maurice Niezer, Fort Wayne, Ind. J. Vining Taylor, Winchester, Ind., sec'y National Hay Ass'n, and C. R. Megee, Michigan Agricultural College, were also present.

Those in charge of the proceedings were Dr. W. A. Wheeler, W. H. Tenny, Ass't Chief, Bureau of Agricultural Economics and K. B. Seeds of the same bureau. H. B. McClure, specialist in standardization of hay, was also present.

Mr. McClure explained how the grades had been made, showing the errors made by inspectors in grading hay when checked up by analysis of the hay, and explained the character of the analysis to which the hay had been put.

Rudolph Raabe, Fort Jennings, O., objected to the inclusion of mammoth clover in clover mixed, and stated that in his opinion such a move would be a great error.

Maurice Niezer, Fort Wayne, Ind., spoke his approval of the proposed grades but denounced the practice of the county agent in assisting in the marketing of hay. The practice, he said, is out of line with their work of instructing in the raising of hay.

Leaking in Transit

Grain dealers can help brother sufferers in the collection of claims for loss by reporting to Grain Dealers Journal, for free publication, car initials, number, place, date and condition of car seen leaking grain in transit.

Recently we have received reports of the following leaking or bad order cars:

C. B. & Q. 117403 was leaking wheat at Holdrege, Neb., from doorpost, Oct. 3.—Holdrege Equity Exchange.

D. & S. L. 53058 was leaking at doorpost at Holdrege, Neb., Sept. 30.—Holdrege Equity Exchange.

G. N. 126377 was leaking Sept. 29 in walls near door post at Big Sandy, Mont.—A. A. Delger, Montana Elevator Co.

Soo Line 108660 passed thru Bordulac, N. D., Sept. 26, leaking wheat at the door post. Train did not stop and I noticed it while going at a speed of 30 miles an hour.—O. R. Beckley.

T. & P. 7633 was seen traveling northeast on the C. G. W. leaking freely at the door post.—A. V. Tischer, chief inspector Des Moines Board of Trade, Des Moines, Ia.

SSW 28085 was leaking badly at bottom of the door while being switched in yards at Kingsville, Tex.—M. Nuckols, Kingsville Ice & Mfg. Co.

C. & N. W. 87498 was set out at Havelock, Iowa, Sept. 21. Side of car was broken and brace post broken. Side of car was leaking mixed corn. Section men repaired leak and car went on.—J. L. Miller, mgr. Farmers Coal & Grain Co.

N. Y. C. 249984 passed thru Hendley, Neb., Sept. 20, east bound, leaking wheat at door post.—Farmers Business Ass'n, J. W. Query, mgr.

A. C. L. 35118 was leaking very badly at Arrowsmith, Ill., Sept. 19. Train crew repaired leak at side of car.—J. Burt Porterfield.

C. C. C. & St. L. 46631 passed thru Saint Paris, Ohio, Sept. 18, going east and leaking wheat around door.—E. S. Allton, mgr. Saint Paris Grain Co.

Letters

[Here is the grain dealers' forum for the discussion of grain trade problems, practices and needed reforms. When you have anything to say of interest to members of the grain trade, send it to the Journal for publication. It may draw out the views of others.]

Advises Against Use of Paradichlorobenzene.

Grain Dealers Journal: The Bureau of Chemistry and Entomology are cooperating in an investigation to find a fumigant more suitable for grain car fumigation than carbon disulphid. After a summer's work nothing has been found that is better than carbon disulphid for killing weevils in carload lots of grain. Several fumigants which gave promise of effectiveness when used in small containers with small lots of grain, failed to prove practical when used in the fumigation of carload lots. This investigation is being continued with the hope that ultimately a more suitable fumigant will be found. The investigation is in the right direction as all grain men will agree.

You ask particularly about paradichlorobenzene for grain fumigation use. On a small scale paradichlorobenzene gave promise some years ago of being a useful fumigant and is now being recommended for certain types of fumigation work. This Bureau, however, has not found it practical on a large scale for fumigating grain in bulk and has not been recommending its use for some years. I considered it very misleading for such an influential grain paper to advocate without qualifications the use of paradichlorobenzene and chloropicrin. No one is today using chloropicrin and very few attempt the use of paradichlorobenzene. Fumigation with carbon disulphid or carbon tetrachloride are, however, practical for use in grain, grain cars and grain bins when the person using them thoroughly understands their application. Of these two gases, carbon disulphid is by far the best, if lanterns, lighted cigars and fire in other forms can be kept away during the period of fumigation. While carbon tetrachloride fumigation does away with the fire hazard that accompanies the use of carbon disulphid fumigation, it is much inferior in killing power and from two to four times as much liquid must be used as in the case of carbon disulphid. Very truly, E. A. Back, Entomologist in Charge of Stored Product Insect Investigations, Bureau of Entomology, Dept. of Agri., Washington, D. C.

Complaints of Grain Inspection at Chicago.

Grain Dealers Journal: Several complaints have come to this office during the last two weeks in regard to the inspection and grading of grain at Chicago.

In one instance a shipper called for appeals on ten cars on nine of which the appeals for the higher grades were granted. In another case four cars shipped from a country station in which appeals were called for and the higher grades granted were as follows:

One car inspected No. 6, changed on appeal to No. 2 mixed.

One car inspected No. 6 white, changed on appeal to No. 3 white.

One car inspected No. 6 yellow, changed on appeal to No. 3 yellow.

One car inspected No. 5 yellow, changed on appeal to No. 4 yellow.

Aside from the above specific cases we are advised by several other shippers that they have had similar experiences.

Shippers should thoroughly understand the situation. The inspection of grain at Chicago is officially done by the State Grain Inspection Department under Federal Supervision, which

is in fact the United States Department of Agriculture. The office of the Federal Grain Supervisor of that district is located at Chicago.

Shippers should understand the difference between a re-inspection and an appeal. Re-inspection is simply a review of the original sample by the State Grain Inspection Department and perhaps by the same inspector who made the original inspection. An appeal is practically a re-sampling and an inspection of the new sample by the Federal Grain Supervisor.

Demurrage accrues after twenty-four hours on cars placed on the inspection tracks. If the call for an appeal is made early during the day of arrival it is possible to avoid expense of demurrage and reconsignment. The charge for the appeal is \$2.00. If, however, the car is held on the inspection tracks longer than twenty-four hours the demurrage and reconsignment charge would accrue making the expense from \$4.50 to \$7.00 per car. If the grain is sold to arrive the cars may be ordered to the elevator within the twenty-four hours and if an appeal is called for, the sample may be taken while in transit or after being placed at the elevator.

Country shippers should carefully inspect the grain as it is loaded into the car and make a record of the grading according to the inspection of it. If for example, corn when loaded is No. 1, No. 2, or No. 3 grain in quality the commission merchant or the terminal buyer should be advised that it is of that quality and instructed that if inspected at Chicago as a lower grade to call for an appeal, if the difference in the value between the grading at the shipping point and the grading as originally inspected at Chicago would exceed the probable cost of the appeal.

I am advised by the Federal Supervisors of Grain Inspection as in the different districts that if shippers would take advantage of the privilege to call for appeals to the Federal Supervisor of Grain Inspection as outlined above that it would give them an opportunity to check the State Inspectors and thereby strengthen the functioning of the Federal Supervision of Grain Inspection.—Very truly, George A. Wells, Sec'y., Des Moines, Ia.

THE SEMI-ANNUAL meeting of directors and delegates of the Millers' National Federation will be held in the Ballroom of the Blackstone Hotel, Chicago, Ill., Oct. 19 and 20.

Grain Movement

Reports on the movement of grain from farm to country elevator and movement from interior points are always welcome.

Rock Valley, Ia., Sept. 26.—Cars are hard to get.—Farmers Elevator Co.

Sheldon, Ia., Oct. 7.—We have shut down while awaiting cars. We took in 35,000 bus. in the last two weeks and have been lucky enough to get seven cars and now the chief dispatcher has shut down on us, stating we have been getting too many. We shipped 31 cars since July 31.—Evander Farmers Co-op. Co.

Burdett, Kan., Sept. 30.—Business here is tied up on account of no cars. About 75% of wheat is yet to move.—T. H. Urton, mgr., Bauer-Vaughan Grain Co.

Hutchinson, Kan., Oct. 7.—The movement of wheat is slow on account of the scarcity of cars, while farmers are generally too busy to haul their grain even if they were disposed to sell it at present prices. The slight improvement in prices in the past week or so makes the growers even more determined to hold their stocks. Movement of kafir and milo will soon be under way.—Cal.

Stillwater, Minn., Sept. 25.—Car condition here has been good so far, with cars in average condition.—C. M. Dickinson.

Douglas, Minn., Oct. 5.—Movement of grain is very slow. Weather is good but farmers are too busy with fall farm work.—Louis Hanson & Co.

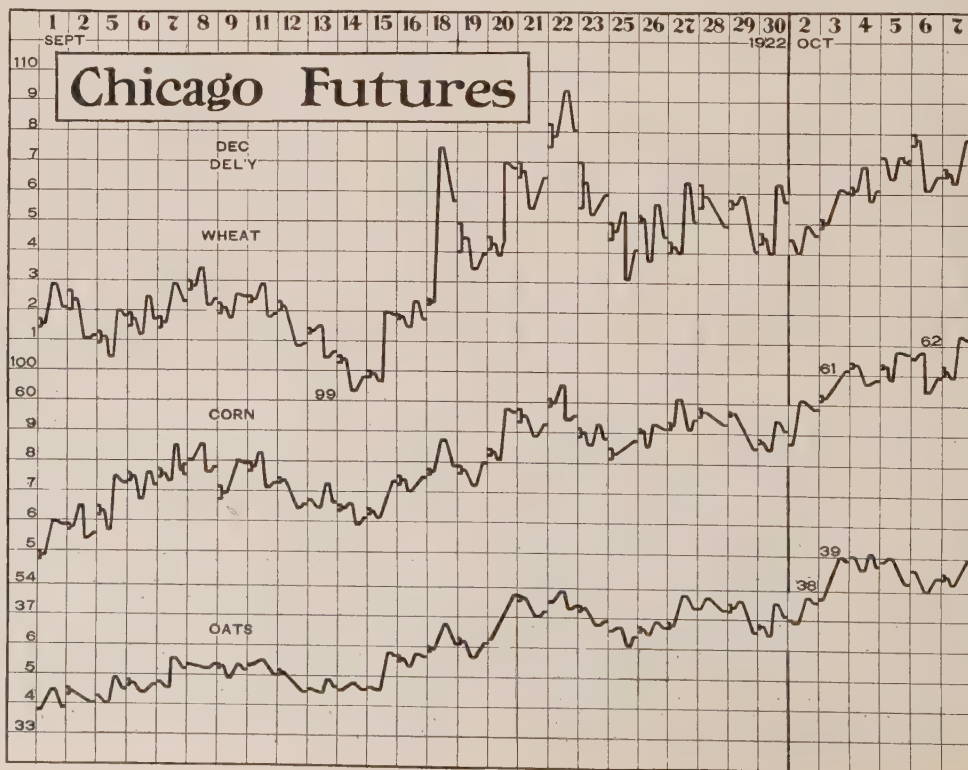
Lynd, Minn., Sept. 26.—Considerable of the small grains have been sold, but most of the corn is not fit for market, altho it will make good feed.—X.

Blooming Prairie, Minn., Sept. 27.—Farmers are not selling grain, as they are dissatisfied with prices. Have not taken in 200 bus. of new wheat as yet.—Dk. Milling Co.

Montgomery City, Mo., Sept. 29.—Some wheat is being sold.—E. H. Baur, mgr. Producers Grain Co. No. 31.

Big Sandy, Mont., Oct. 1.—Elevator is nearly full, with cars in demand.—A. A. Delger, Montana Elevator Co.

Big Sandy, Mont., Sept. 25.—This place will handle from 400,000 to 500,000 bus. this year, and it should not be long until we get back to our 1916 volume, when we shipped 1,250,000



bus.—H. H. Schurmann, agt., Greely Elevator Co.

Rockford, Neb., Sept. 25.—No grain is moving now, but there is some corn to move.—A. T. Burroughs.

Rockford, Neb., Oct. 4.—Quite a bit of corn is moving but we are unable to get cars for it.—W. W. Topf, mgr., Farmers Union Co-op. Ass'n.

Medina, N. D., Oct. 9.—Elevators are all full with only a scant supply of cars available. Farmers are obliged to pile grain on the ground to be hauled later as the elevators get room.—P. M. Ingold.

Greenville, O., Oct. 6.—Movement of grain has been slowed up considerably due to anticipation of higher prices by farmers.—Chas. Minnich, mgr., The Soyco Mills Co.

Dayton, O., Oct. 4.—No grain is coming in as farmers are busy cutting corn and tobacco, and

planting wheat.—Durst Milling Co.

Bradley, Okla., Sept. 29.—The new corn crop will begin moving about Oct. 15.—C. E. Shetton, mgr., Bradley Elevator & Warehouse Co.

De Smet, S. D., Sept. 25.—Grain is moving slowly. I do not look for very heavy run until after the first freeze.—Farmers Co-op. Ass'n.

Frankfort, S. D., Oct. 3.—Our elevator has been full for the past three weeks, due to lack of cars.—The James Valley Elevator Co., Hans Realen, mgr.

Elk Point, S. D., Oct. 6.—Quite a lot of old corn to be moved here, but cannot obtain cars.—D. A. Greig.

Hecla, S. D., Oct. 1.—Cars are very scarce here. Nearly all the elevators along the line are full.—Hecla Co-op. Elevator Co.

Childress, Tex., Sept. 27.—No milo or kafir will be shipped from this county.—J. T. Walling, Scott & Walling.

Crop Reports

Reports on the acreage, condition and yield of grain and field seeds, as well as on the movement to country markets, are always welcome.

ALABAMA.

Mobile, Ala., Sept. 27.—Corn crop is very good and is fast being housed.—J. L. Suttle, Suttle & Hatcher.

CANADA.

Toronto, Ont., Aug. 28.—The estimated acreage of various crops in Toronto this year, compared with last year, is as follows: Wheat, 813,935 acres, 621,420 acres; spring wheat, 124,922 acres, 152,904 acres; barley, 433,922 acres, 462,176 acres; oats, 3,034,090 acres, 3,094,958 acres; rye, 152,709 acres, 122,868 acres; peas, 105,544 acres, 105,964 acres.—Dept. of Agriculture.

ILLINOIS.

Springfield, Ill., Oct. 4.—Corn is practically all safe, and husking is beginning in the northern counties. Winter wheat seeding continues; much of it is finished in the north. The ground is too dry for germination. Clover hulling and the harvesting of peas and beans are under way.—Clarence J. Root, meteorologist.

Chicago, Ill., Oct. 5.—The United States corn crop is estimated at 2,769,000,000 bus.; oats, 1,244,000,000 bus.; spring wheat, 261,000,000 bus.; all wheat, 823,000,000 bus. on the estimate of acreage and 803,000,000 bus. allowing for abandoned acreage. The latter factor will reduce the crop of oats to 1,155,000,000 bus.—P. S. Goodman.

Chicago, Ill., Oct. 5.—The condition of corn is estimated to be 77.6 or a point lower than a month ago, with a crop indication of 2,805,000,000 bus. Hot weather and drought in Aug. reduced yield of late corn and a considerable part of the crop is under weight. The crop is fully matured and it is cured better than usual at this time. Final threshing reports make a winter wheat crop of 552,000,000 bus., while spring wheat threshing to date confirms the earlier estimate of 288,000,000 bus. Dry weather and hard soil conditions have delayed fall plowing, but wheat seeding is now under way with indications pointing to an increased acreage of fall sown grain.—B. W. Snow, Bartlett-Frazier Crop Report.

Springfield, Ill., Oct. 7.—Corn has continued to mature rapidly during the week and 92% of the state crop is now reported safe from frost. There is some chaffy corn due to early ripening. The yield outlook for early corn, especially in the northern area, is reported as fair to favorable as a rule. The bulk of the crop is unusually well dried out and will be available for market earlier than usual. Some husking has already been done. The seeding of winter wheat and rye has made fair progress in the north, but is not up to the usual elsewhere, due to the unfavorably dry soil conditions prevailing. Reports indicate intentions to plant a liberal acreage this fall. Except in the northern area, pastures are of little or no feed value as

Barley Movement in September.

Receipts and shipments of barley at the various markets during September, compared with September, 1921, were as follows:

| | Receipts | | Shipments | |
|----------------|-----------|-----------|-----------|-----------|
| | 1922. | 1921. | 1922. | 1921. |
| Baltimore | 134,757 | 472,369 | 118,334 | 330,441 |
| Chicago | 1,260,000 | 694,000 | 381,000 | 304,000 |
| Cincinnati | 9,100 | 2,600 | ..... | ..... |
| Duluth | 1,200,040 | 1,572,439 | 892,890 | 1,586,819 |
| Ft. William | 2,483,368 | 2,129,860 | 1,513,437 | 1,820,548 |
| Kansas City | 30,000 | 223,500 | 35,100 | 102,700 |
| Los Angeles, | ..... | ..... | ..... | ..... |
| cars | 100 | 79 | ..... | ..... |
| Milwaukee | 889,540 | 920,760 | 356,857 | 403,769 |
| Minneapolis | 1,503,110 | 1,378,880 | 1,283,340 | 1,066,860 |
| New York | 383,300 | ..... | 239,000 | ..... |
| Omaha | 52,800 | 180,000 | 28,800 | 163,800 |
| Peoria | 74,200 | 43,600 | 67,200 | 30,600 |
| St. Joseph | 5,250 | 15,750 | ..... | 3,500 |
| St. Louis | 70,400 | 80,000 | 9,030 | 16,590 |
| San Francisco, | ..... | ..... | ..... | ..... |
| tons | 91,678 | 80,950 | ..... | ..... |
| Toledo | 2,400 | 3,600 | ..... | ..... |
| Wichita | 1,200 | 1,200 | 1,200 | 1,200 |
| Winnipeg | 2,913,400 | ..... | ..... | ..... |

Oats Movement in September.

Receipts and shipments of oats at the various markets during September, compared with September, 1921, were as follows:

| | Receipts | | Shipments | |
|----------------|-----------|-----------|-----------|-----------|
| | 1922. | 1921. | 1922. | 1921. |
| Baltimore | 1,152,753 | 92,271 | 972,350 | ..... |
| Chicago | 9,503,000 | 6,306,000 | 6,907,000 | 5,807,000 |
| Cincinnati | 214,000 | 310,000 | 120,000 | 380,000 |
| Duluth | 416,718 | 864,816 | 231,840 | 1,338,480 |
| Ft. William | 741,868 | 1,972,372 | 701,531 | 3,671,637 |
| Indianapolis | 762,000 | 1,128,000 | 682,000 | 1,250,000 |
| Kansas City | 600,100 | 923,100 | 253,500 | 247,500 |
| Los Angeles, | ..... | ..... | ..... | ..... |
| cars | 29 | 23 | ..... | ..... |
| Milwaukee | 1,804,095 | 1,721,640 | 1,331,910 | 1,997,553 |
| Minneapolis | 3,837,360 | 4,217,340 | 4,191,880 | 1,747,780 |
| New Orleans | ..... | ..... | 65,389 | 27,875 |
| New York | 2,465,000 | ..... | 1,970,000 | ..... |
| Omaha | 1,162,000 | 1,110,000 | 934,000 | 790,000 |
| Peoria | 1,273,000 | 1,140,000 | 1,385,400 | 1,009,800 |
| St. Joseph | 128,000 | 64,000 | 38,000 | 18,000 |
| St. Louis | 1,760,000 | 2,437,260 | 1,341,490 | 1,723,670 |
| San Francisco, | ..... | ..... | ..... | ..... |
| tons | 1,389 | 1,186 | ..... | ..... |
| Toledo | 229,600 | 352,600 | 90,110 | 236,460 |
| Wichita | 13,500 | 9,000 | 12,000 | 7,000 |
| Winnipeg | 1,858,000 | ..... | ..... | ..... |

Corn Movement in September.

Receipts and shipments of corn at the various markets during September, compared with September, 1921, were as follows:

| | Receipts | | Shipments | |
|----------------|------------|------------|-----------|------------|
| | 1922. | 1921. | 1922. | 1921. |
| Baltimore | 832,709 | 965,015 | 1,008,094 | 598,534 |
| Chicago | 20,512,000 | 23,406,000 | 9,798,000 | 17,238,000 |
| Cincinnati | 356,400 | 330,000 | 136,800 | 130,000 |
| Duluth | 928,472 | 1,118,910 | 498,476 | 802,073 |
| Ft. William | 10,810 | 108,440 | 10,810 | 120,611 |
| Indianapolis | 1,738,000 | 1,258,600 | 1,419,000 | 765,800 |
| Kansas City | 451,250 | 372,500 | 706,250 | 633,750 |
| Los Angeles, | ..... | ..... | ..... | ..... |
| cars | 74 | 74 | ..... | ..... |
| Milwaukee | 2,104,460 | 3,937,375 | 1,065,911 | 5,356,775 |
| Minneapolis | 486,600 | 861,110 | 280,960 | 428,360 |
| New Orleans | ..... | ..... | 986,475 | 385,711 |
| New York | 1,034,800 | ..... | 705,000 | ..... |
| Omaha | 1,716,400 | 1,450,400 | 1,297,800 | 1,100,400 |
| Peoria | 2,246,150 | 1,709,185 | 1,908,130 | 684,450 |
| St. Joseph | 556,550 | 594,000 | 324,000 | 316,500 |
| St. Louis | 2,796,300 | 1,855,100 | 1,734,005 | 1,173,240 |
| San Francisco, | ..... | ..... | ..... | ..... |
| tons | 1,368 | 1,024 | ..... | ..... |
| Texas City | ..... | 42,837 | ..... | 55,690 |
| Toledo | 388,950 | 338,750 | 116,750 | ..... |
| Wichita | 76,800 | 21,600 | 50,000 | 15,000 |

Rye Movement in September.

Receipts and shipments of rye at the various markets during September, compared with September, 1921, were as follows:

| | Receipts | | Shipments | |
|--------------|-----------|-----------|-----------|-----------|
| | 1922. | 1921. | 1922. | 1921. |
| Baltimore | 1,937,901 | 791,189 | 1,753,827 | 617,141 |
| Chicago | 368,000 | 207,000 | 272,000 | 298,000 |
| Cincinnati | 20,400 | 16,800 | 21,600 | 16,800 |
| Duluth | 9,729,408 | 3,894,255 | 8,974,940 | 4,417,119 |
| Ft. William | 3,063,528 | 631,709 | 2,584,398 | 498,818 |
| Indianapolis | 43,000 | 26,800 | 44,000 | 23,800 |
| Kansas City | 63,800 | 63,800 | 15,400 | 15,400 |
| Los Angeles, | ..... | ..... | ..... | ..... |
| cars | 2 | 4 | ..... | ..... |
| Milwaukee | 193,885 | 158,070 | 132,952 | 214,190 |
| Minneapolis | 1,239,410 | 562,880 | 398,550 | 191,150 |
| New Orleans | ..... | ..... | 8,571 | ..... |
| New York | 1,234,900 | ..... | 1,117,000 | ..... |
| Omaha | 186,200 | 321,200 | 175,000 | 215,000 |
| Peoria | 6,000 | 9,600 | 8,400 | 9,600 |
| St. Joseph | 6,000 | 3,000 | 1,500 | ..... |
| St. Louis | 42,900 | 75,900 | 14,200 | 17,940 |
| Toledo | 24,000 | 97,200 | 8,115 | 123,180 |
| Wichita | ..... | 1,200 | ..... | 1,200 |
| Winnipeg | 2,609,925 | ..... | ..... | ..... |

Daily Closing Prices.

The daily closing prices for wheat, corn, oats, rye and barley for December delivery at the following markets for the past two weeks have been as follows:

DECEMBER WHEAT.

| | Sept. 25. | Sept. 26. | Sept. 27. | Sept. 28. | Sept. 29. | Sept. 30. | Oct. 1. | Oct. 2. | Oct. 3. | Oct. 4. | Oct. 5. | Oct. 6. | Oct. 7. | Oct. 8. | Oct. 9. |
|----------------|-----------|-----------|-----------|-----------|-----------|-----------|---------|---------|---------|---------|---------|---------|---------|---------|---------|
| Chicago | 104 1/4 | 104 1/4 | 105 1/4 | 105 1/4 | 105 1/4 | 105 1/4 | 105 1/4 | 106 1/4 | 106 1/4 | 106 1/4 | 106 1/4 | 106 1/4 | 107 1/4 | 107 1/4 | 107 1/4 |
| Toledo | 115 | 116 | 116 1/2 | 117 | 117 | 118 1/2 | 117 | 118 | 118 1/2 | 119 1/2 | 118 1/2 | 119 1/2 | 119 1/2 | 119 1/2 | 119 1/2 |
| Kansas City | 97 1/4 | 97 1/4 | 98 1/4 | 98 1/4 | 97 3/4 | 99 1/4 | 97 3/4 | 98 1/4 | 99 | 99 1/4 | 98 1/4 | 99 1/4 | 99 1/4 | 100 1/4 | 100 1/4 |
| St. Louis | 103 1/4 | 103 1/4 | 104 1/4 | 104 1/4 | 103 1/4 | 104 1/4 | 103 1/4 | 104 1/4 | 104 1/4 | 105 1/4 | 105 1/4 | 105 1/4 | 106 1/4 | 106 1/4 | 106 1/4 |
| Minneapolis | 104 | 104 1/4 | 104 1/4 | 105 1/4 | 104 1/4 | 106 1/4 | 104 1/4 | 105 1/4 | 105 1/4 | 106 1/4 | 107 1/4 | 107 1/4 | 107 1/4 | 107 1/4 | 107 1/4 |
| Duluth (durum) | 85 1/2 | 85 1/2 | 85 1/2 | 85 1/2 | 85 1/2 | 86 1/2 | 85 1/2 | 86 | 85 1/2 | 85 1/2 | 85 1/2 | 85 1/2 | 86 1/4 | 85 1/2 | 85 1/2 |
| Winnipeg | 94 1/4 | 94 1/4 | 94 1/4 | 94 1/4 | 94 1/4 | 96 1/4 | 94 1/4 | 95 1/4 | 94 1/4 | 96 1/4 | 95 1/4 | 95 1/4 | 97 1/4 | 95 1/4 | 95 1/4 |
| Milwaukee | 104 1/4 | 104 1/4 | 105 1/4 | 105 1/4 | 104 1/4 | 106 | 104 1/4 | 106 | 106 1/4 | 107 1/4 | 106 1/4 | 107 1/4 | 107 1/4 | 107 1/4 | 107 1/4 |

DECEMBER CORN.

| | Sept. 25. | Sept. 26. | Sept. 27. | Sept. 28. | Sept. 29. | Sept. 30. | Oct. 1. | Oct. 2. | Oct. 3. | Oct. 4. | Oct. 5. | Oct. 6. | Oct. 7. | Oct. 8. | Oct. 9. |
|-------------|-----------|-----------|-----------|-----------|-----------|-----------|---------|---------|---------|---------|---------|---------|---------|---------|---------|
| Chicago | 58 3/4 | 59 1/4 | 59 1/4 | 59 1/4 | 58 3/4 | 59 1/4 | 59 1/4 | 61 1/4 | 60 1/4 | 61 1/4 | 61 1/4 | 61 1/4 | 62 1/4 | 62 1/4 | 62 1/4 |
| Kansas City | 53 1/4 | 54 1/4 | 54 1/4 | 54 1/4 | 54 1/4 | 55 1/4 | 55 1/4 | 57 1/4 | 56 1/4 | 57 1/4 | 57 1/4 | 57 1/4 | 58 1/4 | 58 1/4 | 58 1/4 |
| St. Louis | 58 1/4 | 58 1/4 | 59 | 59 | 58 1/4 | 59 | 59 1/4 | 60 1/4 | 60 1/4 | 61 1/4 | 61 1/4 | 61 1/4 | 62 1/4 | 62 1/4 | 62 1/4 |
| Milwaukee | 58 3/4 | 59 1/4 | 59 1/4 | 59 1/4 | 58 3/4 | 59 1/4 | 59 1/4 | 61 1/4 | 60 1/4 | 61 1/4 | 61 1/4 | 61 1/4 | 62 1/4 | 62 1/4 | 62 1/4 |

DECEMBER OATS.

| | Sept. 25. | Sept. 26. | Sept. 27. | Sept. 28. | Sept. 29. | Sept. 30. | Oct. 1. | Oct. 2. | Oct. 3. | Oct. 4. | Oct. 5. | Oct. 6. | Oct. 7. | Oct. 8. | Oct. 9. |
|-------------|-----------|-----------|-----------|-----------|-----------|-----------|---------|---------|---------|---------|---------|---------|---------|---------|---------|
| Chicago | 36 1/2 | 36 1/2 | 37 1/2 | 37 1/2 | 36 1/2 | 37 1/2 | 37 1/2 | 39 | 38 3/4 | 38 1/2 | 38 1/2 | 38 1/2 | 39 | 39 | 39 |
| Kansas City | 36 1/2 | 36 1/2 | 37 1/2 | 37 1/2 | 37 | 37 1/2 | 37 1/2 | 39 | 38 3/4 | 38 1/2 | 38 1/2 | 39 1/4 | 39 1/4 | 39 1/4 | 39 1/4 |
| St. Louis | 37 1/4 | 37 1/4 | 37 1/4 | 37 1/4 | 37 1/4 | 37 1/4 | 37 1/4 | 38 1/4 | 38 1/4 | 38 1/4 | 38 1/4 | 39 1/4 | 39 1/4 | 39 1/4 | 39 1/4 |
| Minneapolis | 31 1/2 | 31 1/2 | 32 | 31 1/2 | 31 1/2 | 31 1/2 | 32 | 33 1/2 | 32 1/2 | 32 1/2 | 32 1/2 | 32 1/2 | 32 1/2 | 32 1/2 | 32 1/2 |
| Winnipeg | 38 1/4 | 38 1/4 | 38 1/4 | 38 1/4 | 38 1/4 | 39 1/4 | 38 1/4 | 38 1/4 | 38 1/4 | 38 1/4 | 38 1/4 | 38 1/4 | 39 | 38 1/4 | 38 1/4 |
| Milwaukee | 36 1/2 | 36 1/2 | 37 1/2 | 37 1/2 | 36 1/2 | 37 1/2 | 37 1/2 | 38 1/4 | 38 1/4 | 38 1/4 | 38 1/4 | 38 1/4 | 39 | 38 1/4 | 38 1/4 |

DECEMBER RYE.

| | | | | | | | | | | | | | |
|-------------|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|
| Chicago | 69¾ | 69½ | 69¾ | 69½ | 69½ | 70¾ | 69¾ | 70 | 70 | 71¾ | 70½ | 71¾ | 71¾ |
| Minneapolis | 64½ | 64¾ | 64½ | 64¾ | 64¾ | 64½ | 63¾ | 64 | 64¾ | 65 | 64½ | 64¾ | 64¾ |
| Duluth | 65¼ | 65 | 65 | 65¾ | 65¼ | 65¾ | 65 | 66 | 65¾ | 66 | 65 | 65¾ | 65¾ |
| Winnipeg | 64½ | 65 | 64½ | 64½ | 64½ | 64¾ | 64¾ | 64¾ | 65 | 65¼ | 65¼ | 65¾ | 65¾ |

a rule. Feeding in many central and southwestern counties has been necessary for some time. Cloverseed hulling from a largely increased acreage is showing very uneven returns. Yields are somewhat lower than earlier indications in many localities.—A. J. Surratt, Agricultural Statistician.

INDIANA.

Hope, Ind., Oct. 4.—Farmers in this vicinity are just commencing to sow wheat. The ground is dry but in good condition for seeding. About an average acreage will be planted.—Stafford Grain Co.

IOWA.

Toeterville, Ia., Sept. 29.—Stack threshing will finish this week. Oats are of excellent quality and good yield. We have a good crop of matured corn.—H. D. Mundt.

KANSAS.

Burdett, Kan., Sept. 30.—Quality of wheat is not as good as last year.—T. H. Urton, mgr., Bauer-Vaughan Grain Co.

Concordia, Kan., Oct. 5.—Poor outlook for the wheat this fall. We have had no rain here for some time.—E. F. Carpenter.

Winona, Kan., Sept. 29.—A large acreage of wheat is being planted. Ground is dry; we have had no rain in several months.—B. F. Snider, mgr. Wheatland Elevator Co.

Hutchinson, Kan., Oct. 7.—Dry weather continues over practically the whole of southwestern Kansas and farmers are complaining of inability to seed their winter wheat. Much of the seeding has already been done, however, and considerable wheat is up and the fields are green. These growing plants are making little headway and can make none until moisture is received. The acreage will compare favorably with that of last season and on the whole the crop is getting a better start than it did in the fall of 1921. Kafir and milo are maturing under almost ideal conditions.—Cal.

MARYLAND.

Washington, D. C., Oct. 5.—Corn has matured very rapidly in practically all parts of the country, its ripening being hastened by high temperatures. In Illinois, Indiana, Iowa, Kansas, and Michigan much corn is reported to be chaffy due to premature ripening. Silo filling is well advanced and nearly completed in several sections. A smaller crop of merchantable corn than usual is reported from Kansas. Plowing and seeding of fall sown grains has been generally delayed in most sections by lack of moisture in the soil and is reported to be practically at a stand-still in Illinois, Indiana, and Michigan. In many sections farmers are delaying the seeding of wheat waiting for fly free dates. There is ample moisture reported from Missouri, eastern Kansas, and parts of Nebraska and Iowa. Wheat seeding is extending northward into new territory in Iowa. A large acreage of rye and barley is expected in Oklahoma if rains come soon. Seeding is nearly completed in western Kansas, and much of the wheat is up in the eastern section of the state. Threshing of oats continues over most of the country. Yields of spring grains reported to be generally light in southern areas. Threshing of wheat is in progress in the northwest and is grading high in North Dakota and Montana except for slight damage in spots by rains. Car shortage in these states is now causing concern. In New England some late buckwheat fields have been damaged by frosts but most of the crop has been harvested with good yields. A good crop of this grain is reported in Iowa. Broom corn harvest is progressing in Oklahoma. The harvest of rice is nearly completed in Texas where thrashing is well advanced and quality of the grain is good. It is ripening slowly in Arkansas on account of cool nights and has started in California where a better than average yield is expected.—U. S. Dept. of Agriculture.

MINNESOTA.

Lynd, Minn., Sept. 27.—Small grains are very good. Corn is about one-third of a crop.—X.

Wells, Minn., Oct. 1.—Crops are good, generally speaking, altho wheat and buckwheat did not yield good. Corn, flax, rye and oats turned out better than expected.—Wells Farmers Elevator Co.

MISSOURI.

Montgomery City, Mo., Sept. 29.—Corn crop is about 70%.—Producers Grain Co. No. 31, E. H. Baur, mgr.

MONTANA.

Big Sandy, Mont., Oct. 1.—Nearly all wheat is grading No. 1 dark fancy. There is very little dirt, no smut and test weight is 61 lbs.—A. A. Delger, Montana Elevator Co.

Simms, Mont., Oct. 1.—Winter wheat is yielding 12 bus. per acre, spring wheat 10 bus. per acre. No oats, barley or flax is raised around here.—E. P. Herbert, agt. Montana Central Elevators.

NEBRASKA.

Rockford, Neb., Sept. 25.—Weather very dry. Fall wheat about one-half in.—A. T. Burroughs. Sargeant, Neb., Sept. 25.—Crops were very light here this year. Corn is very good.—J. E. Weber, mgr., W. T. Barstow Grain Co.

Government Crop Report.

Washington, D. C., Oct. 9.—The crop reporting board of the Bureau of Agricultural Economics makes the following estimates:

| Crop. | Oct. 1, 1922, forecast.* | Yield per acre, 1921.* 1922.* | Acreage, 1922.* |
|---------------------|--------------------------|-------------------------------|-----------------|
| Wht., bu. | 541,809 | 587,032 14.2 | 38,131 |
| Sp. wht., bu. | 268,314 | 297,861 14.4 | 18,639 |
| All wht., bu. | 810,123 | 794,893 14.3 | 56,770 |
| Corn, bu. | 2,853,399 | 3,080,372 27.6 | 103,234 |
| Oats, bu. | 1,229,774 | 1,060,737 29.4 | 41,822 |
| Barley, bu. | 196,431 | 151,181 26.0 | 7,550 |
| Rye, bu. | 79,623 | 57,918 15.5 | 5,148 |
| Buckwht., bu. | 14,051 | 14,079 19.9 | 707 |
| Flaxseed, bu. | 11,725 | 8,112 8.7 | 1,341 |
| Rice, bu. | 39,159 | 36,515 38.8 | 1,009 |
| Hay, tame | 92,886 | 81,567 1.52 | 61,006 |
| Hay, wild | 15,850 | 15,235 1.00 | 15,774 |
| Grain sorghums, bu. | 95,840 | 115,110 19.0 | 5,057 |

*In thousands of bus., i. e., 000 omitted.

*Tons.

CORN.

| State. | 1922. Pct. | 10-yr. ave. Pct. | Forecast 1922.* Oct. 1. | (Dec. est.) 1921.* |
|---------|------------|------------------|-------------------------|--------------------|
| Pa. | 84 | 86 | 64,735 | 76,272 |
| Va. | 89 | 84 | 54,789 | 47,600 |
| N. Car. | 80 | 83 | 48,499 | 49,254 |
| Ga. | 68 | 84 | 55,533 | 69,975 |
| Ohio | 83 | 84 | 154,828 | 159,326 |
| Ind. | 83 | 82 | 176,391 | 169,848 |
| Ill. | 80 | 77 | 313,251 | 305,966 |
| Mich. | 83 | 79 | 60,552 | 66,417 |
| Wis. | 89 | 81 | 93,681 | 97,482 |
| Minn. | 77 | 84 | 112,442 | 140,507 |
| Iowa | 96 | 84 | 441,201 | 444,190 |
| Mo. | 77 | 71 | 169,372 | 182,880 |
| S. Dak. | 81 | 84 | 113,010 | 125,632 |
| Neb. | 64 | 73 | 166,186 | 207,732 |
| Kan. | 59 | 52 | 105,201 | 102,142 |
| Ky. | 83 | 82 | 89,578 | 82,150 |
| Tenn. | 75 | 82 | 75,214 | 90,713 |
| Ala. | 67 | 78 | 48,749 | 62,651 |
| Miss. | 73 | 76 | 49,186 | 57,096 |
| La. | 70 | 76 | 29,488 | 35,022 |
| Tex. | 69 | 73 | 121,234 | 156,920 |
| Okla. | 61 | 59 | 57,066 | 76,925 |
| Ark. | 69 | 74 | 46,850 | 60,148 |
| U. S. | 78.4 | 77.1 | 2,853,399 | 3,080,372 |

FLAXSEED.

| | | | | |
|---------|------|------|--------|-------|
| Minn. | 87 | 82 | 2,871 | 2,726 |
| N. Dak. | 84 | 70 | 4,628 | 2,534 |
| S. Dak. | 86 | 81 | 2,245 | 1,404 |
| Mont. | 70 | 61 | 1,638 | 1,125 |
| U. S. | 82.6 | 71.1 | 11,725 | 8,112 |

*In thousands of bus., i. e., 000 omitted.

OATS.

| State. | 1922.* | Yield per acre, bus. 10-year ave. | Total production in thousands of bus. 1921. | (Dec. est.) |
|---------|--------|-----------------------------------|---|-------------|
| N. Y. | 30.5 | 32.6 | 32,300 | 24,912 |
| Pa. | 32.0 | 33.6 | 39,616 | 35,283 |
| Ohio | 27.0 | 36.2 | 40,959 | 37,122 |
| Ind. | 21.0 | 34.1 | 33,516 | 45,072 |
| Ill. | 28.0 | 37.2 | 110,628 | 121,741 |
| Mich. | 33.0 | 32.9 | 51,447 | 28,101 |
| Wis. | 41.5 | 37.7 | 104,370 | 63,958 |
| Minn. | 36.0 | 34.4 | 138,456 | 94,176 |
| Iowa | 37.0 | 37.7 | 220,520 | 154,960 |
| Mo. | 16.0 | 27.3 | 20,624 | 42,960 |
| N. Dak. | 33.0 | 25.4 | 83,820 | 49,761 |
| S. Dak. | 30.0 | 31.8 | 76,320 | 58,300 |
| Neb. | 22.0 | 30.5 | 55,154 | 70,054 |
| Kan. | 18.5 | 26.7 | 29,434 | 38,827 |
| U. S. | 29.4 | 32.4 | 1,229,774 | 1,060,737 |

SPRING WHEAT.

| | | | | |
|---------|------|------|---------|---------|
| Minn. | 13.7 | 13.4 | 30,702 | 23,655 |
| N. Dak. | 14.1 | 10.9 | 114,506 | 73,264 |
| S. Dak. | 13.5 | 11.4 | 36,652 | 24,930 |
| Mont. | 17.0 | 15.2 | 32,555 | 23,940 |
| Wash. | 9.2 | 16.6 | 10,552 | 17,205 |
| U. S. | 14.4 | 12.7 | 268,314 | 207,861 |

*Preliminary estimate.

Holmesville, Neb., Sept. 25.—Corn crop will make about 70% of a crop, and will be light in quality. Fall plowing of wheat is about completed with ground in fine condition. About an average acreage will be planted.—Geo. Hunkle, mgr. Holmesville Farmers Elevator Co.

NORTH DAKOTA.

Medina, N. D., Oct. 9.—The crops were good here this year. Red durum is the principal crop and yielded 15 to 25 bus. per acre of No. 1 and 2. Marquis did well, yielding 15 to 18 bus. of mostly No. 2 grade.—P. M. Ingold.

OHIO.

Greenville, O., Oct. 6.—The corn crop throughout Darke county, and Pickaway county is excellent.—Chas. Minnich, mgr., The Soyco Mills Co.

Dayton, O., Oct. 4.—This section is very dry and needs rain badly for fall crops. Farmers are planting winter wheat.—Durst Milling Co.

OKLAHOMA.

Bradley, Okla., Sept. 29.—Corn crop will be short.—C. E. Shetton, mgr., Bradley Elevator & Warehouse Co.

Fay, Okla., Sept. 29.—Plowing was done in good shape, but we need rain for sowing.—Y. A. Abercrombie, agt., Wheeler Grain Co.

Chickasha, Okla., Oct. 4.—Corn crop was cut very short by the hot, dry weather, but there is some good corn on the eastern part of the state.—Kitching Grain Co.

OREGON.

Haines, Ore., Sept. 30.—Crops are very good.—N. E. Dodd.

PENNSYLVANIA.

Orangeville, Pa., Oct. 2.—Owing to the reduced acreage of buckwheat and the dry weather, our buckwheat crop will average only 50% of normal or the ten year average.—H. B. Low & Son.

Wilkes Barre, Pa., Sept. 25.—The acreage of buckwheat in this section is about 70% of last year, and from what we can learn so far, the average yield per acre is from 20 to 25 bus. of good quality. We have not purchased any new buckwheat as yet, but we understand that some of the millers in this section paid farmers \$1.40 per 100 lbs.

SOUTH DAKOTA.

Elk Point, S. D., Oct. 6.—Corn is 15 to 20% less than the 1921 crop. Much of it will be chaffy due to much hot and dry weather. Wheat is all of good quality.—D. A. Greig.

TEXAS.

Fort Worth, Tex., Oct. 3.—Wheat arriving here recently is showing lots of heat damage. We saw a number of samples showing more than 3% heat damage which arrived here today.—Transit Grain & Commission Co.

Conference of Weighmasters and Scalemen.

[Continued from page 480.]

ready reference, we include in this report the aforesaid sweeping regulations of this Conference as follows:

"Resolved, that it is the duty of the unloaders to sweep each car clean of all grain; and, further, that it is incumbent upon the Supervising Weighing Department to insist that such sweeping shall be well and thoroughly done at the unloading sinks; and that where the unloaders of bulk grain do not sweep the cars clean, thereby necessitating the after-sweeping of such cars in adjacent railroad yards, such after-sweeping should be done at the expense of such unloaders; and that the grain so swept from such cars should be credited to the cars from which it was swept."

Respectfully submitted, A. E. Schuyler, M. H. Ladd, Geo. A. Wells.

H. A. Foss of Chicago was re-elected chairman of the conference and H. W. Ewert was re-elected secretary.

Weighing department officials and scalemen present from various markets were: H. A. Foss, C. A. King, C. G. Stoddy and J. A. Schmitz, Chicago, Ill.; Clay Johnson, Peoria, Ill.; A. E. Kahler, Baltimore, Md.; P. P. Quist, Minneapolis, Minn.; A. J. Barnidge, John Dower and T. H. Schlunkert, St. Louis, Mo.; H. H. Alfrey, El Reno, Okla.; J. W. Winchester, Galveston, Tex.; M. H. Ladd, Milwaukee, Wis.

Conference of Weighmasters and Scalemen.

The fifth annual Conference of Weighmasters and Scalemen was called to order by Chairman H. A. Foss, weighmaster of the Chicago Board of Trade in parlor H of the Grunewald Hotel, New Orleans, with a rather light attendance, who addressed the delegates as follows:

Address of Chairman Foss.

It is with extreme pleasure that I call to order this, our 5th Annual, meeting and, on behalf of the association, I welcome you all here today.

It may not be out of place for me to remark that these meetings, of ours, have been helpful to the members of my weighing organization, and helpful to me; and that, so far as within our province and power, we have endeavored to obtain profit for our department from the exchange of ideas, and from the regulations that have been suggested to us, for our guidance by this association.

You will find, in your seats, a program that is suggested for the guidance of this meeting. You will note that the subjects thereon are confined exclusively to "committee reports", and "questions and answers", together with time for the discussion of any matters, pertinent to scales and weighing, that may be brought up by anybody. This suggested outline for our program is carrying out the thought that you will find expressed in one of the questions, which was submitted by a brother member, for our question box, to the effect that our organization is purely a voluntary organization, and that its meetings are held merely for the purpose of education by an exchange of ideas, and by impartial investigations, carried on by specially appointed committees, whose only powers (like those of the association itself) are to present findings and conclusions for the information of anyone interested; and, also, that our meetings are held to permit anyone, interested in scales and weighing, to place subjects, pertinent thereto, before a body of men, concerned in correct weights.

At our last meeting, it was suggested that the open character of our membership and meetings should, in a measure, be somewhat curtailed by the adoption of a constitution and by-laws, adherence to which would, naturally, be expected on the part of the members. I am well persuaded that your judgment will be sound, and to the best interests of correct weights.

The "Question Box" is the first number on our program. Our Secretary sent a request to everyone, on his mailing list, to submit questions for this number, and the response thereto has been about as generous as it usually is. In order to give us a basis for discussion, I submitted the questions to certain available members asking them to present their ideas of suitable answers, the same as I have done with questions of previous meetings. Now, I have not read such answers, and, like those others who have not read them, I am not in position to say whether or not I agree with them. But, in any case, these answers should bring out viewpoints, pro and con, and thereby give us all possible light on the different subjects involved.

Chairman Foss called upon Secretary Ewert to read the Questions and Answers contributed to the Question Box, all of which follow:

The Question Box.

Dust Collecting Devices.

Authorities connected with fire prevention bureaus, national, state and insurance, generally feel that a free vent at the head of the leg should be provided for each and every elevating leg (shipper, receiver and cleaner) for the purpose of reducing the extent of dust clouds in grain elevators. Let it be said to the credit of the Weighmasters' Scalemen's Conference that this organization, some years ago, anticipated the needs of the situation and the demands that might eventually be made up on the grain trade, provided means and measures for free vents not only in the tops of elevators to clear garner and weigh rooms of dust, but also approved methods of removing dust from cellar sinks, pans, or boot housings.

In spite of these intelligent and practical means of caring for dust clouds in elevators due to the handling of grain from car to scale and scale to car, we frequently hear of movements of dust preventing bodies advocating measures that are not nearly as complete, effective, or practical, as the measures provided for in the report adopted by the Weighmasters' Scalemen's Conference on the subject. Some of these bodies recommend vents in the heads of elevating legs leading out into the open air, such vents now being in operation in many of the flour mills throughout the country. Others advocate direct suction, in some cases even to the extent of cleaning the grain before the grain is allowed to enter an elevator. All of this agitation indicates that sooner or later weighing departments will be called upon to

pass on dust collecting appliances at mills, elevators and industries.

Question: (1) Should each Weighing Department stand pat on the Weighmasters' Scalemen's Conference regulations covering dust collecting devices? It was so decided.

And, (2) suppose the pressure on such a Weighmaster reaches a point where it becomes embarrassing, what should he do?—Left to each department to decide for itself.

Note.—The embarrassment suggested in this question relates to the claims and contentions of explosion prevention bodies and of health regulating authorities, national, state and insurance.

The Conditions Making for Excessive Shortages and Overages.

Question: Based on your experience (aside from any possible loss of grain in transit, over which, obviously, weighing departments have no control other than inspecting cars) what three things, or features, or conditions are liable to cause excessive shortages in the weights and excessive overages in the weights?

First, from inbound cars:

(a) in hand operated hopper scales and in connection with the handling of such inbound grain from cars to scale, and

(b) where the weighing is done on track scales.

Second, for outbound cars:

(a) in hand operated hopper scale and in connection with the handling of such outbound grain from scale to car, and

(b) where the weighing is done on track scales.

Answer, as to inbound cars: Condition No. 1—Transposition of car numbers or weights. To prevent this some supervising systems provide punching devices at the unloading hoppers and scales for use on track tickets and printing beam tickets.

Condition No. 2—Mixing of grain of one car during the unloading with that of another car. This can be prevented only by the weighing departments supervisors downstairs and upstairs watching the signals. The downstairs deputies particularly should keep in complete touch with such signals.

Condition No. 3—Before mentioning condition No. 3, we would state that error in recording weights where registering beams are used would not come within these three conditions, and errors in scales is a scale inspector's job, hence as condition No. 3 we would caution the upstairs man to watch out for lodgments of grain in garner and leaking garner and scale slides.

Answer, as to outbound cars: Condition No. 1—Leaking loading spouts where the spouts pass through any elevator bin or bins that are not well in accordance with the regulations for new equipment of the Weighmasters' Scalemen's Conference.

Condition No. 2—Transposition of weights or car numbers.

Condition No. 3—Carelessness in handling spills.

The following five questions were referred to a com'tee, which supplied the answers:

Weight of Car Partly Unloaded and Reloaded.

Would you consider weights correct under the following conditions of weighing: For instance a car loaded out 60,000 pounds. The grain therein misgraded, hence 30,000 pounds of this load was sent back to the elevator, the balance was left in the car and 30,000 pounds of other grain reloaded on top thereof. Would a Weighing Department be justified in issuing a clear certificate of weight for a car under such circumstances?

Answer: We see no reason why the weight of the final load would be jeopardized by such unloading and reloading of part of the car, provided thoroughly and completely supervised upstairs and downstairs as provided for in our definition of Standard Weight Supervision; hence, in our opinion, the supervising weighing department, furnishing such supervision, would be justified in issuing a clear certificate of weight for such a car. However, it is obvious that in every handling and rehandling of grain there is a hazard, even though slight. Therefore, we believe that the supervising weighing department, in such a case, should ask the loaders to unload all of the grain in the car and reload the car, but, under favorable circumstances would not make such unloading of all the car compulsory.

Snow and Ice on Car.

Where cars are weighed on track scale and considerable time elapses between the weighing of the light and loaded car, what provision, to secure the correct weight, should be taken in case there is snow and ice on the car box? Also, what allowance, if any, should be made where the car box was exposed to the rain before the first weighing, or where the car is rained upon between the weighing?

Answer: In weighing cars on track scales, it will happen that the cars are set on the scales dripping with water. In a busy time, at a busy elevator, it is not practical to hold up the weighing of such cars for more favorable conditions. Similarly, there are times when cars are set on track scales, for weighing, covered with melting ice and snow, which is even a more serious condition. Then, we have cases where the cars are not always kept under

cover between the weighings of the loaded and empty cars. These questions are the most difficult to answer of any that have been submitted to us by our Chairman, and while we have some very positive views on the matters involved, we would prefer not to suggest any answers, and would rather listen to, and join in, on the discussions in the open meeting. Then again, so much depends upon the individual case involved that it is not easy to make a general answer to apply to all cases. In connection with such track scale weighing, we would also suggest that the matter of protecting the tare and gross weight of the car, as might be affected by the weight of the cooperative material, both inbound and outbound cars, should receive consideration in the discussions.

Sufficiency of Test Weights.

Is it possible properly to test a track scale with say 10,000 lbs. or 15,000 lbs. of test weights, or less?

Answer: Our conference regulations specifically provide that no less than 8% of the capacity of the scale shall be the amount of test weights that must be used properly to test the scale.

Record of Condition of Grain.

Should the Deputy Weighmaster make a record of the condition of the grain at the time of unloading and loading, and what are the advantages of such records?

Answer: Our conference regulations provide that supervising weighing departments should record any condition of the grain that may have a bearing on the weight which includes, of course, a full notation as to any heating or hot grain. The advantages of such a record are obvious.

F. H. Schinkert, chairman of the Committee on Maximum Speed for safely blocking cars on track scales rendered the following report:

REPORT OF COMMITTEE ON RULES COVERING THE MAXIMUM SPEED OF CARS WHEN BEING BLOCKED ON TRACK SCALES FOR WEIGHING.

It is the sense of your committee that cars should be placed on the scales at the slowest possible speed, and where it is not practicable to control the speed and stop the car with the hand brake, and the chucking of the wheel is resorted to, the obstruction should consist of a yielding substance, such as a pad made from rubber belting united together, or heavy rope, so that the shock caused by the sudden stop, will not cause a derangement or breakage of the scale parts, or change of balance of the weigh beam.

Signed: F. H. Schinkert, C. G. Stody, J. A. Schmitz.

J. A. Schmitz, Assistant Weighmaster, presented the following report of the committee on new equipment.

Report of Committee on New Equipment.

As you know, the elevator builders have no association. We are, therefore, handicapped in getting together with the elevator builders on the question of "New Equipment". However, when the Hopper Scale Specifications of Docket 9009 ICC were discussed by the various organizations interested, a Mr. Sinks represented the elevator builders, and at that time the question of the elevator builders forming an organization was taken up with him, but up to this time no association has been formed. Our committee endeavored to get Mr. Sinks to meet with them, but for some reason or other this meeting never took place. I believe that until an association of elevator builders is formed your committee on New Equipment can not properly function.

M. H. Ladd, Weighmaster of the Milwaukee Chamber of Commerce, presented the following report from the Committee on Sweeping Tolerance.

Conference on Sweeping Tolerance.

REPORT OF SPECIALLY APPOINTED COMMITTEE OF THE WEIGHMASTERS' SCALE MENS'.

The undersigned (a committee appointed under a resolution adopted at the Chicago October meeting of the Weighmasters' Scale Mens' Conference to investigate and report on the matter of grain residue left in bulk grain cars) having completed their investigations, beg leave to submit their findings as follows:

EXPLANATORY (a) Grain residue lodged in lining pocket, that is not reasonably accessible to the unloaders, is not considered in the discussions of this report.

EXPLANATORY (b) The resweepings, and the resweeping tests, which form the basis of this report, were conducted under the direction of your committee, or by Weighing Departments, or by unloaders themselves. In all, thousands of cars were reswept.

(1) Based on reports of weighmasters and car inspectors, and on the above mentioned resweeping data, it appears that a very large proportion of unloaders sweep each car (having inside linings reasonably favorable for freeing

of grain) so thoroughly and well that the amount of grain residue left therein is negligible.

(2) Based on the same data, it appears that some unloaders sweep their cars as satisfactorily when their unloadings are light, but are apt to be less thorough in their sweepings when their unloadings are heavy; and further, it would appear that this tendency sometimes is more pronounced during night unloadings, and where the unloading is done by inexperienced crews.

(3) Also, it would appear that some unloaders clean out their cars, on the average, more thoroughly than other unloaders.

(4) For the information of the members of our Ass'n, we respectfully submit, in the following compilations marked Exhibits 1, 2, 3, 4 and 5, summaries of some of the above mentioned resweepings of cars that were unloaded by five different unloaders who (the terminal weighing departments which look after the weighing of the grain report) sweep out their cars at the unloading hoppers very satisfactorily:

Exhibit No. 1—Unloader R:

| |
|--|
| 298 cars—Amount of grain residue negligible. |
| 4 cars—4 pounds each. |
| 56 cars—5 pounds each. |
| 2 cars—6 pounds each. |
| 9 cars—7 pounds each. |
| 10 cars—8 pounds each. |
| 1 car—9 pounds each. |
| 59 cars—10 pounds each. |
| 5 cars—12 pounds each. |
| 9 cars—15 pounds each. |
| 5 cars—20 pounds each. |

SUMMARY—Total number of cars reswept 458. Maximum amount left in any one car 20 pounds. Minimum amount left in any one car negligible. Average per car 2.94 pounds.

Exhibit No. 2—Unloader S:

| |
|--|
| 45 cars—Amount grain residue negligible. |
| 1 car—3 pounds each. |
| 12 cars—5 pounds each. |
| 2 cars—6 pounds each. |
| 1 car—8 pounds each. |
| 5 cars—10 pounds each. |
| 1 car—15 pounds each. |

SUMMARY—Total number of cars reswept, 67. Maximum amount left in any one car, 15 pounds. Minimum amount left in any one car negligible. Average per car 2.22 pounds.

EXPLANATORY—Exhibits 3, 4 and 5, Unloaders T, U and V, represent "resweepings" which weighing departments term "unannounced" resweepings. These tests were conducted by terminal weighing departments weekly or biweekly, covering a period of 7 months at three unloading elevators, which elevators they report also sweep out their cars very satisfactorily. The sweeping tests at each unloading point consisted of resweeping one or more settings of cars that had been unloaded and pulled out, or away, from the elevators.

Exhibit No. 3—Unloader T.

| |
|--|
| Average maximum amount grain residue per car secured in any one sweeping test, 3 pounds. |
| Average minimum amount grain residue per car secured in any one test, 1½ pounds. |
| Grand average of all cars swept, all tests, 2-27 pounds. |

Exhibit No. 4—Unloader U.

| |
|--|
| Average minimum amount grain residue per car secured in any one sweeping test, 4½ pounds. |
| Average minimum amount grain residue per car secured in any one sweeping test, 1 12-13 pounds. |
| Grand average of all cars swept, all tests, 3 1-45 pounds. |

Exhibit No. 5—Unloader V.

| |
|--|
| Average maximum amount grain residue per car secured in any one sweeping test, 3 3-7 pounds. |
| Average minimum amount grain residue per car secured in any one sweeping test, 1 4-9 pounds. |
| Grand average of all cars swept, all tests, 2 3-25 pounds. |

(5) Based on all the information and data gathered by, and submitted to your committee (a) It would appear that what one unloader can, and does accomplish in cleaning out cars at unloading hoppers could, under like conditions, be accomplished by all unloaders.

(b) It would seem that the volume of business handled (night or day) should not, materially, affect the average amount of grain residue left in cars by unloaders.

(c) And further, it would appear that sweeping tests, such as are conducted by some Weighing Departments, weekly or biweekly, or periodically, should tend to create greater interest in the cleaning out of cars at unloading hoppers; and that, therefore, such resweeping tests could well be a part of the service of terminal grain weighing departments.

Finally, we would respectfully suggest that it might be interesting to all concerned if the Secretary would issue a printed, or mimeographed, circular containing a copy of this report, together with copy of the sweeping regulations of the Weighmasters' Scale Men's Conference for distribution by Weighing Departments, or others wishing to do so. And, for

[Continued on page 478.]

Seeds

MILTON, IND., Sept. 28.—Some wheat and clover seed moving, but other grain is slow.—Milton Grain Co.

FRESNO, CAL.—The Certified Seed Growers of California have been incorporated for \$200,000 to grow and market seeds.

GALVA, ILL.—G. Swank of Champaign, purchased the Glidden Bros. Seed House here and will manage the business himself.

ST. ANTHONY, IDA.—The John Allen Seed Co. opened its plant Sept. 2, with a force of 150 men and women to take care of the heavy yield of seed.

A MEETING was held in Columbus, Ohio, by committees from the Ohio Seed Improvement Ass'n and the Ohio Farm Bureau, with the purpose of establishing a seed exchange. Preliminary plans were made for a pure seed exchange to be operated on a co-operative basis.

COLLINSVILLE, ILL.—The Tiedemann Milling Co. rejected all orders for seed wheat from farmers, due to the failure of the seed wheat they received to meet specifications. The seed had in some way lost its germinating powers and rather than cause a small yield to farmers planting it, the company elected to cancel all orders.

LAMONI, IA.—J. W. Barr will build a seed and feed plant here. The building will be of studded construction and covered with iron, 3-ply roofing. Equipment will consist of 1, 5, 10 and 15-h.p. motors, 6-ton Fairbanks dump scale, distributors, 24-inch attrition mill, sheller and crusher. The Younglove Construction Co. has the contract.

DIRECTORS of the Toledo Produce Exchange approved a change in the rule regarding prime timothy, Sept. 29. The new rule will allow 35% hulled timothy in prime instead of 25%. Members will vote on the matter Oct. 9 and if passed the rule will be in new contracts from that day on. Contracts made prior to that day will be settled according to rules of prime in effect at time contract was made.

CHICAGO, ILL.—The scale of prices for seed sampling and inspection has been changed by the Chicago Board of Trade. Bulk seed inspection remains the same at \$1 per car; but \$1 extra will be charged for each bulkhead. The minimum charge for sampling has been raised from 50 to 75 cents. In bags the charge will be reduced to 1½ cents per bag, instead of the former sliding scale of 2 to 5 cents per bag. For bulk seed loading into or out of boats the charge will be 12½¢ per 1,000 lbs.

WASHINGTON, D. C., Oct. 9.—The Crop Reporting Board of the Bureau of Agricultural Economics makes the following estimates of the condition of crops: buckwheat, 83.8, compared with 87.4 last year and 79.9 average for 10 years; clover seed, 81.5, compared with 73.4 last year and 73.1 10-year average; field peas, 75.0, compared with 72.5 last year and 76.4 for 10-year average; flax, 82.6, compared with 66.8 last year and 71.1 for 10-year average; grain sorghums, 64.9, compared with 85.3 last year and 77.0 for 10-year average.

MINNEAPOLIS, MINN.—The explanation of light shipments of flaxseed to the U. S. may be seen in the late quotations of Argentine seed c. i. f. New York: October, \$2.09 to \$2.15; November, \$2.03; December, \$1.94; January, \$1.87; February, \$1.82. Within a month October could have been bought for \$1.90 and February \$1.70. With this sharp decline in futures, American crushers have not been buying prompt shipments freely, and oil will probably be scarce this winter in the East, and while February seed makes cheap April oil, with winter feeding season past, we will probably have the usual sharp decline in cake in March, increasing oil cost.—Archer-Daniels Linseed Co.

PINE RIVER, MINN.—The H. I. Davey Seed Co. is now established in the new building it recently built. It is equipped with new cleaning machinery.

TOLEDO, O.—Clover continues strong, scoring a sharp advance again this week. Trade more active. Fresh hedging sales and profit taking are in evidence, but offerings are readily absorbed. Better cash demand, with country offerings light. Unless receipts increase, there doesn't seem any likelihood of permanent setbacks. The future trend of the market will depend largely upon the attitude of the farmer to dispose of his seed around present level. Toledo usually acts as host to a large amount of seed. Receipts to date have been heavy, but not burdensome. Clover moved early this year. Will receipts increase? Considerable seed is yet to be hulled. Most of the receipts have been purchased by seed houses and already sold or hedged. A few hundred bags delivered on October contracts this week. Some are accepting delivery, expecting to ship the seed. Many October longs and shorts have traded over to the more distant futures, which are selling at full carrying charge. Receipts this week 1600 bags. Shipments 113. Total receipts season to date 7170 bags, compared with 8610 last year.—Southworth & Co.

TOLEDO, O.—The old saying that a "clover crop is never made until the seed is in the bag" is true of the crop this year. The brilliant prospects early did not materialize. Yields in many sections disappointing. Farmers have been holding. Short interest, who sold freely on scenery, have been badly punished. March seed touched a new high this week at \$12 or nearly three dollars a bushel up from the bottom. Hedgers have been the principal sellers. Long interest appears confident that market will touch \$14.00 sometime on the crop. Low grades have been in excellent demand with many buyers bidding for the small offerings. Some export demand in evidenced. Tariff of four cents a pound will restrict imports. Timothy market firm. Shorts have had trouble in filling contracts account of rule that prime timothy could not contain more than 25 per cent of hulled seed. New rule which allows 35 per cent hulled seed in prime will be voted on Monday. If passed Toledo will start trading in new contracts of timothy Tuesday. All contracts made before the passing of the new rule will require prime seed graded under the old rule to make delivery. Farmers have sold the bulk of their timothy and fresh offerings are light. Cash demand fair.—C. A. King & Co.

GREECE has ordered 666,000 bus. of Manitoba wheat thru New York brokers.

Receipts and Shipments of Seeds.

Receipts and shipments of seeds at the various markets during September, compared with September, 1921, were as follows:

| FLAXSEED. | | | | |
|--------------------|-----------|------------|-----------|-----------|
| Receipts— | | Shipments— | | |
| | 1922. | 1921. | 1922. | 1921. |
| Chicago, bus. | 47,000 | 169,000 | 189,568 | 314,078 |
| Duluth, bus. | 515,112 | 409,293 | 2,902 | 4,065 |
| Milw'kee, bus. | 51,450 | 20,700 | | |
| Minneapolis, bus. | 942,150 | 545,330 | 183,420 | 197,260 |
| Ft. William, bus. | 23,512 | 58,561 | 80,370 | 429,026 |
| N. York, bus. | 203,000 | | | |
| Winnipeg, bus. | 57,200 | | | |
| CLOVER. | | | | |
| Chicago, lbs. | 1,358,000 | 739,000 | 547,000 | 371,000 |
| Milw'kee, lbs. | 112,885 | 215,185 | 138,742 | 1,000,524 |
| N. York, bags | ..... | ..... | 798 | ..... |
| Toledo, bags. | 5,271 | 6,785 | 700 | 379 |
| TIMOTHY. | | | | |
| Chicago, lbs. | 9,593,000 | 6,239,000 | 6,303,000 | 8,567,000 |
| Milw'kee, lbs. | 1,254,295 | 1,565,532 | 50,608 | 87,603 |
| Toledo, bags. | 6,621 | 4,824 | 3,515 | 2,413 |
| OTHER GRASS SEEDS. | | | | |
| Chicago, lbs. | 6,164,000 | 2,678,000 | 570,000 | 1,128,000 |
| Toledo, bags. | 669 | 1,418 | 292 | 56 |
| KAFIR AND MILO. | | | | |
| Kansas City, bus. | 35,200 | 119,900 | 32,000 | 211,000 |
| Wichita, bus. | ..... | 4,800 | ..... | 4,800 |

26th Annual Convention of G. D. N. Ass'n.

The 26th annual meeting of the Grain Dealers National Ass'n was called to order at 10:30 a. m., Monday, Oct. 2, in the Grunewald Hotel, New Orleans, by Pres. B. E. Clement of Waco, Texas, with about 150 delegates present.

Rev. R. H. Harper of the Methodist Church South, invoked divine guidance.

J. R. Murrell, Jr., of Cedar Rapids, Ia., opened the morning session by leading the delegates in the singing of "America," "There's a Long, Long Trail," and "I'm a Little Prairie Flower."

Hon. Andrew J. McShane, Mayor of New Orleans, in welcoming the visitors, referred to the docks of the city, second to none in their facilities; the financial preparations made to take care of any future necessities. "I do not know the equal of our Public Belt Railroad. We are confident New Orleans is destined to be the first port in the United States. Our people are sincerely glad you are here with them and I hope that some of you will remain permanently with us or revisit our city; and I can assure you the latchstring will be outside the door."

Hon. John M. Parker, Governor of Louisiana, welcomed the dealers on behalf of the state: It is a pleasure to have come 130 miles from Baton Rouge this morning by machine to welcome you.

This state has the world's largest sulphur mines, the largest salt mines, the largest standing cypress forests, is the third state in the production of oil, 130,000 barrels a day. We have the safest harbor. We have the largest oil refinery in the world, employing 6,000 men.

One of your prominent grain men is a member of the Board of Commissioners of the Port of New Orleans; and no politics is allowed to interfere with the development of the port.

See to it that you take an interest in public affairs. Most of the business men of the country pay no attention to public affairs. They are to blame for the conditions that confront us.

Seventy-five per cent of the population of New York City is not of American birth. I am deeply impressed by a statement of President U. S. Grant that we will have to look to the West for preservation from the scum of Europe.

Thomas F. Cunningham, President of the New Orleans Board of Trade, followed: "It is a great pleasure for me to extend the sincerest and warmest welcome it is possible for me to voice.

Long before the great sweep of the inland empire was known as the granary of the world, New Orleans was known as a grain market. Men brought their corn and wheat down our magnificent river system in flat boats for transfer here into ships for transport to all the hungry people of the world. We are proud of our transportation system in the United States, but we find ourselves still feebly endeavoring to take care of the tremendous business of this vast nation.

Business has always outgrown transportation facilities. Men like the late James J. Hill have mentioned vast sums which must be invested to move the growing traffic. These sums were not provided during the war. Our canals were neglected. Rude and unfinished and relative small as it is, the Mississippi-Warrior River improvement has fired our imagination. The barge line has this year carried 650,000 tons of freight and saved \$850,000 in freight charges.

We are here to take counsel on how to move and market the grain crops. We should stimulate Congress to provide in full for both rail and river transportation.

Let me extend to you the warmest and sincerest welcome."

F. G. Horner, of Lawrenceville, Ill., in responding to the addresses of welcome, said:

Response to Addresses of Welcome.

The privilege of replying to an address of welcome must always be a most pleasant task, for the man is cold indeed who does not respond with an increased tingle of pride and satisfaction to an expression of a spirit of welcome, and my assignment today is made especially pleasant by the cordiality of our reception to this wonder city and, though my words fail to express our gratification at the warmth and sincerity of this greeting, I wish to assure our hosts that it finds reflection in the heart of every member of the Ass'n.

We must not forget that we are in the Queen City of our wonderful Southland, where hospitality is inherent, where it permeates the very atmosphere, and rolls from the tongue with an insidious softness that lulls us into a state of bliss entirely foreign to the experience of our craft.

Middleman's Lot Unhappy.—I believe that I am violating no confidence when I say that the lot of the middleman has not been either a happy or a profitable one. We, ourselves, know that the grain trade of the United States is now and has been performing functions necessary for the welfare of the entire world, and performing them with an efficiency unequalled in the history of distribution. Yet we now find ourselves in the unfortunate position of being the target for the abuse of both producer and consumer—verily, middlemen in the most undesirable sense of the term. We have often subjected ourselves to the most critical self-analysis and while we have found that human limitations have not permitted to us the perfection which we have desired, we have had to conclude that the recent criticisms of our trade have been not only undeserved, but manifestly unfair. They have emanated partly from ignorance and partly from cupidity, but more largely from a world wide epidemic of crooked cross-thinking, which would nonchalantly repeal all natural laws by the simple process of man-made decrees. And furthermore we are in the midst of a reign of ill-advised selfishness. Ill-advised because our existence has become so complex that no minority can long profit at the expense of the majority without suffering therefrom the inevitable reaction of their own acts. This country was built by construction and it can not be continued by destruction.

Building Up Autocracies Within the Nation.—As a nation, we seem unable to grasp the most patent elemental principles necessary for our existence and, paradoxical as it seems, while we are now living in the very presence of the utter collapse of such foreign nations as North Dakota and Russia and the prostration of our own country by preferential legislation which has built up an oligarchy of labor, we are now confronted by a larger minority, compounding a felony, if you will, and aggravating their own difficulties, by an attempt to build up another more powerful autocracy.

How simple, if they would only unselfishly join hands with the expectant mass of right thinking citizens in repealing the preferential legislation which has made such a predicament possible and thereby insure a natural and orderly return to a state of legal and rightful existence.

The Herrin Massacre.—I may, perhaps, be especially impressed with these conditions from the fact that I come from a State which is hanging its head in shame from a massacre which would arouse the envy of the blood-thirsty hordes of the Turks or Kurds. But, gentlemen, I would ask you not to be too free to criticize your sister State, whose name, I will, in charity, refrain from mentioning, but remember that the same fires are kindled all over the land and only lack the spark of ignition for a repetition of the unpunished, damnable butchery of Herrin. The unfortunate victims who went to their death while honestly working for the sustenance of themselves and their dependents are now beyond human succor but of immediate and vital concern to those of us who remain is the menace presented by the existence of a condition which makes such wholesale human slaughter possible and by the frequency with which we find the actual crime condoned and the contributing factors thereto sustained by otherwise law-abiding citizens.

If this country is to continue; if civilization, itself, is to survive, we must have a return to fundamental thinking and a wholesome respect and observance of the Golden Rule, and a thorough realization that this United States exists and will continue to exist for the protection and welfare of each and every citizen and not as an

instrumentality for the aggrandizement of the Labor Group, the Farm Bloc, or Special Business Interests.

We are especially gratified by our welcome to New Orleans because it is so fitting to the traditions of this great metropolis and because it typifies that unselfishness, that regard for the rights and welfare of our fellow citizens which must be the basis for the reconstruction of our national moral fabric. I hope that I may be pardoned for a spirit of pride when I say that if the United States is to continue on a plane above the nations which have already sunk into the depths of anarchy and chaos, that it will be done by giving heed to the example and teachings of such constructive organizations as the Grain Dealers National Ass'n, which has so consistently and so unselfishly stood for every principle of righteousness in our public and business life.

We are not proud and we are not glad that we are martyrs to this paroxysm of ignorance, cupidity and hate; but we are proud and we are glad that we belong to an organization which has the vision to discern the truth and the courage to spend its might for right.

H. B. Dorsey, Ft. Worth, Tex., urged that members endeavor to increase the attendance at the sessions.

Sec'y. Chas. Quinn announced that the mixed feed manufacturers had opened an office for the convenience of the delegates at 27 Board of Trade.

Pres. B. E. Clement delivered his annual address, from which we take the following:

President's Address.

We are assembled for the twenty-sixth time in annual convention, and for the second time in the history of our organization we are gathered in this great and typical southern city, here to deliberate upon questions affecting the trade, pass judgment upon the performance of your officers and committees, and to declare your wishes and outline your purposes for the next twelve months, give your mandates and select those to whom shall be committed those tasks that naturally and inevitably fall to those whom you honor with your call to service.

Fortunately our annual conventions are not humdrum affairs, so full of the serious and so devoted to portentous events, that there is no time for those amenities and social diversions so necessary for the complete enjoyment that the right kind of a convention is capable of affording.

Happy indeed am I to be able to proclaim, after the two years I have been privileged to serve as your President, filled as these years have been with strenuous activities on the part of enemies of the grain trade, that the Grain Dealers National Ass'n has so conducted its activities, sustained and supported by a loyal membership, that today the Ass'n stands out as one of the foremost commercial organizations of America.

It has justified its existence through its ability to defend its membership, their organizations, the economic necessity and value of their individual and collective service from attacks of both the agitator and the politician, the one bent on supplanting the competitive system with a great aggrarian trust; the other seeking blindly and ignorantly our destruction only as a means of political preferment.

The U. S. G. G. Incorp.: The opposition to the competitive grain trade of America was crystallized by the agitator under the banner of the U. S. Grain Growers, Incorporated, an organization fathered and supported and encouraged, contributed to, wet-nursed and advertised and praised by the Agricultural Department of the United States Government, which finally stood in the wings and tried to speak its lines to it while it stood upon the stage quivering in every limb from a vertigo caused by the economic vagaries it was endeavoring to foist upon an unsuspecting public. A visionary and wholly incompetent management soon brought it to the rocks where it is now foundering, while those who profited by its short and expensive life are now seeking new fields for their activities.

The last I heard of the abortion itself was to the effect that the Agricultural Department

was still endeavoring to incubate the embryo while a few of its sympathizers (those who had made loans to it) were patriotically (?) endeavoring to reorganize it, appealing first to Baruch, then to Hoover and from them to anybody to lend their name and fame to the wonton, not in the hope of giving it new life or growth, but with the vain hope of being able to give it a semblance of respectability, while it proceeded to collect the thousands of ten dollar notes which was its hope for meeting its hundreds of thousands of dollars of obligations.

It is dead now; let the dead past bury its dead, but let us hope that those who may be solicited in future to contribute to the "Hope Chest" of such an unpromising creature will be constrained to refrain from numbering themselves among the easy.

During the winter of 1921-2 and the early spring months it was necessary for Sec'y Quinn and me to visit all of the Exchanges in the United States east of the Rocky Mountains. We made four different trips, visiting every market from Hutchinson, Kansas, on the west to New York on the east, from Fort Worth in the Southwest to Boston in the Northeast, and from Milwaukee in the North to New Orleans in the South. Our mission was everywhere successful, Sec'y Quinn making detailed report of the result of those trips. I am mentioning them here only for the purpose of testifying in this public way, and that this testimony may be made a matter of permanent record in the archives of the Ass'n, to the splendid co-operation that was given the Officers of the Grain Dealers National Ass'n, to the high character, to the honorable purpose, the universal fellowship and charming personality of the splendid men that we met in every market in America.

Nowhere did we meet narrow, provincial individuals, but everywhere we found the grain trade filled and running over with men of brains, character, honor, patriotism and skill, each typifying those qualities of courage and capacity so characteristic of grain men that your leaders will ever have cause for pride, but never need to apologize for you.

Commerce and business and industry instead of being regarded as a species of piracy wherein all the offenses against honesty and justice were committed, at one time, with impunity are now conducted upon a high plane with service as the watchword.

Railway rebating, commercial shortweighting, adulteration and misrepresentation are evils that have been consigned to the oblivion of the past.

Indifference to the welfare of the employee, disregard for his rights and disrespect for his services, have been replaced with almost universal, and in fact competitive, effort to demonstrate the finest appreciation of the rights and the needs of those who toil. This has been made possible because of an enlightened public conscience to which commercial organizations have contributed no less than the influence of the Church through the high ideals that have universally characterized their leaders, permeated the organizations and manifested itself in all the practical services rendered. Through these organizations bad men and bad methods have been prescribed.

Burdens of Taxation.

When I contemplate the marvelous progress, the stupendous achievements of America during the past half century and realize it is all the result of the profits that American citizens have been able, through their genius and enterprise, to acquire in all the multitudinous scientific, educational, professional, industrial, commercial and agricultural pursuits to which they have devoted their time and talents I cannot but feel indignant and resentful at the disposition of men in high places to denounce all men who make big money as profiteers, ranking them as enemies of the country and endeavoring to levy special tribute upon them through unjust and inequitable taxation. This is nothing more nor less than a species of socialism. It is axiomatic that the power to tax is the power to destroy.

The income tax is a wise and efficacious method of raising revenue for the support of the federal government. It, however, is the most dangerous system of raising revenue that has ever been devised. It is a power too easily capable of abuse and misuse. It conduces, as has already been demonstrated, to governmental extravagance. It affords the politician the power and offers him the temptation to discriminate, as has already been done, in laying the burdens of taxation.

How often do we hear the politician say that

the burdens of taxation ought to be placed upon those who are best able to bear them? That is a socialistic doctrine that has been made use of to justify the discriminatory practice of a graduated tax on incomes. The rate of taxation should be the same whether a man's income is a thousand, a million or tens of millions. No government has the inherent right to say to one class of its citizens your income is so large that the government must have sixty or eighty percent of it. The only theory on which such a practice on the part of government can be right is that brains are a handicap, that good fortune should be penalized, that confiscation is commendable, that the rewards of industry accrue not to the industrious, but to the power that taxes.

The difference between such a practice by the United States Government and the socialistic Russian government is only one of degree. There the State owns everything and the people are supposed to toil for the common good, while here private property still exists, but those who are wise enough to be frugal, industrious enough to toil, courageous enough to take a chance, must have their rewards converted to the common good through a system of taxation that is confiscatory.

If a graduated income tax is not resented for its injustice it should be attacked because of the fact that it is uneconomic; it is destructive; it is paralyzing to the hopes and discouraging to the ambitions of young men.

Radicalism is growing in America. It is lamentable but it is true. Government paternalism, both state and national, is growing apace. While business has grown clean and constructive our politics have grown degenerate and contemptible. The paternalism in government and the radicalism permeating our national life today are the direct result of electing small men to high office; men who are guided and controlled by expediency; men to whom the call of the rabble is the voice of duty; men who, if they ever read the constitution, did so for information and not for guidance; men who, though they may know the fundamental principles upon which this government was founded, either lack the courage or patriotism or both to stand up and defend them.

These are days of experimentation with our government. An astonishing and incomprehensible thing when we contemplate the fact that for seven thousand years mankind struggled with various forms of government, during which one historian tells us that the pendulum of government was swinging back and forth from one extreme to the other, from the mob to the monarch; from the monarch to democracy; from democracy to the demagogue; from feudalism to communism and from bondage to license and, that during all those thousands of years there was no government that secured for its people religious freedom, civil liberty, freedom of speech, freedom of the press, security of individual rights, popular education and universal franchise. The constitution of these United States gave us all of these and marked a new epoch in the history of the human race. By this document the rights of man were segregated and defined and governmental machinery created and empowered to preserve and perpetuate those rights.

These difficulties the fathers foresaw, and provided against, by the terms of the Constitution, have arisen and instead of adhering to that Constitution amendments have been sought and obtained at the hands of the American people. The fathers foresaw the grave dangers and the probable injustice of direct taxation so they provided in Article One, Section Nine, Paragraph Four of the Constitution, that no direct tax should be laid against the citizens of the United States.

But our modern statesmen in 1913 secured the sixteenth amendment to the Constitution which authorized the income tax and that amendment was so worded that the Federal Government can tax the citizen one hundred per cent of his income if it chooses. If that amendment had included a provision that income tax, when levied, should apply at the same rate upon the incomes of every citizen there would have been protection against discrimination, injustice and wanton extravagance. There would have been retained in the people power to control the taxing proclivities of Congress. Excessive rates of taxation, extravagant expenditures of public moneys would have had quick rebuke at the hands of the people.

Under the present system the great mass of American voters are taught to believe, and it is true, that the great corporations, big finan-

ciers, capitalists and commercial enterprises are bearing the burdens of Government on a discriminatory basis while the masses are either not taxed at all or only nominally. The numerical strength of those with large incomes is so small that they cannot control their fortunes with their ballot or protect their interests at the polls, while those who pay no tax are so numerous and their interests so opposed to those who have large incomes that naturally they view with complacency the socialistic tax scheme that takes from the rich, while not touching them at all.

On this one amendment alone rests the governmental paternalism practiced by the federal government to-day. It is only since 1913 that government bureaus, commissions and inspectors have increased at such an alarming rate until now there are more than a hundred commissions, bureaus and boards, employing about thirty thousand agents and inspectors and spending, according to Senator King of Utah, the stupendous sum of seven and a half billions of dollars during the past ten years.

The power on the part of Congress to assess direct and discriminatory taxation gives the federal government a source of revenue far beyond the needs of a government honestly and economically administered, confining itself to the proper functions of government, therefore it is perfectly natural for it to engage in paternalistic enterprises. Perfectly natural therefore that it should invade the domain of states' rights and seek to bribe the state from its sovereignty by federal aid in building roads; by contributions to state health departments under provisions of the new federal maternity bill which appropriates millions of dollars to be distributed among the several states, where the states will match the federal appropriation and give the federal government the power permanently to maintain its agents in the state vested with the authority to come into the most intimate relations of the home. How wonderfully sympathetic and solicitous is this new and extraordinary power that it must project its benign influence across the continent, through the shield of state sovereignty, pass the portals and into the privacy of the American home to see with its own eyes what the procreative powers of its creatures are producing. This erstwhile servant of the people has now grown to regard itself as the master, the patron of its former sovereign—the citizen.

No patriotic citizen enjoys the idea, though every citizen should exercise the right, of criticising the government under which we live



B. E. Clement, Waco, Tex.,
Retiring President.

and which we love, but I frankly confess my conviction that there is more justification, in fact necessity, for criticising our federal government to-day than ever before in its history. The departures from the principles upon which this government was founded are so persistent, so glaring and so destructive that one is almost constrained to conclude that most of our public men are imbued with the idea that the Constitution was a series of mistakes that are to be corrected or ignored in these modern times instead of being regarded as Gladstone said as the "Greatest document ever conceived by the brain and purpose of man."

Government in Business.

Ours is a government of limited authority and power. The Constitution defines that authority and prescribes the power of the federal government. There is not one line, nay, not even a word in that Constitution that gives the government the right or the power to engage in or control business. Yet there has been introduced in Congress during the past four years legislation seeking to project this government into the commerce of this nation, to have, if adopted, changed this from a nation of competitive individualism wherein the citizen is the sovereign to a nation of servants doing the will, and serving in the place indicated to him by some bureaucrat.

As illustrating the fact that our public men are not alone in their departure from those principles upon which this government was founded I draw your attention to the testimony of Mr. Baruch before a congressional committee recently. His utter disregard for the limitations of this government was illustrated by his declaration that the federal government should build a system of warehouses over the United States for the storing of farm products. Such suggestions cause one to wonder what would be left for private enterprise and initiative if the government should embark in every enterprise that is proposed by those who neither understand nor appreciate the spirit and purpose and function of government. As it is, the practice of passing regulatory measures designed to put business under the control and domination of some department of the government is growing and new excuses are being found daily for further political control and domination of business.

I should like to see this convention go on record, through a dignified and impressive resolution, as opposed to further regulation, control, dictation, supervision or inquisitorial authority, by or on behalf of the federal government through the instrumentality of any department, bureau, board or committee; but let us insist that if there is now or hereafter any wrong or threat of any kind against the rights and interests of American citizens that laws be passed that are definite and unequivocal, the willing and intelligent obedience to which will insure both security and tranquility. A horde of petty officeholders set up with inquisitorial and dictatorial powers, backed by federal authority, is a menace to liberty, a threat to security, a damper upon individual initiative, an incentive to graft—a degrading and debasing influence to which no free-born American should be subjected.

The farm bloc is another tendency and alignment on the part of public men detrimental to the public weal. The farm bloc is a practical manifestation of the class spirit that is growing in America. There should be no such thing as class consciousness, class purpose or class aggrandizement. The whole spirit and purpose of America are opposed to class effort reflecting itself in class legislation.

The evil tendency of the class spirit manifesting itself at Washington found expression in a senate resolution requiring an investigation of the activities of the Grain Dealers' National Ass'n in the campaign it undertook as a result of the Cincinnati conference, held June 27th, 1921. At that time the attacks of the agrarian agitator, encouraged and abetted by the politician and many of the newspapers of the country, offered a threat to the grain trade and the grain exchanges that made it imperative for the grain trade of the nation to take action looking to its own protection. It was perfectly legitimate and proper for the Grain Dealers' Nat'l Ass'n to function as a disseminator of information calculated to enlighten the public before whom the trade was being tried and at whose hands its destruction was being sought. The investigation revealed a spirit, a purpose, a service and an ideal profoundly creditable to the grain trade and wholly disconcerting to its enemies.

Two matters of first importance in their re-

lation to the individual and collective interests of America: First, the foreign indebtedness to the American government, and second our merchant marine. I feel that it is altogether proper that a great national organization such as the Grain Dealers' National Ass'n should deliberate on these questions and express by resolution its convictions, for it is by such thought and action that the national views are eventually crystallized into definite action.

There has been considerable newspaper agitation of the foreign debt question, and I fear some propaganda seeking to convince America of the advisability of canceling the foreign debt. For my part, I am greatly opposed to the remission of any of this indebtedness. I am, however, heartily in favor of accommodating the settlement of whatever is due us by foreign nations to the needs and the peculiar conditions of each separate nation. I am, however, unwilling to give them a clean sheet. I think this indebtedness should be held against the foreign nations if it takes them a hundred years to pay it. It is no doubt a wise thing to even defer interest payments for as long a period as ten years until those nations can get on their feet and get to going again.

All the foreign nations indebted to us have, for the past century, been supporting constantly increasing armies and navies. If this government should remit their obligations I am sure these people would continue their military establishments on a more or less large scale. I think they might as well pay us as to maintain large military establishments that are nothing more or less than a menace to their own peace.

To my mind the simplest method of handling the indebtedness of foreign nations to the American government would be to accept capital investments in the foreign nations and by some means transfer these investments to private capital in America. This would do away with the necessity of either having a continued large influx of gold or the payment of the indebtedness due this nation by the shipment of goods, either of which method is admittedly dangerous. A continued influx of gold into America simply means that foreign currency and moneys will continue to depreciate, while certainly a large and continuous movement of goods and merchandise to liquidate the indebtedness of foreign nations to America would inevitably mean the breaking down of the various economic forces that sustain this nation, through the employment of both capital and labor. It is self-evident that we cannot accept the services and goods of foreign nations that owe America and at the same time keep employed the capital and labor of America in supplying the common needs of American citizens.

An investment, therefore, in foreign industries and enterprises of the billions that are due the United States, scattered among the several nations indebted to us in proportion to their indebtedness would eliminate both the menace to their finances and the menace to our economic structure. At the same time this would insure the payment of every dollar due America and give American citizens participating in taking over the capital investments a continuous income from the operation of foreign industries and enterprises. In other words, it would reverse the condition that existed prior to the war when tens of thousands of foreigners were drawing their sustenance in the shape of interest and dividends from the operation of American enterprises to a condition wherein American citizens and business men will be drawing dividends and returns from various enterprises in foreign countries.

Just a little common sense is all that is needed to effect such a settlement of the financial difficulties resulting from American government loans abroad during the war as will redound to both the glory and the good of this republic.

A repudiation of international obligations or a voluntary remission of same would be a most unfortunate example. It would encourage the loose-thinking everywhere in the world to look upon financial obligation as imposing no restraints, no sacrifices and no burdens whatever. Men would come to feel that as soon as they became involved in financial straits that might make it necessary for them to make sacrifices to meet their obligations that they would be thoroughly justified in "laying down" instead of cracking down. I can conceive of no greater disaster than that which would come from such an example. Governments that have heretofore held their obligations sacred should not, in the presence of Bolshevism, with

its threat of confiscation and repudiation, give encouragement and endorsement to their program by a resort to their methods. Besides, why should the American people assume tax burdens and place upon the backs of their children such burdens in order to relieve the foreigner?

It would be well to consider the facts concerning the ability of foreign nations to meet their obligations to the United States. Seven nations exclusive of Great Britain are indebted to the United States \$5,161,000,000. These seven nations include Belgium, France, Greece, Italy, Roumania, Servia and Czecho-Slovakia, which countries have a combined population of 125,700,000, indicating a per capita indebtedness to the United States of only \$40. The total present indebtedness of these seven countries is approximately \$81,000,000,000. If Germany is finally made to pay to France as much as \$25,000,000,000 of reparation instead of the \$56,000,000,000 now being demanded, this would leave a total indebtedness of only \$56,000,000,000 for the seven countries indebted to the United States, or a total per capita indebtedness of only \$448. The per capita indebtedness of the United States is at present about \$215. The present indebtedness of Germany is \$71,000,000,000 and to add to that \$25,000,000,000 that she will probably be compelled to pay in reparation would give Germany a total indebtedness of \$96,000,000,000, or a per capita indebtedness of \$1,477. If the per capita indebtedness of Germany with the reparations fixed at \$25,000,000,000 runs as high as \$1,477 there certainly can be no logical reason why the seven nations indebted to the United States should not pay this government what they are due it when their total per capita indebtedness is only \$448.

As for Great Britain, which nation is indebted to the United States \$4,277,000,000, there is no question but what this money can be paid and no argument should be entertained that would abate one dollar of it, not that I do not feel the greatest sympathy and friendliness for the British government, but because Great Britain doesn't need any charity and is amply able to meet her obligations to the entire world.

Merchant Marine.

There is no question but what America should have an adequate merchant marine and to my mind there is no question but what the merchant marine should be operated under the competitive system by private capital. Our maritime laws should be so amended that those who operate the merchant marine can do so under competitive conditions with any nation on earth. A system of subsidies is a dangerous one and could easily lead to graft, but with proper safeguards if it should become absolutely necessary to maintain the American flag on the seas a subsidy should be provided. The committee on merchant marine will, I am sure, give us an enlightening report that will better enable us to pass intelligently upon this question so that whatever resolutions are formulated may be fairly representative of the views of the business interests of the nation.

Chas. D. Jones of Nashville, Tenn., moved that extra copies of Pres. Clement's address be printed and sent to the members of the



Pres. Elect. F. E. Watkins,
Cleveland, O.

federal Congress and the 48 state legislatures; and on motion by Mr. Dorsey that his remarks on remission of the debts of foreign nations be made the expression of the ass'n, it was carried.

Pres. Clement named the following com'tes:

RESOLUTIONS: E. C. Eikenberry, Camden, O.; W. A. Hottensen, Milwaukee, Wis.; C. E. Blewett, Fort Worth, Tex.; G. B. Wood, Buffalo, N. Y.; H. I. Baldwin, Decatur, Ill.; Roger P. Annan, St. Louis, Mo.; T. W. Hall, Minneapolis, Minn.; Geo. A. Wells, Des Moines, Ia.; H. B. Dorsey, Fort Worth, Tex.

AUDITING: Tom Connally, Clarendon, Tex.; R. W. Hale, Nashville, Tenn., and Geo. E. Booth, Chicago, Ill.

NOMINATIONS: E. M. Wayne, Delavan, Ill.; J. A. Bishop, Houston, Tex.; O. P. Hall, Greenville, O.; Geo. F. Powell, St. Louis, Mo.; E. M. Combs, Chicago, Ill.; C. W. Wright, Omaha, Neb.; J. J. Rammacher, Buffalo, N. Y.

Sec'y Quinn read his annual report as sec'y, from which we take the following:

Report of Secretary.

In presenting my ninth annual report as secretary-treasurer of your Association I shall begin by craving your indulgence for what may seem to be its great length. While the other officers of the Association will doubtless cover some of the ground traversed in this report, no one of them is called upon to give you a complete picture of the Association's activities during the last year. It devolves upon the secretary, who is the nerve-center of the organization, as it were, to touch upon the work of all the committees.

The Mansfield Committee.

At the Chicago convention it was apparent that the bizarre scheme of the United States Grain Growers, Inc., was about to end in failure and the directors of the Association decided to close the Chicago office and transfer its affairs to the headquarters at Toledo. At the time this was done the Special Executive Committee (known as the Mansfield Committee) had obligations outstanding to the amount of \$26,198.22. The expenses of the committee were \$71,144.56, while there had been raised by subscriptions from the grain exchanges a total of \$44,946.34.

It then devolved upon President Clement and your secretary to raise sufficient money not only to pay off the obligations of the Mansfield committee, but enough in addition to create a fund of \$30,000 or \$40,000 to continue for another year the work of educating the producers to the economic value of the middleman. The directors at the Chicago convention had voted to create this fund for educational purposes.

President Clement and your secretary paid a visit to the following grain exchanges, at each one of which we addressed either the exchange members as a body or their board of directors: Fort Worth, Oklahoma City, Enid, Salina, Hutchinson, Wichita, St. Joseph, Omaha, St. Louis, Kansas City, Milwaukee, Chicago, New Orleans, Memphis, Nashville, Louisville, Cincinnati, Indianapolis, Buffalo, Boston, New York, Philadelphia and Baltimore.

Through the efforts of your officers the financial affairs of the Mansfield Committee were liquidated, and there is in the fund of this Special Executive Committee the sum of \$4,291.46.

The Ass'n found itself unable to do much more than wind up the affairs of the Mansfield committee. The effort to create a fund of \$30,000 or \$40,000 to continue the work for another year had to be abandoned because the members of the Association did not feel that further contributions were necessary in face of the fact that the U. S. Grain Growers, Inc., was in a hopeless condition financially and unable to market any grain.

The financial statement of the committee is briefly summarized here. A complete itemized report audited by a chartered accountant has been brought to the convention and may be seen by any one interested. The condensed statement follows:

RECEIPTS AND DISBURSEMENTS

Of the Special Legislative Fund for the Period from July 1, 1921, to Sept. 5, 1922.

RECEIPTS.

| | |
|---|-------------------|
| Contributions: | |
| Grain Exchanges and Boards of Trade | \$63,175.00 |
| Individual subscriptions from members of the Chicago Board of Trade | 7,225.00 |
| Miscellaneous individual subscriptions | 6,772.00 |
| Grain Dealers' National Ass'n | 500.00 |
| Indiana Grain Dealers' Ass'n | 250.00 |
| | <hr/> \$77,922.00 |

| | |
|---------------------------------------|-------------------|
| Sale of furniture, waste paper, etc. | 517.99 |
| Interest received on bank deposits... | 20.34 |
| | <hr/> \$78,460.33 |

DISBURSEMENTS.

| | |
|--------------------------------|-------------------|
| Advertising and publicity.... | \$44,351.35 |
| Postage and letter service.... | 9,634.38 |
| Salaries and wages..... | 6,799.77 |
| Office supplies and expense... | 4,477.55 |
| Traveling expenses | 3,570.18 |
| Speakers' expenses | 2,577.00 |
| Rent | 1,622.26 |
| Legislative expenses | 1,136.38 |
| | <hr/> \$74,168.87 |

Receipts in excess of disbursements

\$ 4,291.46

Now that the effort to supplant the middleman by the U. S. Grain Growers, Inc., has failed so ignobly it might be well to state, without show of pride, that the Mansfield Committee did splendid work in bringing to the attention of the producers the impracticability of the scheme that was proposed. No well informed person doubted the ultimate failure of the project, nevertheless the scheme would have been carried much further had not the regular grain trade taken a hand in showing the farmers the utter futility of supporting such a plan as the one proposed by the grain growers.

Deeply in Debt: As a result of the work of the Association through the Mansfield Committee the U. S. Grain Growers, Inc., is now in debt to the sum of \$285,267.27, notwithstanding that it collected more than \$600,000 from confiding agriculturists. About \$900,000, therefore, has disappeared with nothing to show for it but an expensive suite of office rooms in Chicago.

When it is remembered that less than \$75,000 was used by the Mansfield Committee in a nation-wide effort to combat this dangerous scheme, the work of the committee stands out in bold relief. It was money well spent.

The amount expended by the committee was a great disappointment to the members of the agricultural "Bloc" in congress. The leaders of this bloc in the senate forced through a resolution to investigate the alleged "slush fund raised by the Ass'n to defeat co-operation." Senator Kenyon, of Iowa, introduced this resolution before he retired from the senate to go on the federal bench. A committee of three senators was appointed to conduct the investigation and when it was shown that the Ass'n had not raised \$250,000, but only about one-quarter of that sum, interest in the proceedings began to wane.

The representatives of the Ass'n acquitted themselves creditably throughout the hearings which began on Jan. 23 and did not terminate until the middle of June. There were several hearings, each lasting two or three days, with interruptions of several weeks. These hearings became more farcical as they progressed.

The Ass'n was not on the defensive at any time. It took the position before the senate that the grain dealers of the nation as American citizens had a constitutional right to defend their business from the misrepresentations of the farm leaders who were seeking not the interests of the producers but that of their own. A complete statement of the receipts and disbursements of the Mansfield Committee was filed with the senate and it has been published in the printed record of the hearings.

The failure of the co-operative movement was due not only to the inherent weakness of the scheme proposed but to general conditions. The troubles of the farmer, real as they were, were not confined to him alone. The whole business world was and still is suffering. The middleman had nothing to do with the plight of the producer. All attempts to prove that he had only reacted upon the farm leaders when the truth finally became apparent to all. The collapse of the scheme only shows the futility of agitation that is not based upon truth and fact.

The agitation through which the trade has gone in the last two years will be regarded by the future historian with tolerant amusement. He will smile at the attempt of the farm leaders to lift up the producers of the country by their boot-straps. Economic conditions never can be remedied in that way, nor can they be improved by any effort to defeat the law of supply and demand. Both Canada and the United States are finding this out.

In the British Dominion to the north of us the farmers this year tried to re-establish compulsory wheat control through the agency of a wheat board similar to the one employed during the war. In the United States there is an attempt to maintain or raise the level of prices by a system of credits which will make it easier for producers to secure cash advances before the sale of their grain. Both plans will fail, because in each country there is a big exportable surplus which must be disposed of on a world level. This surplus will always control the price of the grain that is sold for domestic consumption.

Bills Proposed: For months the daily and weekly newspapers were full of sensational articles concerning the varied activities of the farm bloc. One would have thought, to read the daily journals, that congress was considering nothing but legislation for the relief of the

farmers. A great number of bills were introduced. There were bills to regulate future trading in grain, bills to "stabilize" prices of grain, bills to authorize associations of producers of agricultural products, bills to permit the organization of agricultural pools, bills to amend the War Finance Corporation Act and the Federal Farm Loan Act, bills to create rural credit societies, bills to provide capital for agricultural development, bills to provide a landschaft system of rural credits, bills to "encourage the development of the agricultural resources of the country through federal and state co-operation," bills to exempt interest on farm loan securities from taxation under the Revenue Act of 1921, bills to prevent the cornering of markets, bills to amend the Grain Standards Act, bills to allocate the control, administration and supervision of all functions of the government affecting agriculture, forestry and food to the Department of Agriculture, bills to provide for the purchase of farm products in the United States and to sell the same in foreign countries, ad infinitum.

The manifest purpose of most of these bills was to hand over government money to the farmer so that the producer would be enabled to hold his grain off the market at harvest time and thus to raise the natural level of prices. The grain trade did not offer any objection to these bills, futile as they are for the most part, because to object would be but to leave in the minds of the farmers the impression that the trade was opposing their legitimate aspirations. Nothing could, of course, be further from the truth than this. Anything that will help the farmer is bound to help the middleman because the interests of both are bound together notwithstanding anything the agitators may say to the contrary.

Of the many bills introduced by the farm bloc but few were enacted into law. Bills to amend the War Finance Corporation Act and the Farm Loan Act became laws as did a bill to legalize co-operative associations. These, with the Tinch-Capper bill, to regulate future trading on the grain exchanges, were all that ran the gauntlet of the house and senate.

The Tinch-Capper law, as all the members know, was declared unconstitutional by the United States supreme court, on the grounds, first, that it was an abuse of the federal taxing power, and second because the transactions on the Chicago Board of Trade were held by the



F. G. Horner, Lawrenceville, Ill.,
First Vice President.

court to be intrastate in character and hence beyond the authority of congress to regulate. It will thus be seen that after all the agitation, all the clamor, all the excitement, all the alarm, not much has been accomplished by the agricultural bloc. The danger, however, is not by any means over. The whole tendency of government at Washington is paternalistic. Bills by the bale to regulate every human activity have been introduced.

Transportation.

During the last year the Executive Committee of the Association found it necessary to discontinue the services of W. K. Vandiver, transportation commissioner, and return to the old system of using volunteer workers through the regular transportation committee.

This action of the executive committee was dictated partly by considerations of economy and partly by the fact that there is not enough work to keep a transportation expert constantly employed. It was thought when Mr. Vandiver was engaged that a transportation department headed by a paid expert, could be built up, but when this was tried, it developed that the Association could not create a field for such activity.

Few, if any, of the direct members of the Association made use of the department. This was doubtless because every market has a transportation commissioner of its own to whom the local exchange members look for relief for their transportation troubles. The affiliated members did not use the department freely either because of the fact that most of them have their own claim departments.

It has always been understood that the Association, being a national body, could not engage in rate disputes because of the interrelationship of the various markets. In nearly all rate questions there are conflicting market interests. The Ass'n cannot take sides in these controversies without antagonizing some interests within the organization, hence their rate disputes are left to the various markets to fight out among themselves.

All of this, it will be seen, narrows the field of activity for a transportation commissioner. The only problems left with him are those of a national character that effect all of the members alike, such as general reduction or increase in rates, demurrage, reconsignment rules and charges, minimum carload weights, war tax problems, transit rules, charges for bulkheading, installation and maintenance of side tracks, and all problems of that nature.

In other words, the Association, it has been learned from experience, can grapple with only such questions as effect all grain shippers from coast to coast, and it cannot, from the very nature of the organization, engage in the solution of local or sectional problems.

When general business conditions improve an arrangement will doubtless be made between the association and some expert like Henry L. Goemann, whereby the problems of a purely national character may be handled upon a reasonable basis of compensation. Mr. Goemann kindly consented to become the active head of the transportation department upon the resignation of Mr. Vandiver and he has been doing the work since May 1 last, at which time Mr. Vandiver left the employ of this organization.

Telephone and Telegraph Service.

The decision of the Interstate Commerce Commission in the matter of the liability of telegraph companies for errors and for delay in the transmission of messages increased the liability of the wire companies from practically nothing to \$500 on unreported messages and \$5,000 on repeated messages.

The decision was given on May 3, 1921, and it was put into effect on July 13 of the same year. It was felt that a splendid victory had been achieved, but it now develops that the victory was not complete, because the telegraph companies inserted as one of the conditions on the back of their telegraph blanks that they would not be responsible for errors "in cipher or obscure messages."

As most of the messages sent by grain dealers are in code the wire companies, by the inclusion of these few simple words, destroyed much of the value of the commission's decision, so far as the grain trade is concerned.

Shortly after July 13 (when the decision of the commission went into effect) complaints began to reach the Association from members. These complaints were all of the same nature—the telegraph companies had refused to pay claims which involved cipher messages.

In one of the complaints a member of the Ass'n at Terre Haute, Ind., sent a telegram on July 21, 1921, in which the word "absolutely" was transmitted as "absolute." The first word means ten thousand and the second word five thousand. As a result of the error in transmission the Terre Haute firm had a dispute with a customer in Baltimore as to whether there was a sale of ten thousand or five thousand bushels. The Terre Haute shipper lost three cents a bushel on 5,000 bushels and made a claim against the telegraph company for \$150.

Another claim arose from the transmission of a telegram from Indianapolis to Philadelphia. The sender used the word "affording" which word read "afforded" when the wire was delivered. The first word means two cars while

the second one means one car. This caused the Indianapolis shipper a loss of \$192.12 and the telegraph company declined to pay.

These two illustrations will serve to show that trouble was experienced from the shippers almost immediately following the time when the decision of the commission went into effect. The two complaints cited are typical of a number of others received by the Association.

The matter was at once taken up with the Western Union, but the officials contended that the decision of the commission did not obligate the telegraph company to pay for errors in the transmission of cipher messages. They pointed out that this is one of the rules on the back of the telegraph blanks and that this makes it a part of the rate which must be as strictly observed as the rates themselves.

The Ass'n contends, of course, that there is nothing in the decision of the commission that absolves the telegraph company from liability for errors in cipher messages. A communication was addressed by the Association to Chairman McChord, of the Interstate Commerce Commission. He was asked if the wire companies were justified in excluding cipher messages from the provisions of the commission's decision. Mr. McChord replied:

"In the course of the hearing in the case some mention of cipher messages was made, but the active interest of the parties to the case was manifested principally in what may be called the ordinary unreported and repeated messages, and our finding was limited accordingly. Because of the absence of adequate evidence on the subject we did not disturb the telegraph company's rule disclaiming liability for 'errors in cipher or obscure messages.' In other words, our report in that case is silent on the subject of such messages. It is evident, therefore, that in the instances you cite the Western Union relies upon its rule and the absence of condemnation of it by us."

The wire companies have taken the position that the question of cipher or obscure messages was not decided by the commission and is still an open one.

It is evident that nothing can be done except by bringing a suit or by instituting a formal complaint before the commission. It would seem that the latter course is the logical one because the commission should complete its record by deciding this phase of the liability of telegraph companies. Mr. Goemann is in communication with the National Industrial Traffic League and other interested bodies and a joint complaint before the commission may be the result.

The Telephone and Telegraph Committee should be abolished, or that it be merged into the Transportation Committee. This is because the Interstate Commerce Commission now has jurisdiction over the telephone and telegraph companies in all interstate business. The Supreme Court of the United States has held that the wire companies are common carriers within the meaning of the interstate commerce act, hence the Association must go to the commission when seeking relief from its telephone and telegraph difficulties. All wire troubles of an intrastate character come under the jurisdiction of the various state utility commissions, and these troubles should be given to each state association to handle, leaving only the larger field for the National organization.

Trade Rules.

This organization is to be congratulated in having as the chairman of its trade rules committee F. E. Watkins, who is also first vice-president of the Ass'n. Mr. Watkins has done more to arouse interest in, and consequent study of the trade rules than any one who preceded him as chairman of this most important committee.

It must always be remembered, in dealing with trade rule amendments, that the rules are necessarily of a general nature and that they cannot be made to apply to purely local conditions which may be quite transitory. Changes in the rules should be made with great deliberation and only after the fullest discussion and consideration.

Proposed Changes in Feed Rules.

At the Chicago convention a year ago the Ass'n adopted new trade rules governing transactions in feedstuffs. These rules have been in effect now for twelve months and quite naturally some weaknesses have developed. Experience has shown that some of the rules require amendment. At this convention changes in the existing feed rules will be proposed by the feed dealers after the various amendments have been thoroughly discussed by them in a group meeting.

It is manifest that the method of handling amendments to the feed rules through the group meeting of feed dealers is the proper one. Grain dealers are not familiar enough with the conditions governing the handling of feed to pass upon amendments to feed rules.

It is desirable to have uniformity, but absolute uniformity cannot be realized because of the different conditions surrounding the handling of the two products.

It is the difference in the feed and grain business that made it necessary for the Ass'n at the last convention to create a feed com-

mittee to dispose of all disputes arising in feed transactions.

At the last annual meeting the Ass'n admitted to affiliated membership the Mutual Millers and Feed Dealers Ass'n of New York state and in March of this year the Eastern Federation of Feed Merchants was admitted to membership. This gives the Ass'n two affiliated bodies of feed dealers, making of the National Association a feed as well as a grain organization.

Uniform Grades.

The annual effort has been made by certain parties in the northwest to lower the standards for spring wheat. On March 4 last Secretary of Agriculture Wallace issued a circular proposing a number of changes in the wheat grades.

There was great commotion in the trade when a telegram came from Washington at the last moment announcing that some of the changes were to apply to winter wheat as well. Secretary Wallace was at once swamped with telegrams demanding that the trade be heard before any changes were made in the winter wheat grades. He received so many protests that he finally decided to give two hearings.

Representatives from the Ass'n attended these hearings at both of which strenuous objections were made to the inclusion of more moisture in all of the grades and in the change in foreign material other than dockage.

The politicians in the northwest who were backing the Steenerson bill to debase the present grades used all of the old stock arguments in the naive belief that the producer would in some mysterious way receive the same money for water that he now gets for sound grain.

It ought to be stated, as a matter of justice to the department of agriculture, that those in charge of grain supervision were opposed to making any material changes in the wheat grades. Their daily contact with the trade and with the actual grading of the grain taught them that the debasing of the wheat standards can lead only to trouble for the farmers.

The Secretary of Agriculture, Congressman Steenerson and others, after a meeting with the house committee on agriculture at Washington, came to an agreement and Mr. Steenerson withdrew his bill, which was known as H. R. 7401.

The final result of the Chicago and Kansas City hearings was the announcement of Secretary Wallace that some minor changes would be made in the white wheat grades and that grain inspectors would be required to place on all grade certificates more information concerning hard red spring and durum wheats. The changes in the inspection rules require the inspectors to state, "in addition to the grade of the wheat, adequate information as to the kind and quantity of foreign material other than dockage and the amount of moisture when effective as grading factors, and as to what would be the grade of the wheat otherwise than on account of these grading factors."

Constitution and By-Laws.

At the last annual convention the constitution and by-laws were revised after a committee of three appointed by President Clement to rearrange and codify them had completed its work. The revision has proven eminently satisfactory. The new constitution and by-laws,



Elmer Hutchinson, Arlington, Ind.,
Second Vice President.

which needed revision badly, have been brought up to date and the experience of the last twelve months has shown that they now respond to all reasonable requirements.

In the committee's revision an oversight is noted. Provision was not made for the creation of a committee on rejected applications. This is an important committee and each year it is called upon to do considerable work in investigating applicants against whose admission to the Association objection has been made by one or more members of the board of directors.

Arbitration.

The following is a detailed statement of the work of the seven arbitration committees and the arbitration appeals committee in the last year:

| | |
|--|------------|
| Number cases at beginning of year..... | 51 |
| Number new cases filed during year..... | 110 |
| Total | 161 |
| Number decisions during year..... | 49 |
| Cases withdrawn | 8 |
| Cases settled direct..... | 26 |
| Cases dismissed | 12 |
| Expulsions for refusing to arbitrate or for refusing to pay arbitration awards | 18 |
| Cases pending | 48 |

| | |
|---|------------|
| Total | 161 |
| Appeal cases pending at beginning of year | 10 |
| Cases appealed during year..... | 16 |

| | |
|----------------------------------|-----------|
| Total | 26 |
| Appeals decided during year..... | 18 |
| Appeal cases withdrawn..... | 5 |
| Appeal cases pending..... | 3 |

| | |
|---|-----------|
| Total | 26 |
| One hundred and ten members asked for arbitration as compared with 157 last year. | |

There are now pending 48 cases against 51 a year ago. On the appeal docket there are but three cases pending, while at the last annual meeting there were 10.

At the Chicago convention a year ago an arbitration committee to handle feed cases was created. This committee has done excellent work and has disposed of several cases during the year. The Association now has seven arbitration committees, counting the feed committee.

Eighteen expulsions for failure either to arbitrate disputes or for refusal to pay arbitration awards are reported since the last annual meeting. This number is five less than last year when twenty-three members were expelled.

It is evident that the country has not yet recovered from the demoralization of the war. In every business the same complaint is heard. Merchants do not seem longer to be bound by obligations once regarded as sacred. It is impossible to understand how men will sign a solemn pledge to arbitrate a trade difference and abide by the decision of the arbitrators only to regard such a promise lightly. They either do not know or care that their business honor is at stake. Eighteen expulsions in one year is a record that gives no cause for pride. Before the war the Ass'n had but two or three expulsions a year.

What is the cause of this unwholesome condition? Has the moral fibre of the members weakened, or has the Ass'n in recent years taken into its ranks many men who are not up to the required standard in probity and business honor? Have men lost their sense of right and wrong or is it but a passing phase of the aftermath of war? Why was it necessary to expel eighteen members last year when but a few years ago the expulsions were only two or three?

Each member may answer these questions for himself. The Association has taken in many new members in the last few years. The vast majority of them are of the best possible material for membership. They are leaders in their several communities and they have come through the trying times of the war reconstruction with great credit to themselves. Unhappily there were others enrolled about whose conduct the same cannot be said. And it is these others who have not measured up to the standards necessary in any trade organization that preaches business morality.

What is to be done with these unworthy members? Manifestly there is but one thing—expel them from the Association—and that is just what is now taking place. The Association is purging from its ranks these who cast nothing but discredit upon it.

Arbitration is the corner stone of the Association and it must be preserved at all hazards. Anything that threatens it endangers the very foundation of the organization. The eighteen members expelled since the last convention are:

| |
|--|
| The Paddock-Hodge Co., Toledo, Ohio. |
| Cecil Brokerage Co., Birmingham, Ala. |
| Nashville Grain & Feed Co., Nashville, Tenn. |
| Weurding Grain Co., Holland, Mich. |
| B. G. Estill Grain Co., Enid, Okla. |
| Kerr, Gifford & Co., Seattle, Wash. |
| Arnold A. Thurnau Grain & Feed Co., St. Louis, Mo. |
| C. L. Lee Grain & Elev. Co., Abilene, Texas. |
| F. C. Pew Company, Uniontown, Pa. |

Cobb Grain Co., Plainview, Texas.
C. E. Smith Co., Jacksonville, Fla.
Panhandle Grain & Elev. Co., Amarillo, Texas.
Woodson County Grain Co., Yates Centre, Kans.
James S. Tuttle, Oklahoma City, Okla.
T. L. Price & Co., Columbia, S. C.
Circleville Milling Co., Circleville, Ohio.
Bolin-Hall Grain Co., Liberal, Kans.
Dalhart Equity Exchange, Dalhart, Texas.

In the last annual report of your secretary mention was made of the suit brought against the Association by the Paddock-Hodge Co., of Toledo, Ohio, to restrain this organization from expelling it because of the refusal of the Ohio concern to pay an arbitration award given by Committee No. 3 in favor of the Park & Pollard Co., of Boston, Mass. At the time of the Chicago convention a year ago the case had been argued in the common pleas court and in the circuit court of appeals, in both of which courts the Association won. The injunction prayed for by the Paddock-Hodge Co. was denied in both tribunals.

We are pleased to inform the convention that the supreme court of Ohio refused to docket the case after a motion to certify had been argued before it. In taking this action the supreme court affirmed the decision of the court of appeals which, in effect, stated that the Paddock-Hodge Co. must either pay the award of the arbitration committee or stand expulsion.

The Ohio courts have, therefore, placed their stamp of approval upon the compulsory arbitration as conducted by the Ass'n. They have for all time settled the question as to the legal right of the Ass'n to expel any member who refuses to live up to the obligations which he voluntarily assumed when he joined.

Expelling members who violate section five of the Grain Standards Act. This section reads as follows:

"Whenever it is determined by the secretary of agriculture that any quantity of grain has been incorrectly certified to conform to a specified grade or has been sold, offered for sale or consigned for sale under any name, description or designation which is false or misleading, he may publish his findings."

There have been published periodically the findings of the Secretary of Agriculture in a number of cases where grain dealers have attempted deliberate fraud by changing grade certificates or by issuing invoices or other documents that stated the grade of the grain was higher than that actually shipped.

Section five, giving the Secretary of Agriculture authority to investigate such frauds, is an excellent one. It is to be regretted however, that the provision does not go further than the mere publication of the "findings." No penalty under section five is attached to such dishonesty except publication. Men who follow such practices should be punished in the same manner as those who commit any other kind of fraud.

A number of letters received from members urge that the Ass'n expel all those who violate section five of the Grain Standards Act. These members do not believe that such dishonest dealers should be permitted to remain in the organization after the Department of Agriculture has found them guilty of such fraud.

Under the present by-laws of the Ass'n a member can be expelled for but two causes—refusal to arbitrate or refusal to pay an arbitration award. Provision should be made in the by-laws for the automatic expulsion of all members found guilty by the Department of Agriculture of violations of section five of the Grain Standards Act. A new section should be added to Article IV of the by-laws to cover this matter.

Another question is the disposition of an arbitration dispute where the defendant admits that he owes the amount of the plaintiff's claim and pleads that he will pay it "some time," or when he "is able," or "when business conditions improve," etc.

There have been several cases of this character within the last year. Members have filed applications for arbitration and when the contracts were sent to the defendants for signature the latter admitted liability for the full amount of the claim, but pleaded that they were not financially able to meet the obligation. The question then arose as to what disposition should be made of the case. It was plain that there was nothing to arbitrate. Should a defendant be permitted to remain a member of the Ass'n and continue to do business with other members while he is unable or unwilling to meet his obligations? If the defendant shows some evidence of good faith by attempting to pay part of the claim and giving notes for the remainder he should, of course, be given every encouragement by the Ass'n and should not be expelled. If, on the other hand, he simply admits the claim but makes no effort to liquidate it he should be expelled.

Provision should be made for the discipline of such members who refuse to meet their honest obligations. Most of the directors believe that there is authority under existing rules to cover such cases but some of the other directors doubt this. The rules ought to be made so plain on this point that there could be no room for discussion.

Such cases might be handled by the incorporation in the arbitration rules of a provision covering what would correspond to a "confession of judgment" in a civil court so that, in cases where one party admits the debt, the case could be referred to one of the arbitration committees which committee could render a judgment and this judgment would be as binding as an ordinary arbitration decision.

Membership.

Direct and Associate

| | |
|---|--------------|
| Number direct and associate members reported at last convention | 1,937 |
| Direct and associate members secured since last convention..... | 143 |
| Total | 2,080 |
| Direct and associate members lost during year | 497 |
| Direct and associate members Oct. 1, 1922 | 1,583 |
| Direct and associate members in good standing Oct. 1, 1922..... | 1,476 |
| Number of delinquents..... | 107 |

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|--|--------------|
| Total | 1,583 |
| Net decrease in direct and associate members during the year, 354. | |

Direct Members

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|--|--------------|
| Direct members reported at last convention | 1,865 |
| Direct members secured since last convention | 141 |
| Total | 2,006 |
| Direct members lost during year.. | 478 |

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| Direct members Oct. 1, 1922..... | 1,533 |
| The losses of direct members were from the following causes: | |
| Resignations | 193 |
| Dropped for non-payment of dues | 90 |
| Gone out of business..... | 170 |
| Expelled | 18 |
| Rejected | 2 |

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|--|--------------|
| Total | 473 |
| Direct members in good standing Oct. 1, 1922 | 1,430 |
| Number of delinquents..... | 103 |
| Total | 1,533 |

Net decrease in direct members during the year, 332.

Associate Members

| | |
|---|-----------|
| Associate members reported at last convention | 72 |
| Associate members secured since last convention | 2 |
| Total | 74 |
| Associate members lost during year | 24 |

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|--|----|
| Associate members on Oct. 1, 1922 | 50 |
| Associate members in good standing Oct. 1, 1922..... | 46 |
| Number of delinquents..... | 4 |

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|--|-----------|
| Total | 50 |
| Net decrease in associate members during the year, 22. | |

Affiliated Members

| | |
|--|-------|
| Affiliated members reported Oct., 1921 | 2,624 |
| Affiliated members Oct. 1, 1922..... | 2,562 |

| | |
|---|-----------|
| Decrease | 62 |
| Total number of direct, associate and affiliated members reported at the last convention..... | 4,561 |
| Total number of direct, associate and affiliated members Oct. 1, 1922 | 4,145 |

| | |
|-----------------------|------------|
| Decrease | 416 |
|-----------------------|------------|

As will be seen there has been a considerable decrease in membership, both in the direct and affiliated classes. The cause for this decrease will be apparent to everyone in the grain business. It is but a reflection of general business conditions throughout the country. Every trade organization in the land has lost many members during the past two years.

At the last annual meeting the dues of the members were increased from \$15 to \$20 a year. This increase, slight as it was, had some effect in the loss of members, but not as much as might be supposed. Of the total number of members lost 171 went out of business. There is no doubt that most of those who resigned this year will be back in the ranks in a short time.

The Ass'n has now been deflated like all other business institutions. It has reached bedrock. The weak members who came in during the war, when everyone was riding on the flood tide of prosperity, have gone. Only those who weathered the storm remain. From now on the Association will show an increase in membership.

There were taken into the Association during the last year 142 new members. This is a small number when compared with the prosperous years of the war. But it is enough to show that even under the most adverse conditions, when all business is suffering from war reconstruction, the boosters can go out and get

new members. The measure of their success is, of course, determined by general conditions and when these conditions improve an increase will at once be noted in the number of new members brought into the Ass'n.

It is the intention of the Ass'n to continue the booster campaigns. This is the only way the membership can be built up and maintained. It costs too much to place solicitors on the road. They cannot bring into the Ass'n nearly enough new members to defray the cost of their salaries and traveling expenses. The Ass'n has tried it a number of times, but always with the same result.

The Boosters. At this convention President Clement will present prizes to the four boosters who landed the most new members during the year. The Smith-Sayles Grain Co., of Columbus, O., secured first prize with eight new members to their credit. Three of the boosters are tied for second place, each having landed six new members. They are: P. Kilmartin, Malvern, Ia.; the McCaull-Dinsmore Co., Minneapolis, and the Cleveland Grain & Milling Co., Cleveland, O.

The booster campaign started on Feb. 1 and ended on May 26. This is the first time in years that the winner of the first prize secured such a small number as eight. Last year Lester Stone, who captured first prize, had 48 to his credit and the year before D. M. Cash, winner of the first prize, secured 140.

Your secretary does not hesitate to predict that it will be some years before any member will be lucky enough to win the first prize with such a small number of applications as eight.

Financial Statement.

The financial statement for the last convention year shows a surplus of \$10,831.23, a decrease of \$2,952.22 from the surplus of a year ago when the Ass'n had \$13,784.05 in its treasury.

This loss of \$2,952.82 in the surplus is explained by the expense of the transportation department. The transportation commissioner's salary of \$500 a month consumed \$4,000 of the organization's funds. He was in the employ of the Ass'n until June 1, or for eight of the twelve months of the last convention year.

While this statement shows that the surplus is but \$10,831.23, there is an actual surplus of \$15,122.69 counting the \$4,291.46 in the fund of the Special Executive Committee. Of course, this fund of \$4,291.46 was not collected with the understanding that it was to be used for the ordinary expenses of the Ass'n, and it will not be so used. It will be spent in educational work. The promises made to the exchanges that contributed the money will be kept. However, the Ass'n would be required to make certain contributions to this work in any event. The creation of a special fund for the purpose relieves the general fund from this drain upon it, with the practical result that the general surplus of the Ass'n is increased from \$10,831.23 to \$15,122.69.

The books of the Ass'n are examined each year by chartered accountants and their reports are presented to the auditing committees at the convention. The financial statement follows:

Financial Statement of the Grain Dealers National Ass'n, Covering the Period from Oct. 6, 1921, to Oct. 1, 1922, Inclusive.

| Receipts | |
|-----------------------------|-------------|
| Cash on hand last report... | \$13,784.05 |
| Direct dues | 25,458.00 |
| Direct membership | 2,610.50 |
| Associate dues | 862.50 |
| Associate membership | 58.50 |
| Regular subscriptions to | |
| Who is Who..... | 803.00 |
| Affiliated subscriptions to | |
| Who is Who | 991.25 |
| Who is Who advertising... | 15,457.39 |

| | |
|------------------------------|----------|
| Sundries | 24.48 |
| Arbitration deposit fees.... | 4,796.70 |
| Affiliated dues | 991.25 |
| Investments | 956.25 |
| Subscriptions to arbitration | |
| decision book | 20.00 |

Total receipts \$66,813.87

| Expenditures | |
|-------------------------------|-------------|
| Salary account | \$15,928.92 |
| Office supplies | 973.31 |
| Express and telegrams.... | 254.36 |
| General printing | 426.28 |
| Who is Who in the Grain | |
| Trade | 16,049.68 |
| Office rent | 948.00 |
| Telephone rent and tolls... | 198.00 |
| Refund arbitration fees.... | 3,272.16 |
| Legislative expense | 2,360.57 |
| Officers' traveling expense.. | 996.10 |
| Secretary's traveling exp... | 257.00 |
| Postage | 1,273.00 |
| Sundries | 431.41 |
| Convention expense..... | 2,937.21 |
| Arbitration expense | 4,711.70 |
| Returned application fees.. | 30.00 |
| Transportation expense | 4,690.53 |
| Loss on sale of liberty bonds | 244.36 |

Total expenditures..... \$55,982.64

| In Bank: | |
|-------------------------|-------------|
| Commercial account..... | \$ 5,554.23 |
| United States Liberty | |
| bonds | 5,000.00 |
| Petty cash account..... | 327.00 |
| | \$10,881.23 |
| | 50.00 |

Outstanding checks 10,831.23

..... \$66,813.87

Adjourned to Monday afternoon.

Monday Afternoon Session.

Calling the Monday afternoon session to order at 2:30 p. m., Pres. Clement introduced United States Senator A. O. Stanley of Kentucky, who addressed the delegates on "A Prussianized State" as follows:

A Prussianized State.

It is with genuine pleasure that I avail myself of this opportunity not only to address but to confer with the Grain Dealers of the United States in National Convention assembled. Engaged, as you are, in an industry intimately and vitally affecting the health, the comfort and the prosperity of every community within the vast confines of a continent, yours is an exceptional opportunity to see and to feel the baneful affects of unwise legislation or a maladministration of the law upon the agriculture, the commerce, the industrial activities and financial operations of the whole country. For your business is necessarily affected, for good or ill, by agricultural conditions, by the cost and efficiency of transportation facilities, by the financial strength and solvency of the village baker as well as of metropolitan banks and national exchanges.

You know not as theorists or doctrinaires, but as practical business men, the baneful effect of every governmental blunder upon the production, manufacture and distribution of a commodity essential to the life and vigor of the American people.

It is becoming more and more manifest to thoughtful and discerning men everywhere, that we are literally staggering under the weight of super-government, that our industrial activities are shackled and hampered by a pernicious and indefensible paternalism. It is the painful consciousness of this evil which has inspired a just and nation-wide cry for "less politics in business and more business in politics."

The caustic observation of Voltaire that physicians are "men who pour physic of which they know little, into bodies of which they know less," may well apply to doctors of law as well as of medicine. The day is happily past when the sufferer from any ailment must be bled first and saturated with mercury afterwards and when he only escapes who has sufficient vigor to combat both the doctor and the disease.

Medicine has learned from the retort and the microscope, that the first essential is a knowledge of the human body and that its real mission is simply to aid and not to direct or to supersede the operations of nature; that the very blood is peopled with infinitesimal but active curative agents operating with an almost human intelligence and that fever itself has its function.

It is high time for the legislative empiricist to learn that this industrial body is just as intricate and complicated in its relations, that it is governed by natural laws silent and universal as gravity and that a half-baked and ignorantly administered nostrum is infinitely worse than no remedy at all.

Ignoramuses Eager to Make Laws.—It is incomprehensible that men who would never attempt to repair the simplest mechanism without some knowledge of machinery, or cure the simplest ailment without some knowledge of medicine, show no hesitancy in attempting the most drastic interference with industrial relations more intricate than the most complicated machine, vast as the continent, and varied and complex as the human form.

Herbert Spencer, in discussing this inexplicable readiness of the legislator to regulate or supervise the most vast and intricate financial and industrial enterprises, upon which depend the prosperity of the whole people as well as the personal fortunes of millions of men, who have devoted the talents and energies of a life time to their upbuilding, has aptly suggested that it might have been assumed in legislation affecting "a social life too vast, too varied, too involved, to be even vaguely pictured in thought, man would have entered on the business of law-making with the greatest hesitation. Yet in this more than anything else do they show a confident readiness. Nowhere is there so astounding a contrast between the difficulty of the task and the unpreparedness of those who undertake it. Unquestionably among monstrous beliefs one of the most monstrous is that while for a simple handicraft, such as shoemaking, a long apprenticeship is needful, the sole thing which needs no apprenticeship is making a nation's laws!"

Socialism Not a New Panacea.—The most amazing thing about this present proposal to supervise, nationalize or socialize the private affairs and public business of the country is that it is hailed as a newly discovered panacea for all our spiritual, industrial and political ills. No patent medicine was ever quite so efficacious, and the patentees, mistaking the setting for the rising sun, actually dub themselves progressive. There is nothing progressive, there is nothing new or original in the whole program. That old and exploded fallacy that the independence of the citizen is incompatible with his moral or spiritual development or with the prosperity or the splendor of the state, is as old as servitude and as odious as tyranny. It was prated by Seneca and Petronius to flatter the vanity and soothe the leathern conscience of the imperial Caesars. These hated powers were exercised for centuries by absolute and medieval monarchs. Says Herbert Spencer:—

"There were scarcely any bounds to governmental interference; agriculture, manufactures, trades were regulated in detail; religious beliefs and observances were imposed; and rulers said by whom alone furs might be sold, silver used, books issued, pigeons kept, etc."

Since Herodotus, no historian has displayed so keen and discriminating an insight, not so



Standing: Directors, H. W. Reimann, Shelbyville, Ind.; J. W. McCord, Columbus, O.; Vice Prests. Elmer Hutchinson, Arlington, Ind.; and F. G. Horner, Lawrenceville, Ill.; Seated: Sec'y Chas. Quinn, Toledo, O.; Pres. F. E. Watkins, Cleveland, O.; Director John S. Green, Louisville, Ky.; Retiring Pres. B. E. Clement, Waco, Tex.; and Director E. M. Wayne, Delavan, Ill.

much into the lives of men, as the great movements of mankind, as Buckle, and Buckle attributes the ignorance, destitution and misery of the middle ages to this infringement upon the personal liberty of the citizen, to this abominable supervision of the activities of the people.

"Our civil and political institutions are," says he, "the consequence rather than the cause, of that tone of independence and that lofty bearing of the citizen."

"It is to this, and not to any fanciful peculiarity of race, that we owe the sturdy and enterprising spirit for which the inhabitants of this island have long been remarkable. It is this which has enabled us to baffle all the arts of oppression, and to maintain for centuries liberties which no other nation has ever possessed. And it is this which has fostered and upheld those great municipal privileges, which, whatever be their faults, have, at least, the invaluable merit of accustoming free men to the exercise of power, giving to citizens the management of their own city, and perpetuating the idea of independence, by preserving it in a living type, and by enlisting in its support the interests and affections of individual men."

He describes in detail this debasing and vexatious interference.

"Its prying eye follows the butcher to the shambles, and the baker to the oven. By its paternal hand, meat is examined lest it should be bad, and bread is weighed lest it should be light. A monopoly which comes home to the business and bosoms of men, follows them in their daily avocations, troubles them with its petty, meddling spirit, and, what is worse than all, diminishes their responsibility to themselves; thus depriving them of what is the only real education that most minds receive—the constant necessity of providing for future contingencies, and the habit of grappling with the difficulties of life. * * * For men can never be free, unless they are educated to freedom. And this is not the education which is to be found in schools, or gained from books; but it is that which consists in self-discipline, in self-reliance, and in self-government."

According to this great authority the restoration of constitutional government and the renaissance of freedom in Europe was but another name for the abolition of these abominable restrictions.

"Every great reform which has been effected has consisted, not in doing something new, but in undoing something old. The most valuable additions made to legislation have been enactments destructive of preceding legislation. * * *

"Among the accessories of modern civilization there is none of greater moment than trade, the spread of which has probably done more than any other single agent to increase the comfort and happiness of man. But every European government which has legislated much respecting trade has acted as if its main object were to suppress the trade and ruin the traders. Instead of leaving the national industry to take its own course, it has been troubled by an interminable series of regulations, all intended for its good, and all inflicting serious harm. * * *

"Then, too, we find laws to regulate wages; laws to regulate prices; laws to regulate profits; laws to regulate the interest on money; custom-house arrangements of the most vexatious kind. * * * A system was organized and strictly enforced, of interference with markets, interference with manufactories, interference with machinery, interference with shops. The towns were guarded by excise men, and the ports swarmed with tide-waiters, whose sole business was to inspect nearly every process of domestic industry, peer into every package, and tax every article; while, that absurdity might be carried to its extreme height, a large part of all this was by way of protection; that is to say, the money was avowedly raised, and the inconvenience suffered, not for the use of the government, but for the benefit of the people; in other words, the industrious classes were robbed in order that industry might thrive."

"Such are some of the benefits which European trade owes to the paternal care of European legislators. But worse still remains behind. For the economical evils, great as they were, have been far surpassed by the moral evils which this system produced. * * *

To maintain order, to prevent the strong from oppressing the weak, and to adopt certain precautions respecting the public health, are the only services which any form of government can render to the interests of civilization."

It is not necessary to review the history of the past, replete as it is, with warnings against this insidious and exasperating form of oppression. We have before us at this hour, a living and vivid example of the evils and the dangers indissolubly associated with every form of despotic and inquisitorial power.

The Prussian Heresy.—Among all the governments of continental Europe, Prussia alone clung to her medieval chains and the old and abominable heresy that men were created for governments and not governments for men.

Prior to the Franco-Prussian war, Germany might well be described as a loose federation of twenty-six sovereign states, and free municipalities. Outside of Prussia, it was the abode of thrift and industry, music and poetry, of eloquence and of art. It was indeed the land of strong men and weak states.

At the close of the Franco-Prussian War, Bismarck, playing skillfully upon a common as-

piration for German nationality, led a people drunk with victory and dazzled by the lure of power and dominion, into a compact federal despotism. Federal aid was promised and federal control was given over every detail of the intellectual, industrial, moral and spiritual existence of a once free and independent people. A bureaucracy of more than two million officials regulates every detail of the private and public life of the German people.

What Is Wrong With Germany.—Mr. William Harbutt Dawson—in his late work, "What Is Wrong With Germany"—says, "The Prussian theory proceeds from the supposition that the State makes the citizen, not the citizen the State. It follows that the citizen, so made is not an individuality, but a piece of mechanism. It is not personality that the State needs for its purposes in man or woman, but function; and the function to perform is assigned just as methodically as the motion of a crank or pinion. * * * Nowhere else in the world does the State exert so large a control over the activities of the citizen as in Germany. All the more important professions are either administered or regulated by the State. The doctor and the lawyer, the dentist, and the engineer, the clergyman and the civil servant, the schoolmaster and the architect—all must enter their profession through the State door. * * * The effect is to convert the State into a sort of brick press, that works efficiently, it may be, but relentlessly, moulding people exactly to the form and size desired, but in the process crushing out of them all individuality."

"Never," says the author of William II—Romanticist or Socialist, "does the world hear of any free, manly utterance spoken at Court before the Emperor; servility, impudently parading itself, increases more and more—a servility which treads all manliness under foot and is grateful for every glance thrown it from the ruling circles. "Everybody sighs for gracious acknowledgement or generous kicks," said Bismarck once.

"What wonder that so many Germans seek and find congenial surroundings elsewhere than in their own country, that * * * in leaving his Fatherland he leaves it forever. * * * The explanation of the looseness of the patriotic tie is that Germans who have had a breath of real freedom, as citizens of countries in which the love of freedom is regarded as a virtue and not a crime, can live no longer in the suffocating atmosphere of their own land."

It is possible that the advocates of autocracy, these unconscious apostles of the Prussianizing of America, can forget that but yesterday we embattled four million men, not against a people but against a principle. We had no quarrel with the peaceful peasantry and the industrious artisans of Saxony and Bavaria. We were there with frowning guns and bristling bayonets to halt and to hurl back in bloody rout, the armed emissaries of imperialism, the creators and sponsors of centralized power. We were there to destroy the Kaiser and his cult, to bury beneath its millions of mangled victims, the pernicious and abominable doctrine that any power, be it a monarchy or a majority, shall ever again dare to imperil the "unalienable rights of freemen."

I recall to these gentlemen who would vest in this government the dread powers lately wrung from the bloody hands of the Kaiser, the ringing words of Woodrow Wilson:

"We are accepting this challenge of hostile purpose because we know that in such a government, following such methods, we can never have a friend; * * * We are now about to accept the gage of battle with this natural foe to liberty and shall, if necessary, spend the whole force of the nation to check and nullify its pretensions and its power. We are glad * * * to fight * * * for the rights of nations great and small and the privilege of men everywhere to choose their way of life and of obedience. The world must be made safe for democracy."

Deaf to the warnings of the past, blind to the impending menace of the future, the adherents of every form of intolerance and oppression continue to vest in a fleeting and irresponsible majority powers which discerning men for cen-

turies have denied to kings and emperors.

Bills to fix the price and control the distribution of lumber and coal, to regulate the making or marketing of cloth and grain. To censor, supervise, stop, or cure baseball, horse racing, moving pictures, venereal diseases, and the social evil.

Nobody escapes, everything in the moral, industrial, and commercial world is to be owned, operated, supervised, or censored from the birth of a baby to the burial of a corpse.

This condition of affairs is equally appalling, whether we consider its cost or its consequences.

Why intelligent citizens should pay millions to be oppressed when it is so much cheaper to be free, is beyond my ken.

A government of limited powers, conscious of and obedient to constitutional limitations is neither complicated in its mechanism nor costly in its administration.

Thomas Jefferson's second Inaugural graphically portrays the character and conduct of such a government.

"At home, fellow citizens, you best know whether we have done well or ill. The suppression of unnecessary offices, or useless establishments and expenses enables us to discontinue our internal taxes. These covering our land with officers and opening our doors to their intrusions had already begun that process of domiciliary vexation which, once entered, is scarcely to be restrained, reaching successively every article of produce and property. * * * It may be the pleasure and pride of an American to ask what farmer, what mechanic, what laborer ever sees a tax gatherer of the United States?"

Senator Foraker in 1907 bemoaned the multiplication of useless offices and officers in his "degenerate day" declaring that—"The little band of 167 special deputies, agents and inspectors on the pay rolls of the Government 10 years ago has been swelled to an army of more than 3,000."

Three thousand agents and inspectors! Sump-tuary and inquisitorial bureaus and commissions have increased since then one thousand-fold and Federal agents and inspectors filling the highways and byways, tread upon each others' heels at every turn and corner, more numberless and pernicious than the fleas and frogs of Egypt.

Every business man finds an inspector at his elbow, a Federal sleuth at his heels; houses are searched, homes outraged, and the public highways dappled in the blood of unoffending citizens by a multitude of agents and inspectors, ignorant or indifferent to the law of the land and the rights of the citizen.

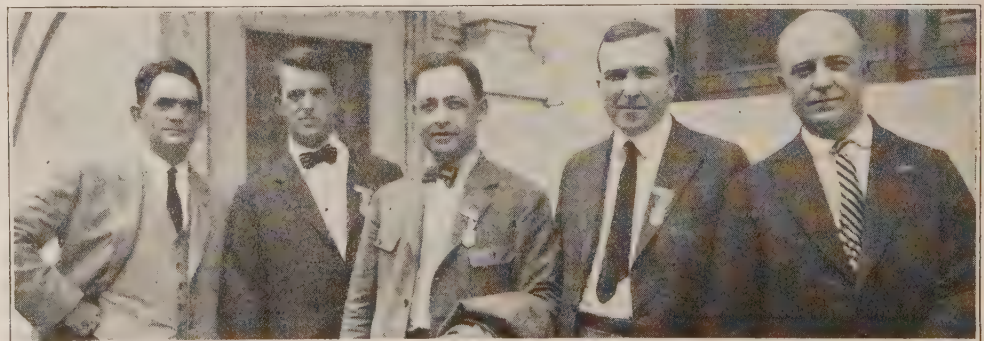
Ex-Governor Haskell of Oklahoma, in a recent address delivered at the Hotel McAlpin says—

"Less than a quarter of a century ago, in the greater enjoyment of individual rights and local self-government, our Federal authorities found it necessary to have upon the pay roll of the Federal Government fewer than 200 sleuths and special agents and inspectors to aid in the enforcement of Federal laws. Will anyone defend the policy of the Federal Government which today employs more than 42,000 inspectors, sleuths, and inquisitorial agents to dog the footsteps of him who should be, in the exercise of his constitutional rights, enjoying the hitherto dignity and freedom of an honest American citizen."

In the ownership, operation, or supervision of the industrial and commercial activities of an empire, from telegraphs and telephones, steamships and railroads to the elimination of the boll weevil and the setting of a hen, Congress has created 100 different committees and commissions, boards and bureaus, and within 10 years has actually expended the appalling sum of \$3,921,360,167.55, according to the official figures of these departments. If, however, the more dependable estimates of that able and accurate statesman, Senator King, of Utah, are accepted, it reaches the incomprehensible aggregate of \$7,500,000,000.

The bulk of this money was squandered in doing for the people what they could and should have done infinitely better for themselves. The

The Corn Belt Quartette.



Bert K. Smith, Ft. Worth, Tex., Substituted in the Fotograf for H. A. Rumsey of Chicago; S. W. Wilder and J. R. Murrel of Cedar Rapids, Ia., Geo. E. Booth, Chicago; and Chet Davis of Cedar Rapids.

entire cost of the operation of the Federal Government from the inauguration of George Washington to the Civil War does not approach this colossal and incomprehensible sum.

And worse still, this insatiable lust for inquisitorial power daily begets new boards and bureaus. The appetite for attending to other people's business "grows by what it feeds on," and their devastating cost increases by leaps and bounds. For instance, in the year 1916 the cost of operating the Government exclusive of expenditures for war purposes was \$231,625,333; for the fiscal year ending June 30, 1922, the amount appropriated for the same purpose, not including the cost of the War and Navy Department, pensions, Veterans Bureau, and reduction of the national debt, is \$1,115,517,366, or an increase of nearly 500 per cent. The difference in expenditure for practically identical services in 1916 and 1921 is, broadly speaking, the cost of these commissions with their innumerable army of agents and inspectors.

Aaron Hardy Ulm, writing for the Dearborn Independent, of Dec. 17, 1921, states—

"What amounts to the 'support' of around 15,000,000 individuals passes in the form of taxes from the public as a whole to the minority of the people in these United States. This means that 15 per cent of the population, or what is tantamount thereto, is supported in some form by government. * * Not many more than 30,000,000 persons are actually engaged in producing and distributing food, clothing and other necessities of life. Every two actual producers now maintain * * the equivalent of one individual that is maintained by governmental expenditure of some sort."

It is better, far better, that a people should be bankrupt than that they should be debased. The effect of such legislation upon the character and independence of the citizen is infinitely worse than its devastating result upon the industrial life of the community.

Said Lincoln—"A nation can not exist half slave and half free," and no more can it live half socialist and half individualist. You can never blend bolshevism and democracy. Such a system must inevitably impoverish the citizen by its cost and debase him by its oppression.

Specious Phrase "Federal Aid."—So conservative a statesman as Governor Lowden of Illinois has sharply called the attention of the country to the fact that to preserve the republic we must cut this socialistic cancer from the body of the state.

"There is scarce a domain," says he, "in the field of government properly belonging to the municipality or the state which the federal government is not seeking to invade by the use of the specious phrase, 'federal aid.' Education, public health, private employment are a few instances which readily come to mind. * * This rapid extension of federal administration not only means greatly increased expenses because of duplication of efforts but it means the gradual breaking down of local self-government in America."

Again he says, "This trend toward centralization of all authority and power in Washington strikes at the very foundation of our government. A country to the extent of ours possibly may be governed in all its details as an empire from a central capital, but not as a republic."

Legislators ignorant of or indifferent to the mistakes of other governments, might at least profit by their own.

U. S. Railroad Labor Board.—In 1919, Congress passed a bill providing for the organization and the complete unionization of all the railroad employees in the United States and this plan was supplemented in the Senate by complicated and detailed plans involving arbitration commissions, adjustment and labor boards, by which the prevention of strikes, the fixing of wages and hours of labor and working conditions were all practically removed from the control of the executives of the several railroad lines and placed in the hands of boards and commissions to be named by the President from labor organizations, railroad executives and the public generally. By this arrangement two mil-

lion men were practically consolidated under the control of a single and distant federal agency.

At that time I proposed an amendment to the Transportation Act eliminating this Labor Board with all its cumbersome and useless machinery, since, as I stated then, and as I reiterate now, the proposed board had no power on earth except to invite disputes it could not settle or to start fires it could not put out.

On the 18th of last August, the President in a message to Congress declared that the public menace in the coal situation was attributable in great measure to "a strike against a wage scale made by the Railroad Labor Board directly affecting approximately 400,000 men."

"This Railroad Labor Board," said the President, "was created by Congress for the express purpose of hearing and deciding disputes between the carriers and their employees, so that no controversy need lead to an interruption in interstate transportation."

"But," said the President, "The law creating the Railroad Labor Board is inadequate. Contrary to popular impression it has little or no power to enforce its decisions. It can impose no penalty on either party disregarding its decisions."

In other words, this was a strike by approximately a half million men not against the management or executives of one or more railroads but against a governmental agency—the Labor Board.

This Labor Board, like similar boards in Australia and New Zealand must infallibly provoke the very troubles it was designed to prevent and produce the strikes it is unable to adjust.

From the very nature of things, it must follow that whenever the reward of the laborer is determined not by his individual worth but by universal law, and his wage—whether it be a maximum or a minimum wage, I care not—is fixed by a governmental commission and is measured not by his efficiency but by his hypothetical needs, then the laborer will invariably look for relief not to himself or to his immediate employer, but to that governmental agency which has taken upon itself the necessarily dangerous and invidious task of managing the whole business for both. Under such a regime the restless and discontented will invariably seek relief, not in daring enterprise or bold endeavor, not in any effort to find new employment, more pleasant or more remunerative, but in an attempt to direct the course of the government and to increase the return upon standardized toil; not in an adequate wage but in a more generous governmental subsidy. Then unrest is transformed into insurrection and all the envy poverty feels for wealth or weakness for power, is diverted from private enterprise to governmental authority. This nationalization of great industries, this socialistic supervision of every form of enterprise has made of industrial conditions, political issues, and given to strikes and lockouts the hideous aspect, not of resistance to an employer but of rebellion against the state.

This attempted federal control of a great utility has been as disastrous to capital as it has been vexatious to labor.

Nationalization of Coal Business.—It is amazing that in the face of this practical demonstration of the impotency of municipal ownership, of the vast discontent and the disaster which inevitably follow in its wake, that we should be brought face to face at this hour, with a proposal for the organization of a commission for the avowed purpose of submitting to Congress among other things, recommendations relative to the standardizing of mines upon the basis of their economic and productive capacity, the standardizing of the cost of living of the mine workers and the living conditions which must be supplied, and further recommending the advisability and the wisdom of the nationalization of the coal business and the advisability and necessity of government regulation and control.

The experience of Prussia, the experience of England, the experience of every other country attempting the ownership or the socializing of

this industry has demonstrated its failure. Under this system of governmental interference and governmental supervision the productiveness of the English miner fell from 312 to 244 tons. Today, under private ownership and private operation, free from governmental intervention, in spite of strikes and lock-outs, the American miner is 300 per cent more effective than the socialized operatives of Wales and Germany.

It is true, all too true, that in America and all the world the masses are restless and resentful, but such legislation as this will never appease that resentment or allay or quiet that unrest. Labor and capital alike, are weary of governmental supervision and governmental inspection, tired and heart-sick of the dominations of monarchs and of mobs that have debased the people of Prussia and of Austria, decimated the peasantry of Russia and maddened the masses of America.

It is amazing that in the Twentieth Century, seasoned and experienced statesmen, repudiating the principles and forgetful of the traditions of a century of freedom, should, at this late hour, with an inexplicable glee, exhume the dead and buried despotism of the past in an attempt to force upon a free people, an ancient and inquisitorial regime condemned by the universal experience of all mankind and for three thousand years abhorred by freemen of every creed and every clime.

The crying need of the hour is for more business in politics and less politics in business. A truce on this endless multiplication of commissions and commissioners. Oh, for a government that can find one thing which it can not do, one socialistic heresy it will not accept, one governmental nostrum it will not inflict upon a long-suffering people.

Senator Stanley was accorded a standing vote of thanks for his able address.

Those present who were especially interested in the feed business withdrew for their branch session.

Henry L. Goemann, of Toledo, O., read the report of the Transportation Com'te, of which he is chairman, as follows:

Transportation Com'te Report.

The Transportation Com'te Report will have to be somewhat brief due to the fact that Mr. W. K. Vandiver, former Transportation Commissioner, was in charge of the work until May 1 when he resigned and, therefore, since that time, as Chairman of the Com'te, I have taken up the question of handling the unfinished work.

Docket 9009.—The American Railway Ass'n in response to the opinion of the Interstate Commerce Commission in this Docket 9009, issued their circular No. 1, which was distributed to the various interested carriers and was not made effective by being made a tariff measure making this circular by reference a part of the various carriers' tariffs so that the work of this com'te to date has been without results.

This Ass'n has been actively endeavoring ever since the issue of this circular No. 1 to have the railroads make it a part of their tariffs so that claims for loss would be settled thereunder and also to endeavor to have the Western Trunk Line Com'te amend their rule so as to make their deduction on corn one-eighth of one per cent instead of one-fourth of one per cent and have this allowance made uniform in all the various traffic territories.

I believe that the carriers are opposed to making this report of the Commission effective due to the fact that they strongly object to the use of automatic scales as shipping scales where the capacity runs for each dump less than 2,000 lbs. at least and strongly object to the large number of these shipping scales that are of a capacity from 3 to 15 bushels to the dump and improperly installed.

Reduction of Natural Shrinkage on Corn.—In order to have the Western Trunk Line roads reduce their corn deductions to one-eighth of one percent it will be necessary for the ship-

Chief Grain Inspectors in Attendance.



Laurel Duval, New York; Geo. S. Colby, New Orleans; Lee D. Irving, Louisville, Ky.; Frank B. Tompkins, Peoria, Ill.; D. H. Larkin, Baltimore, Md., and Paul Larson, Sioux City, Ia., in Front Row. C. F. Raker, Texas City, Tex.; A. A. Breed, Milwaukee, Wis., and Samuel A. Holder, in Rear Row.

ping public to agree that it will be understood that this will apply only where the liability of the carriers has been clearly established and not have any bearing on the difference in scale weights. If such a rule is put into the tariffs it will mean that only defective cars or cars that are transferred en route by the carriers will carry this deduction and that it will not take care of difference between scales irrespective of how accurate the scales may be. I am afraid if such a rule is embodied in the tariff it will mean to establish a loss in court by the shipper for loss between markets, that each case in the courts would be prejudiced by such a rule. I have, therefore, requested a meeting of all the State Secretaries as well as the Transportation Commissioners of the various terminal markets to meet with me today after the close of our meetings in this room for a thoro discussion of this matter and to take such action as is necessary to get this matter properly adjusted.

I also have a number of complaints relative to the carlot minimum weights on sack grain which in the Central Freight Ass'n territory have been construed by their weighing and inspection department as being the same as bulk grain and which a number of the shippers complain they are unable to comply with. Also complaints on the \$5 charge for bulk-heads on mixed cars. I would be very glad to have an expression from those who are interested and if there is a general complaint on this subject, to endeavor to have the rules modified.

Liability of Telegraf Companies.—As you are aware, the Transportation act now gives the Interstate Commerce Commission authority over the telephone and telegraf companies as it has been arranged that the Chairman of the telephone and telegraf companies com'ite, Mr. F. G. Horner, with the transportation com'ite, handle this subject in order to secure some changes in the rules as now are effective in accordance with the rules printed on the back of telegraphic forms used by the telegraf companies. By referring to paragraph one, of the Western Union blank which reads as follows:—

"The company shall not be liable for mistakes or delays in the transmission or delivery, or for non-delivery, of any message received for transmission at the un-repeated message rate beyond the sum of \$500, nor for mistake or delays in the transmission or delivery, or for non-delivery of any message received for transmission at the repeated message rate beyond the sum of \$5,000, unless especially valued; nor in any case for delays arising from unavoidable interruption in the working of its lines; nor for errors in cipher or obscure messages."

Evade Liability for Cipher Messages.—From this you will see that the company is not liable for errors in cipher messages or messages that are not plainly written so that it is legible to the operators. This practically means that the majority of messages sent by the grain trade will not be covered by this rule and my suggestion, therefore, on this is that we have the Interstate Commerce Commission modify the rule by amending the cipher word and make the company liable when such cipher words are DICTIONARY words. This, then, would protect the users of ciphers that are made up from DICTIONARY words. As to obscure messages, do not believe that any rule can be made that will protect every user of the telegraf and my suggestion would be that wherever possible messages be typewritten and that will take away the burden of proof as to the legibility of the message.

I would also like to call attention of the trade that in my judgment where the sender of messages calls a messenger boy from the company to receive a message or where the telephone is used, that in case of non-receipt by the company or errors in such messages telephoned, that it would be at the sender's risk. This is just my viewpoint and I am calling attention to it so that each sender can investigate and avoid any claim.

I have also received a great many complaints from members regarding the rates charged as well as the rules and regulations governing messages and especially against the telephone companies and I would be very glad to have a free discussion of this subject as I expect to call a meeting of various national organizations in Chicago shortly after this convention to see what can be done in this matter. At present, however, it will mean simply negotiations with the telephone and telegraf companies by the users of these companies as there is no way of bringing a case before the Interstate Commerce Commission due to the fact that telephone and telegraf companies are not compelled to file tariffs of rates with the Commission.

There is now before Congress a bill to amend the Interstate Commerce Commission act and the Transportation act of 1920, prepared by the National Industrial Traffic League and introduced in Congress on May 27, last, by Hon. Walter H. Newton of Minnesota and which has been referred to the com'ite on Interstate and Foreign Commerce and ordered to be printed. I will only quote that part of the bill which relates to the filing of tariffs and will quote the amendment which is to be added to Section 3 and which reads as follows:—

"(14) That every telephone, telegraf, and cable company subject to this act shall file with the Commission, and print and keep open to public inspection, schedules showing the rates and charges, rules, regulations and practices for all

classes of service in connection with the transmission of intelligence between points on its own route, and between points on its own route and on the route of any other such carrier where a thru route and joint rate has been established. If no joint rate over the thru route has been established, the several carriers in such thru route shall file, print and keep open to public inspection as aforesaid the separately established rates and charges applied to the thru transmission. Such schedules, printed as aforesaid, shall plainly state the points between which intelligence shall be transmitted, the rates, charges, rules and regulations applying thereto and the respective grades of service, and all provisions of this Act with reference to the authority of the commission over the schedules of common carriers and over the printing, filing, posting, or changing thereof, shall apply in like manner to the schedules of common carriers engaged in the transmission of intelligence and subject to the provisions of this Act as in the case of other common carriers, in so far as the same can be made applicable to the service of transmission of intelligence."

I would suggest that when Congress reconvenes that those who are interested in having this bill passed, take it up with their Congressmen and endeavor to secure passage of the bill.

Reading of Chairman F. G. Horner's report on Telephone and Telegraf was dispensed with.

Sec'y Quinn read the following report for W. S. Washer, St. Joseph, Mo., chairman of the Com'ite on Uniform Grades:

Uniform Grades Com'ite Report.

No matters of especial moment have been submitted to this com'ite during the current year, which is indicative either of a state of satisfaction with the grading machinery of the nation or a lack of interest in the matter. The Chairman addressed letters to all members of this Com'ite asking for any suggestions to be embodied in this report. Replies were received from but four of fourteen members and suggestions were contained in but one reply. In view of this situation this report is largely the observations of the Chairman of the com'ite.

Sec'y Quinn in his report has already advised you of the annual efforts of politicians of the Northwest to endeavor to debase the existing grades of wheat. They seem to be under the impression that No. 4 wheat can be made No. 2 simply by calling it so and that value can be legislated into grain. This is no more possible than to legislate value into fiat money, and the condition of the currencies of Germany and Russia would seem object lessons enough in this respect.

Skin-burnt Grains not Necessarily Heat Damaged.—Your Chairman attended a hearing in Kansas City in April, held under the auspices of the Department of Agriculture in respect to proposed changes in wheat grades. This hearing presently developed into a discussion of the grief encountered by the trade in the matter of heat damaged wheat. It was apparent from the experiences related that the lack of uniformity in determining heat damage had been the cause of very severe loss to the handlers and producers of wheat. Member Culp of this Com'ite, representing Missouri, makes the constructive suggestion that it might be possible to either modify or subdivide the heat damage determination; that skin-burnt grains are not necessarily heat damaged; and that wheat damaged in shock from weather is often not heat damaged in the sense that all worth has been burned out of the grain as in the case of true heat damage.

The matter of protein content in wheat is receiving universal attention from millers at this time. From some discussions noted in the trade journals there is beginning to appear quite a difference of opinion amongst some of the chemical experts as to the real value of the protein test. The fact remains, however, that the great majority of millers are now buying on protein content and that the wheat trade must adjust themselves to the situation. Whether or not protein content should be constituted as a grade factor is a moot question and one that might well be discussed at this convention. Some State Inspection Departments are arranging to install laboratories to test wheat for protein while in other markets this is being left to the commercial laboratories. The method of formula for the determination of protein should certainly be standardized and this matter could be taken up with advantage with the proper officers of the cereal chemists organization.

Federal Supervision Extremely Technical at Times.—As a general proposition the supervision of grades by the Department of Agriculture has been reasonably satisfactory, but there is a tendency on the part of some supervising officials to make their supervision so extremely rigid and technical that it is well nigh impossible to conduct any volume of grain business under such auspices. An extremely technical application of rules in one market and a less rigid application in neighboring markets unduly prejudices the one and grants unfair advantage to the other. In so highly competitive a business as the grain trade this may well spell ruin to the disfavored market.

Adjourned to Tuesday.

After adjournment Mr. Goemann told of a conference with the Western Trunk Lines, on Docket 9009 and the following proposed rule of the carriers:

"Claims for Shortage of Grain in Bulk."

(a) Claims for shortage of grain in bulk, will only be paid when, after investigation, it is demonstrated that such shortages are the result of wreck or defective equipment or transfer of the grain by the railway companies en route, or other causes for which the carrier is liable. This means that when the record is perfect, claims will not be paid.

(b) Carriers will not be responsible for invisible loss and waste, due to evaporation or other natural causes, nor for discrepancy due to defective scales or other shipper's facilities or to inaccurate weighing or other error at point of origin or destination.

(c) When liability of carriers is established, there shall be deducted from the gross amount of the ascertained actual loss in weight, $\frac{1}{4}$ of 1 per cent of the loading weight of the contents of the car to cover invisible loss (natural shrinkage) and waste.

(d) When grain heats in transit, and carrier's liability for leakage is established, there being no other evidence of carrier's negligence, the ascertained actual amount of the invisible loss due to heating of the grain will be deducted (in addition to $\frac{1}{4}$ of 1 per cent), but if the heating of the grain was also caused by carrier's negligence, then only the $\frac{1}{4}$ of 1 per cent will be deducted.

Mr. Goemann asked for instructions.

On motion by Hiram N. Sager of Chicago a resolution was adopted by those present that

It is the sense of the meeting that Mr. Goemann in presenting this matter to the Interstate Commerce Commission request and urge that the report and opinion in Docket 9009 be promulgated as an order by the Commission.

Resolved that we indorse the specifications adopted March 13, 1922, by the special joint meeting called by the Weighmasters Scalemen's Conference of all interested in the specifications for hand-operated hopper scales, when amended to meet the approval of the U. S. Bureau of Standards, and that we earnestly request the Bureau to pass upon them immediately.

Seconded by J. W. McCord, and adopted.

On motion by H. B. Dorsey, seconded by A. E. Reynolds, the following resolution was adopted:

We oppose such portions of the rules proposed by the carriers to be placed in the tariffs covering weights and allowances for shrinkage and variations in scales, which are not in accord with I. C. C. Docket 9009.

Tuesday Morning Session.

Tuesday's session was called to order at 10:15 a. m.; and Pres. Clement named the following a com'ite to report on the recommendations made in the sec'y's annual report: E. M. Combs, Chicago; C. D. Sturtevant, Omaha, Neb.; John S. Green, Louisville, Ky.

Congregation singing was then led by J. R. Murrel, and all joined with enthusiasm in reciting the words accompanying his "Ship of State" cartoon which is reproduced herewith.

Frank Kell, not being in attendance at the convention, his scheduled address on "The Grain Trade and the Duties of Citizenship" was not delivered.

F. E. Watkins, Cleveland, O., chairman, presented the report of the Trade Rules Com'ite, following:

Trade Rules Com'ite Report.

The past year has shown some decrease in inquiries relative to interpretation of trade rules and appeals to this Com'ite for assistance in untangling trade disputes without the necessity of arbitration—which small "straw" might lead one to infer that the winds of trade are now blowing less boisterously and with fewer cross currents than during the two previous years.

While your Com'ite has never considered that it should assume any of the functions of the Arbitration Com'ites, we are occasionally asked by both parties to a dispute to express an opinion on a difference which involves an amount too small to make it appear necessary to go through the regular process of arbitration and to take up the time of the already burdened Arbitration Com'ites. Several cases of this kind are handled each year by this Com'ite and the past year has been no exception in this particular.

In accordance with the conclusions arrived at during the discussion of methods of amending our rules, which occurred on the convention floor at Chicago last year, and in line with the procedure of this Com'ite during recent years, about June 20th last, a request was made of the various members of the Arbitration Com'ites to submit any changes in the rules which would seem advisable growing out of their experience in dealing with arbitration cases. A number of

suggestions were received which we assembled in a bulletin to these same Com'ites requesting their further comments and opinions. Those amendments receiving the support of a majority of the members of these Com'ites are submitted to you today for your consideration.

Before proceeding to the discussion of these proposed amendments, the Com'ite would again call your attention to the desirability of limiting our rules to those which state underlying principles of trade and consequently to those having practically universal application. It would be most unfortunate, in our opinion, to adopt rules which have more or less local application and which attempt to deal with special or occasional matters that could best be covered in the original articles of trade. It would do no harm for us to reiterate at this point the fact that these rules have been pronounced to be the most complete and comprehensive code governing any trade of such magnitude as the grain trade, and our every effort should be made in the direction of strengthening rather than diversifying or weakening a code which can command such high praise, and which, with comparatively few modifications, has demonstrated its sterling worth in daily transactions throughout a period of over twenty years.

Not all of the suggestions offered for your consideration at this time are of equal importance, and we feel sure that no plea from us is needed to cause the members assembled here to give most deliberate and thoughtful consideration to these proposed changes in our code.

It seems only reasonable to limit the time for filing papers in arbitration and I offer the following amendment to the arbitration rules:

Art. V., Sec. 1, add:
* within six (6) months after expiration of contract on which dispute occurs.

Adopted.

I offer the following new rule on contracts subject to embargoes, strikes or shortage of cars:

Contracts Made Subject to "Embargoes," "Strikes" or "Cars."

Section (a). When contracts are made "subject to no penalty on account of embargoes," shipment must be made within the number of days specified plus the number of days the embargo is in effect; except that when the contract provides for shipment during a certain month or half month, shipment must be made within fifteen days after the embargo is lifted.

Section (b). When contracts are made "sub-

ject to no penalty on account of strikes," shipment must be made within the number of days specified plus the number of days strike conditions at point of origin prevent billing or loading; except that when the contract provides for shipment during a certain month or half month, shipment must be made within fifteen days after strike conditions at point of origin permit loading and billing.

Section (c). When contracts are made "subject to no penalty on account of inability to secure cars," the seller must use due diligence in ordering and obtaining empty cars from the carrier and should, so far as possible, ship all contracts in the order in which sales are made.

Section (d). Burden of proof shall be upon the seller to show that conditions specified in Section (a), (b) and/or (c) above prevented shipment within contract time.

Section (e). It shall be the duty of the seller to advise the buyer by wire upon or before the expiration of the contract of his inability to make shipment within contract time; failure to wire as above shall make the embargo, strike and/or car clause void.

Adopted.

After an explanation by Mr. Watkins the following rule 30 was adopted as a substitute for the present rule:

Rule 30, Buying In.—Where the words "Buy in" occur in these rules they shall mean an actual purchase of grain of like kind and quantity on the open market, provided that when this is not feasible or would result in undue penalty to the seller, the buyer shall have the privilege of establishing a fair market value for the purpose of determining any loss properly chargeable to the seller.

Rule 16 providing that " * * * any loss resulting from irregular or incorrect invoices shall be paid by the seller" came in for considerable discussion.

Elmer Hutchinson, Arlington, Ind., said he was not much in favor of it.

Mr. Dorsey called for a ballot vote. Before the tellers completed their arrangements Mr. Wells referred to the voting power of affiliated ass'ns and Mr. Quinn informed him that only 4 of the 13 affiliated ass'ns were represented on the floor, whereupon Mr. Sturte-

vant moved that a ballot vote be taken by mail on Rule 16. Carried.

A rule, 4, on confirmations providing for confirmations to be binding on the other party when only one party confirms was tabled.

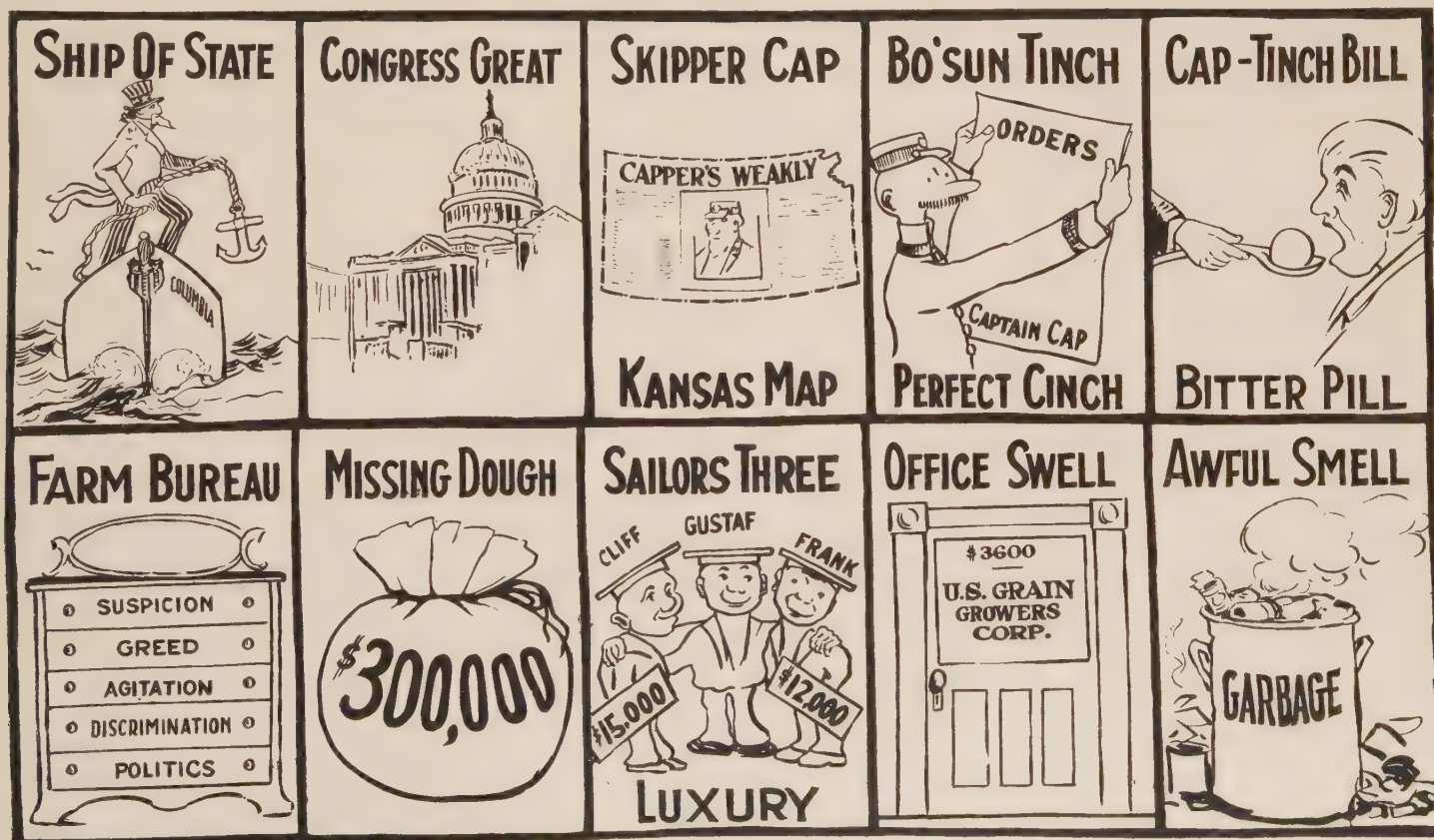
Another proposed new rule that the "federal grading" shall mean inspection by an inspector licensed by the Federal Government nearest the market of seller, also was tabled.

E. M. Combs, Chicago, moved that paragraph (a) of Rule 38 be adopted; and with an amendment offered by Mr. Reimann that the shipper should have received notice it was adopted as follows:

Rule 38. Condition Guaranteed Upon Arrival. (a) Where grain is sold with condition guaranteed at destination, AND DESTINATION IS GIVEN IN THE ORIGINAL BILLING INSTRUCTIONS, it shall be the duty of the buyer to ascertain by inspection or otherwise the condition or grade of the grain AND REPORT SAME TO THE SHIPPER so that he will receive such report or notice within forty-eight hours after arrival at SAID destination. In case the buyer fails to ascertain AND REPORT the grade or condition, as provided above, he shall waive all rights under the guaranty of the contract. Diversion of the shipment by the buyer to some point beyond the SAID destination shall constitute an acceptance of the grain and a waiver of the guaranty.

Paragraph (b) of Rule 38, proposed by the com'ite was completely rewritten on suggestions by Mr. Hutchinson and Mr. Sturtevant to make it more specific. As adopted it reads as follows:

B.—When grain is sold for shipment to group or rate basis points and condition is guaranteed at destination and shipment is ordered billed to a blind billing or diversion point, it shall be the duty of the buyer to file diversion orders with the carrier within 24 hours after the arrival at such diversion point and send a copy of such notice to the shipper, and to assume all expenses incident to or caused by such diversion and on his failure to so notify the carrier the seller's liability under the guaranty shall cease at such diversion point. Only one diversion shall be permitted under the guaranty, and in no case shall the seller's liability continue to points taking a higher rate than the rate basis on which the grain is sold.



Cartoon from Iowa with which Song Leader Ray Murrel of Cedar Rapids Led the Class on the Convention Floor in the "Dutch" Recitation:
Ist Das Nicht die Ship of State? Ja, Das Ist die Ship of State. Ist Das Nicht der Congress Great? Ja, Das Ist der Congress Great. Ist Das Nicht der Skipper Cap? Ja, Das Ist der Skipper Cap. Ist Das Nicht der Bos'un Tinch? Ja, Das Ist der Bos'un Tinch. Ist Das Nicht der Bitter Pill? Ja, Das Ist der Bitter Pill. Chorus All: Ship of State, Congress Great, Skipper Cap, Kansas Map, Bos'un Tinch, Perfect Cinch, Cap-Tinch Bill, Bitter Pill. Oh, Die Schoenes Ship of State.
Ist Das Nicht der Farm Buro? Ja, Das Ist der Farm Buro. Ist Das Nicht der Missing Dough? Ja, Das Ist der Missing Dough. Ist Das Nicht die Sallors Three? Ja, Das Ist die Sallors Three. Ist Das Nicht der Office Swell? Ja, Das Ist der Office Swell. Ist Das Nicht an Awful Smell? Ja, Das Ist an Awful Smell. Chorus, All: Farm Buro, Missing Dough, Sallors Three, in Luxury, Office Swell, an Awful Smell. Oh, die Schoenes Ship of State

Rule 37 on war tax was repealed and eliminated.

In place of the rule on stored grain the following was adopted, with the number 33:

No. 33. Grain in transit or in store:—Grain shipped prior to date of sale or grain in store at destination shall not be applicable on contract except when so specified or with the consent of the buyer.

M. P. Williams, of Norfolk, Va., commercial agent of the Norfolk & Western R. R., called the attention of shippers to his road's superior connections to the new municipal elevator at Norfolk.

The booster prizes were then awarded. First prize went to P. C. Sales, of Columbus, O., who obtained 8 members. The following three were tied, each having obtained 6 new members: F. E. Watkins, Cleveland, O.; P. Kilmartin, Malvern, Ia., and E. F. Huber, of Minneapolis, Minn.

Henry C. Taylor, Washington, D. C., read a paper on "The Department of Agriculture and the Grain Trade," from which we take the following:

The Department of Agriculture and the Grain Trade.

While the Bureau of Agricultural Economics, which has this work in charge, was organized within the past year the work in this field has been gradually developing throughout the whole history of the Department of Agriculture.

About ten years ago a Secretary of Agriculture said: "The other half of agriculture is marketing," and the Office of Markets was organized for the purpose of gathering and disseminating market information and rendering service in the establishing and administering of standard grades for farm products. The purpose of this work was to put all who were interested in the marketing of farm products on an equal footing with regard to the facts that influence market prices and to establish standards as a basis of transactions and as a basis of price quotations which would put the business of marketing farm products on a higher plane, in the interest of all concerned. This work developed rapidly and became the Bureau of Markets, which became widely known throughout the United States, though I feel not thoroughly understood by all whose interests were touched by it.

The U. S. Department of Agriculture is responsible for the integrity of the administration of the United States grain standards act. Your business from day to day demands that grain grades shall be as standard and as immutable as the weight and quality of the gold used as the standard of value.

Government Promotion of Co-operative Undertakings.—It is not the policy of the Department of Agriculture to participate in the organization or operation of either co-operative or corporate marketing undertakings. It does attempt to collect and disseminate information which may be useful to anybody who is interested in marketing farm products. This information has been rather inadequate in the past, and, as a result, farmers have not had full information with regard to middleman services and the cost of rendering these services. It is our hope to give more complete information on these points and this information may show the farmers that much of the middleman service is now being performed for them as cheaply or more cheaply than they can perform it for themselves through their hired employees.

On the other hand it may show to them that at certain stages in the marketing processes they can participate to advantage. We are interested in seeing farmers' co-operative marketing undertakings organized wherever they can render new service or render old services more efficiently. But, in the marketing of grain, whether by independent grain dealers or co-operative grain dealers, we are willing and anxious to help both. Our purpose is to promote the most efficient and cheapest system of marketing by whomsoever it may be done.

Some county agents have at times been too enthusiastic for certain independent dealers. These activities are not approved by the Department of Agriculture and it is the effort of the Department to confine the activities of all of the extension agencies to educational work.

County agents receive their salaries and expenses from three sources—Federal, state and county. The Federal government puts in the least money, the state next, and the county the most. Extension agents are very carefully chosen and instructed, but there are several thousand of them and they are widely scattered, so that the supervision cannot be very intensive. But, while the county agents are left to direct their own work most of the time, the general policies that guide them, and the programs of work, are subject to the administrative control of the Director of Extension in each state, who are responsible to both the U. S. Department of Agriculture and the State College of Agriculture. Thus the Department's control of

county agents is through the State Director of Extension and it is through them that the educational material and the educational policies of the Department reach the county agents.

While the Department of Agriculture is definitely opposed to the participation by county agents in the organization and operation of business undertakings, it looks upon the dissemination of marketing information and the teaching of well established facts and principles regarding methods of marketing as an important function of the county agent. Secretary Wallace on another occasion made the following statement: "There is just as much reason why the department should assist the farmer in developing methods of marketing his crops efficiently as it should assist him in increasing his production. The study of improved systems of marketing as well as the study of conditions the farmer should understand to produce intelligently and to adjust his production to the needs of consumption, are proper functions of the department. This does not mean that the department should assume the leadership in the organization of marketing associations, co-operative or otherwise. Neither does it mean that the department should have any active part in carrying forward the work of such organizations. It means simply that we should follow exactly the same policy with reference to marketing that we have followed with reference to production."

To illustrate the type of educational work which the Department is urging upon the county agents in connection with the marketing of grain, I wish to refer to the "Know Your Own Wheat" campaign which is now in progress in the spring wheat regions of the Northwest. You are familiar with the fact that the Secretary of Agriculture was urged to change the federal grain grades, particularly with respect to certain classes of wheat grown in the spring wheat area of the Northwest. After giving the matter full consideration it was decided that the legitimate complaint arose from the custom so commonly prevailing of paying the same price at the loading stations for wheat of widely different milling values.

The Department arrived at the conclusion that the solution of this problem lay not in the changing of the standards but in educating the farmers to know their wheat, and in developing a market news service which would enable the farmer to know the price at which his particular wheat would sell in the Minneapolis market where the millers are bidding for his product. In order to carry out this idea, the county agents from thirty counties in the Dakotas and Minnesota were brought together into schools, where they were given instruction in the grain grading. Following these schools, grain grading demonstrations were given at several points in each of the counties, to which both farmers and local grain dealers were invited. In the office of each of the county agents of the thirty counties equipment for grading grain has been installed. This gives an opportunity for any farmer in these counties to know approximately the grade of wheat which he has for sale and by studying the information now being secured in the Minneapolis market, the farmer is in a position to approximate the value of his product.

The local grain dealers in these counties have taken quite as much interest in this campaign as have the farmers and the movement is definitely on toward buying grain at the local market on the basis of the price at which the particular grade would sell in the Minneapolis market. It is true that this will mean a lower price for some grain, as well as a higher price for other grain, but it meets the complaint that the farmer who produces the better wheat receives no extra pay for that product and points in the direction of a definite improvement in wheat production and marketing in the Northwest.

Another example of the type of marketing activity which is being asked of the county agents is the spreading of information with regard to methods of cleaning grain. The Department has for several years carried on experiments in the cleaning of wild peas, cockle and king head from the wheat as it comes from the threshing machines. This work has now come to the point where every farmer should know of the possibilities not only of cleaning the grain which he has for sale, in order to eliminate the cost of transporting foreign material to the central market and in order to secure a better price for the product which he has for sale, but also in order that he may use this equipment for cleaning the seed which he sows and by this means reduce the amount of weeds in his growing crop.

Many farmers are dissatisfied with the present marketing system. There may be two very different occasions for this dissatisfaction—namely, lack of knowledge or misinformation; or the inefficient or unfair operation of the present marketing system at some point which affects them. The best remedy for the first of these occasions of dissatisfaction is the presentation of the facts about middleman service, middleman margins and the costs of these services. If these facts satisfy the farmer that he is getting efficient service at a fair price this probably ends the matter. But if after the plain truth is told the farmers want to co-operate and enter the marketing field in competition with independent dealers, who of you would say we should tell him he should not do

so? And if the farmer asks questions about the methods of organizing co-operative marketing companies, who will say we should not give to him the best information that can be secured?

I believe you will agree with me that in the past there has been occasion for dissatisfaction on the part of farmers because of the way in which they were treated by the local agents of the line elevator companies. It was this dissatisfaction which led to the organization of so many farmers' elevator companies throughout the grain belt. It is rather generally believed that the result on the whole has been good. The probability of successful co-operation in the next stage in the marketing of grain is not so clear. The local co-operative is under the eye of the farmer. He sees the manager and the plant in which he has invested his money every time he sells grain. In the second stage in marketing it is different. The agent is far away and free from constant inspection of the co-operators. Experiments are being made. It is not for us to encourage or discourage them, but it is our duty to furnish full information regarding practical plans of organization and the facts relating to the marketing processes, and to write the history of these experiments as a basis of future education.

The first thing the Grain Futures Act calls for is the gathering of facts. The plain statement of the truth may satisfy the farmers that all is well, that grain is being handled in an efficient manner and for a fair charge. In case the facts do not point to this conclusion in the first instance, the Act carries the basis for making helpful suggestions which will tend to rectify the situation.

While the Congress has asked the U. S. Department of Agriculture to administer the Grain Futures Act and the Packers and Stockyards Act, it has also asked the same Department to administer the Capper-Volstead Act. The former laws have for their purpose fair charges for middleman services and the latter law prohibits farmers from securing monopoly prices for the products through co-operative efforts.

It is through your co-operation that the facts can best be secured, so far as they relate to the marketing of grain. The facts clearly told will certainly give the general public a better opinion of the grain trade than any opinion they may hold based upon such information as they secure through unscientific and prejudiced channels. In the absence of thoroughly worked out scientific knowledge of the grain trade as a basis of the general information of the public, an unscrupulous agitator is able to secure a large following.

The administration of the Grain Futures Act has the possibility before it of rendering the grain trade of the United States a very great service by securing the facts and educating the public with regard to the functions performed and the services rendered by those who handle grain and by the organized markets in which business is transacted.

A. E. Reynolds, Crawfordsville, Ind., presented the report of the Legislative Com'te, from which we take the following:

Legislative Com'te Report.

As I see the situation, there is a general feeling of uncertainty, uneasiness and distrust throughout the entire business world. While business cannot succeed on confidence alone, no business can succeed without it. Everything came too easy during the war. We were spoiled. We now find ourselves hesitating and balking at the ordinary baffles and hurdles which have always beset business. Our people demand that war time prosperity be extended over into times of peace. Our misguided legislators strive to meet these demands.

Life for the business man is a little more strenuous than ever before. We are living under entirely new conditions. Old methods won't fit present new conditions. Everything has to be learned all over again. Lifetime experiences in business must be thrown in the discard as useless. We are bearing burdens of taxation never before dreamed of. All this is naturally disconcerting and discouraging. Business deserves a surcease from legislative attention. It is on a higher and more honorable basis than ever before. This is particularly true of the grain business.

The business world lives from day to day in dread of what may happen in Congress. Up to about fifteen years ago American business could confidently appeal to Congress for aid. Since that time, there has been an ever increasing tendency to legislate against business. This adverse legislation has been carried on under the guise of business regulation. Our lawmakers claim that business has been successfully regulated. This regulation in effect has been similar to the very successful surgical operation—the patient died. In conversation with Senator Stanley not long since he said, "American business cannot continue for another ten years to stand legislative assaults such as have been recently hurled against it."

We have been in a continual legislative turmoil since our last report. More bills have been introduced than ever before. The final summing up of the situation, however, shows

that we had a lot of smoke and little fire. The constant menace of something to happen keeps us continually on the anxious seat. The incessant barking does more harm than the bite.

The Cincinnati Meeting. Probably no single act of the Grain Dealers National Ass'n has caused so much commotion as this meeting. During the past few years so much false propaganda had been circulated against business in general, and the grain business in particular, and such strong efforts were being made for special class legislation, and so little had been done to counteract these baneful influences, that it was thought wise to start a campaign to educate the people to a true understanding of the business situation.

I call your attention to the wording of this Resolution for the reason that more falsehood has been uttered about the action taken at the Cincinnati meeting and subsequent events concerning it than all other activities of the Grain Dealers National Ass'n in the past five years.

On July 12, 1921, Senator Kenyon introduced Senate Resolution No. 110, to investigate the Cincinnati meeting and the action of the Association subsequent thereto. This Resolution is not only false and misleading in many of its statements, but bears the unmistakable earmarks of a deliberate attempt to smirch this Association. Not a word was said in the Cincinnati meeting about fighting Legislation for the relief of Agriculture. Notwithstanding this fact, the Senator in his preliminary whereases used this language: "On June 27, 1921, at Cincinnati, a Convention was held under the auspices of the Grain Dealers National Ass'n, at which a National organization was projected with the pronounced purpose of combating Legislation for the relief of the farmers of the country." Since no foundation in fact exists for this statement we must conclude that the Senator stooped to the level of the common, malicious demagogue to misrepresent our Association. It was a dastardly act and deserves the condemnation of all honest men.

You know the result of the Senatorial investigation of the Cincinnati meeting and of the work of the Mansfield Committee. The carpers and bleaters who instigated the movement were shown up in the proper light. They were sick of the job before they got through with it. Your Association came through the whole thing with a clean record.

In my judgment, the Cincinnati meeting and the work of the Mansfield Committee will stand as the greatest achievements of the Grain Dealers National Ass'n. At the hearings the whole proceeding on our part was dignified, truthful and manly and the falsehood and chicanery of our enemies was glaringly exposed. We have nothing whatever to regret regarding the Cincinnati meeting or the work of the Mansfield Committee.

The Capper-Tincher Bill, known as the Future Trading Act, which was in force at our last meeting, was on May 15, 1922, declared unconstitutional. Immediately thereafter the authors of the old Bill prepared a new Future Trading Act hoping to accomplish the same result aimed at by the old Bill and yet have it meet constitutional requirements.

The new bill has some objectionable features. Whether it will meet the same fate as the original bill remains to be seen.

County Agents: In my report last year I touched briefly on the County Agents. My remarks seem to have gone home—at least quite a little comment on my observations was forthcoming from some County Agents and from authorities higher up who have control of the County Agents' activities.

The principal objection found was based on my reference to the County Agents' activity as an agitator, creating unwarranted prejudice among country people against townspeople and legitimate business. My exact words were, "Instead of performing the legal function for which the position was created, in many cases he (the County Agent) has become a common demagogue and agitator and as such should be eliminated."

I see no reason to alter that opinion. It should not in any way offend the Agents to whom it does not apply. If the shoe fits any Agent he should wear it without complaint. The Agent is either doing his duty and following the law or he is not. He is either standing for co-operation of the whole community which he is supposed to serve, or he is not. He is either standing for peace and harmony in his County as between producer and business, or he is striving to stir up animosity. There can be no two sides to the question—no extenuating conditions or no palliating circumstances regarding the matter.

The misquotation of rates has grown to be a matter of serious moment to the shipper. The Interstate Commerce Act and various decisions of the Courts have combined to free the Railroads from all responsibility to shippers for losses on account of misquotation of rates.

Your Committee has been earnestly striving for a long time to have enacted an amendment to the law to cure this defect. Congressman Purnell introduced Bill R113 to amend Section 6 of the Interstate Commerce Act to cure this evil. The Bill was referred to the Committee on Interstate and Foreign Commerce and this

Committee referred it to the Interstate Commerce Commission. We have had several meetings with various Committees on this Bill. The Commission is very much in sympathy with the end to be accomplished. It realizes that the Railroads should be responsible for their acts. The main trouble, however, is that it feels that the passage of the amendment might open an avenue for rebating and as the rebating practice was one of the most serious difficulties the Commission has had to contend with, it is very loath to do anything that might lead to a renewal of this evil.

After very careful consideration it was agreed between the Commission and your committee that the Bill should be withdrawn and that in the meantime the Commission would take up the question of having railroad rates entirely revised and simplified. It was apparent that errors in quoting rates were largely occasioned by the complicated system of rate sheet making and that these errors might be avoided by simplified methods of quoting and publishing rates.

It was further determined that if the bill should be passed that the railroads would be perfectly justified in withdrawing from their agents at small country stations the authority to quote rates. This would necessitate a delay in securing rates until same could be quoted from the main office of the company. This delay would seriously inconvenience the shipper. The Commission is now actively at work on a plan of simplifying rate sheets. If these rate sheets can be so simplified as to make them easily understood by everybody it will meet all requirements. We think this will soon be accomplished.

Tariff: At this writing it seems reasonably certain that out of the smoke and turmoil of a twenty months Legislative struggle will emerge a tariff law.

As patriotic Americans let us hope that the law will be fairly satisfactory at least to our lawmakers to the end that they cease farther tariff tinkering. It is certain that it will be very unsatisfactory to a large majority of the people, but a bad law definitely in force is even better than a worse one threatened.

We can only determine how good or bad this law is to be by fair trial. The ramblings and meanderings of the Legislative mind on the tariff question are beyond the comprehension of the layman. No matter what a few Legislators may think of this tariff there seems to be a general agreement among the people that the measure will be short-lived and its existence extremely turbulent and that a political upheaval will mark its end.

Tariff tinkering is a dangerous pastime for any political party. In the present unsettled state of public mind everybody is seeking a scapegoat on which he may saddle his troubles. The revived tariff issue will be the suffering Billy on to which all troubles will be loaded. It will be made the excuse for everything that happens and for everything that does not happen. Personally, I look with extreme distrust on any tariff measure that ignores reasonable efforts toward establishing reciprocity with our neighbors. We produce in the aggregate vastly more than we consume. We always need a foreign outlet. The most vicious thing that can happen to American business is an over production with no foreign outlet. Foreign nations cannot buy our products until they sell some of their own. If a tariff closes our market to foreign sellers they automatically close their buying markets to us.

The U. S. Grain Growers' Inc.: What is to be said about it? Why say anything at all? All know how it was conceived, how it came into existence, how it has wasted its vituperative fragrance on the desert air, how it filched money from the unsuspecting farmer by slandering the Grain Trade, how it misrepresented that present methods were dishonest, that it would eradicate existing dishonesty in the grain business and install its own honest methods, how its paid agents stood on the village street corners and the country crossroads and sat by the farm fireside and Pharisee-like rendered thanks that they were not like other men—that they were honest—all for the ten dollars per which they filched from the farmer.

All know that they spent almost a million dollars thus secured or borrowed from Farm Organizations—all know they owe, according to their own statement, over \$286,000.00 so borrowed—all know that the movement was organized and brought into existence to correct imaginary evils; evils that never existed, that it was a delusion and a snare—all know how the farmers have generally become wise to the hollowness of the whole proposition—all are cognizant of its present tobooggan movement toward oblivion, so why say anything further about it?

Criticizing Congress: Our people are in a critical mood. Nothing pleases them. Congress seems to be the target for widespread criticism. This should not be. A wholesome regard and respect for our lawmakers and those in authority is the first elemental principle of democracy. Few people realize the vastness of the responsibility and labors of our Congress. Visitors to the galleries of either the House or Senate look on, listen for a few minutes, smile at the seemingly do-nothing methods being

pursued of which they know nothing and depart to offer ignorant criticism.

Let us briefly review the working of the House. This branch of our Congress is composed of 435 members. Just how do these 435 members proceed with Legislation? What do they have to do?

The troubles of the House are daily increasing. Some measure of relief must soon be devised.

Approximately 20,000 Bills and Resolutions have been introduced during the last session of Congress. These are distributed to the various committees, which committees are supposed to determine on their merit; to approve, disapprove or amend each bill and report same back to the House. In some cases bills are pigeonholed in the office of the Chairman of the Committee and pass into oblivion at the end of the session. If these bills all came back to the House for consideration that body as a whole could not give, on an average, three minutes consideration to each bill. A large number of bills are really jokes. About the only way an individual member has of showing the people at home that he is a man of importance in the House is to introduce a bill. Little difference what kind of a bill—just a bill.

Considering all that the House has to do and contend with, I think it does very well and should command our respect and admiration.

The same is true of the Senate. The work of both branches of Congress is very arduous. The responsibility is great, the remuneration small and the criticism and condemnation everlasting.

Are we lacking in men of genius in our Legislative bodies and high official places in the United States? This matter has been so freely discussed in political circles that no affront can possibly be taken by men in high positions in having it further aired.

It has been said that our primary elections tend to select men of mediocre ability. In some cases possibly true, but to the point—are we really lacking in men of genius? My most emphatic answer is NO. I believe that we have as large a percent of great men in public life as we ever had. Then what is our trouble? First, as I see it our trouble is that members of Congress are overcrowded with work. There is no time for genius to assert itself. No time can be given for proper study and investigation of public questions. No man can show the best that is in him when overworked.

Second, our people are less appreciative of real genius than ever before. In proportion to the number of members in the House, we have as many able, conscientious workers now as fifty years ago. There is as much patriotism and ability in the Senate as ever. In both bodies are faithful, able workers who fearlessly do what they conceive their duty. The best and ablest public servants are least appreciated. They are turned down because they dare to assert themselves and do their duty as they see it.

If we have boobs in Congress it is because they have enough boob votes at home to send them there. Our people get on the general average the kind of Legislators and the kind of Legislation that the majority want. That is just the reason why a genius in high places is not recognized. Our people are not willing to be led by men of genius. The commonality has come to believe that it possesses sufficient genius to direct Legislation. In the past few years a custom has grown up of giving the constituency at home a chance to be heard on every subject before Congress. The public generally knows nothing about the subject matter under consideration and vastly less about its relation to the whole scheme of government.

I entertain serious doubts about the advisability of the present custom of public hearings on all bills under consideration. It may be preferable to the old plan of individual lobbying but even that is questionable. The individual lobby only carries with it the influence of the lobbyist. The public hearing partakes more of the nature of organized effort to influence Legislators to do what the uninformed public wants done. These hearings are too often the big noise transported from the outlying districts of the country to Washington to represent to the Legislators (and quite as often to misrepresent) the feeling of the constituency on the proposed legislation and how its enactment may be expected to affect the forthcoming election. If it were possible for each Legislator to properly inform himself through channels of his own choosing on each public question we would fare much better.

By the process of allowing the people to pass on Legislation, Statecraft is brought down to the level of the commonality and Legislators of high genius and ability are overwhelmed and submerged, and we then come to the conclusion that genius is lacking. It is not lacking but it is not heeded. It has no chance to assert itself. The present process of making laws is democratic in the extreme. To where it will lead future experience only can determine. The process of hearkening to the people on every Legislative act reduces to the minimum the influence of men of genius and increases to the maximum the chances of cunning, crafty politicians who can most accurately judge of the trend of uneducated, misdirected public opinion at home. It puts a premium on the astute

politician and a handicap on the Statesman.

The public man of the day is he who can keep his ear closest to the ground and have it most accurately attuned to the radio rumblings from his constituency. With this state of affairs existing, with these methods being pursued, it is much more difficult for genius to manifest itself than ever before.

I believe that true genius still exists in high places, but the people refuse to be led or directed by it.

The strongest sign of the decline of our boasted civilization is the tendency of our people to disregard law, to refuse to be led and directed by men of high genius and mental attainment. The people are seeking the new reform method—the easy path.

This tendency has been called the rise of the under-man or the self-assertion of the middle classes. As I view it it is the submerging of the higher classes. It certainly is not an upward movement in any sense of the word. Our people are in open rebellion against law and authority. They refuse to submit to law. There is prevalent an idea that submission to law is an abridgment of personal liberty. The personal liberty idea has gone too far.

We must reach a compromise, a reconciliation between ideal personal liberty and established authority. The recent massacres in the mine riots in Illinois, the train wrecking and murder throughout the country in the railroad strike are all the direct result of conflicting ideas regarding personal liberty. Our boasted individual liberty is becoming a dangerous thing.

E. F. Huber's report as chairman of the Membership Com'ite was read by the sec'y. It follows:

Membership Com'ite Report.

Your Booster Com'ite wishes to report that as a result of the 1921-1922 canvass 142 new members have been brought into the Ass'n. At the beginning of the canvass we set a mark of 300 new members to be enrolled, but unfortunately from the effects of the World's War, and thru no fault of the Boosters, we did not reach this goal. The campaign began on Feb. 1, 1922, and was discontinued on May 26, after 102 new members were secured. Since May 26 we have enrolled 42 new members, making the total for the year 142 new members.

Taking everything into consideration, such as unsettled business conditions and deflation in grain prices, you will readily see that the work of the boosters for the fiscal year deserves highest praise. Hundreds of grain, feed, seed and milling firms have been forced to close their doors within the past 12 months, and it has been no easy task to secure new members. As a matter of fact, the 142 new members secured this year would equal about 350 members secured in previous yearly campaigns when conditions were normal and trade was prosperous.

Adolph Kempner, Chicago, Ill.: I resent the remarks about the members of the Chicago Board of Trade who fought the Capper-Tincher bill.

Adjourned for the boat ride.

Wednesday's Session.

Wednesday morning's session was called to order at 10:15 a. m. with a song led by Mr. Murrel.

W. R. Scott, pres. of the Southern Pacific Railroad Co., delivered an address in which he pointed out that the interests of the grain men and the railroads are interwoven "and it is to our interest to give the very best service possible."

He asked the co-operation of the grain dealers.

Mr. A. E. Reynolds asked the privilege of having stricken from the records a part of his remarks of the preceding day, with regard to the Capper-Tincher bill, which was granted.

Mr. Connally moved that all reference to the Ku Klux Klan be stricken from the Legislative Com'ite report, which motion brot on a heated debate and resulted in the defeat of Mr. Connally's motion by a vote of 20 to 18.

T. W. Hall, of Minneapolis, Minn.: We are going to fight the Capper-Tincher regulatory act to the last ditch.

Pres. Clement stated that in the absence of Soap Box Jack O'Brien he had granted a socialist radical 10 minutes of the time of the meeting. He did not know what he was going to say. The unknown was invited to step to the platform, and as he did so kept his cap on his head and he declined to remove it before "a capitalistic audience."

After indulging in a characteristic tirade against property rights and eulogizing communism until the audience was ready to mob him he suddenly stripped off his cap, yellow

goggles and red sweater and stood revealed as none other than Jack O'Brien himself.

He then proceeded to answer most effectively the alleged arguments in favor of collectivism put forth by his pseudo predecessor.

He cited failures of collective enterprises wherever tried.

The reading of the reports of the arbitration com'ite was dispensed with.

The report on "Merchant Marine," by C. B. Fox, chairman, was read by Mr. Watkins, and from it we take the following:

Merchant Marine Com'ite Report.

Of the advisability and necessity of maintaining and of further upbuilding the merchant marine, there can be no doubt, but the question as to whether this should be done through the instrumentality of the Shipping Board, or through individual American firms under the subsidy plan, is an open one and a basis for thought.

Before proceeding further, some statistics as to the volume of grain exported in American and foreign vessels will doubtless be of interest.

During the calendar year of 1920 American steamers loaded at Atlantic ports 28% as against 72% in foreign vessels; in 1921 they loaded 25% as against 75% in foreign vessels; and in the first six months of 1922, 31% as against 69% in foreign vessels.

At the Gulf the proportions were about the same—27% in 1920; 23% in 1921, and 42% from January 1 to June 30, 1922.

At the Pacific Coast ports, American vessels loaded 50% in 1920; 24% in 1921, and 17% from January 1 to June 30, 1922.

The total exports from the United States carried in American vessels were 30% in 1920; 24% in 1921, and for the first six months of 1922, 32%.

Of the total grain movement, wheat shipments in 1920 amounted to 67.7%, and in 1921 amounted to 68.5%.

This showing, considering the fact that the American merchant marine is practically in its infancy, is a very good one, and it is, therefore, of the utmost importance that means be devised whereby the good work may be continued in some way or other.

The efforts of the Shipping Board in the upbuilding of our merchant marine have been most laudable and have undoubtedly resulted in a large expansion of American trade in all quarters of the globe. In nooks and corners where the American flag was unknown it has made trade possible by taking parcels of grain, flour, etc., to ports where full cargoes were unworkable, and, by sending its steamers from port to port has opened up markets which were previously supplied by other countries.

The results obtained in so far as American commerce is concerned show the absolute necessity of not only maintaining but of increasing our merchant marine.

Your Com'ite realizes, however, that the Shipping Board in order to do this must, like any business concern, find a way to operate its vessels without loss, but such operation must not, as far as grain products are concerned, be at the expense of the farmer, the grain or milling industries.

The Merchant Marine Act of 1920, otherwise known as the Jones Bill, contains a section, No. 28, which provides that export rates on grain or other commodities shall apply only to such goods as are to be transported abroad by vessels documented under the laws of the United States, or, in other words, American bottoms. This means that the domestic rates would apply on shipments by foreign vessels, and as these domestic rates are in many instances higher from 2 to 14 cents per 100 lbs., it can be readily seen that foreign vessels could not compete.

The enforcement of Section No. 28 would be harmful also because foreign ships would seek Canadian ports, through which already large quantities of American grain are handled, and still larger quantities would be diverted there to the detriment of American ports. So far Section No. 28 has not been enforced, but the danger exists, for the Act was passed and it behooves grain men to keep their eyes open.

Competition in the matter of ocean freight is a necessity and our export trade must not be handicapped by restrictions as above.

Interior traffic arrangements and conditions should not be disturbed.

Doubtless many Congressmen were not aware of the significance of Section No. 28 when they voted for the passage of the Jones Bill.

The elimination of foreign competition means higher ocean rates and consequently less money to the producer for his grain. It stands to reason that if an exporter can obtain a lower rate of freight by foreign than by American vessels, he can afford to pay more for his grain, the difference going into the pockets of the American farmer instead of the Shipping Board.

It may not be generally known, but it is, nevertheless, a fact that the Shipping Board from time to time holds conferences with agents of foreign steamship lines, relative to ocean rates, and in this manner is instrumental in having

rates maintained, or advanced, as the case may be. The foreign lines trail along pleased at the idea of having the Shipping Board uphold rates for them. At times, however, they have been willing to make reductions, but as parties to said conference, were compelled to abide by the so-called "conference rates." It does not seem right that the Shipping Board should dictate the policy of the foreign steamship lines, as we feel that the latter should be permitted to take any rate they please, for the lower the rate the better the price for the American producer.

Our merchant marine must stand on its own bottom and be placed in a position to compete with foreign tonnage.

There is no doubt but that the present Shipping Board fleet has the foundation of a first-class merchant marine, but there are a lot of Government owned vessels which it does not pay to operate. All such craft as this should, in our opinion, be sold regardless of cost, for deterioration has undoubtedly set in and these ships cannot be operated except at a very heavy loss.

We are of the opinion that the Government should retire from the operation of merchant vessels, and we favor the sale of the entire Shipping Board fleet to American firms, who would agree to operate them for a term of years under a fixed subsidy. It is very evident that no matter how experienced these individual firms might be in the handling of ships, they could not, without subsidy, operate except at a loss, on the basis of ocean freight rates now ruling.

Subsidy.—We repeat that the American merchant marine should stand on its own bottom and be operated on a competitive basis. The only practical way for this is by subsidy, which could be so adjusted as not only to protect the owners from losses, but to allow them a reasonable profit, in order to encourage them in the purchase and operation of existing tonnage and the building of additional tonnage for the expansion of American trade.

Chas. D. Jones asked if the adoption of the report would commit the ass'n to indorsement of a ship subsidy; and objected to increased taxation.

Mr. Dorsey argued in favor of a subsidy.

The report of the Com'ite was adopted.

Mr. Dorsey moved a reconsideration of the decision to take a ballot by mail on the preceding day. *Carried.* The proposition, embodied in Rule 16, was tabled.

John S. Green, for the com'ite on recommendations in the sec'y's report, made the following report, which was adopted:

After consideration of Secretary Quinns' suggestions with reference to:

The Committee on Telephone and Telegraph Service;

The appointment of a committee on rejected applications, and

The matter of the violation of Section 5, of the Grain Standards Act;

The Committee recommends that the matter of telephone and telegraph service be handled by the Transportation Committee; that there shall be eliminated from Section 1, of Article 1, the following words: "A Committee on Telephone and Telegraph Service," and add "A Committee on Rejected Applications."

Repeat Section 6, of Article 2.

Amend Section 1, of Article 2, by inserting immediately after the word "facilities," in the sixth line, the words "Telephone and Telegraph Service."

Insert as article 6, of Section 2: The Committee on Rejected Applications shall consist of three members appointed from the Board of Directors, and their duty shall be to investigate all applications which have received one or more negative votes and report their findings and recommendations to the Secretary.

Add to Article 4, a Section 4, as follows: "Any member found guilty by the Department of Agriculture of a violation of Section 5 of the Grain Standards Act shall automatically stand expelled from the Association."

The Committee presents for your consideration and action the above changes in the By-Laws of the Association.

Tom M. Connally, of Clarendon, Tex., chairman of the auditing com'ite, reported the accounts of the sec'y and treas. correct, and recommended that same excellent method of accounting be continued.

Adopted.

E. C. Eikenberry presented the following report of the Resolutions Com'ite, which was adopted practically without discussion:

Resolutions.

Taxes and Reduction of Number of Government Employees.

Whereas: The crying complaint of the American people today is against excessive taxation, both State and National, and

Whereas: The grain trade of the United States, in convention assembled, recognizes and shares in this increasing burden, therefore, be it
RESOLVED: That the Grain Dealers Na-

tional Ass'n does now go on record as endorsing the budget system of control of Government expenditures and as favoring every possible measure of economy and conservation in government, both State and National, and we specifically urge on the national government the necessity of immediate and large reductions in the personnel of the employees of said government now engaged in the transaction of public business throughout the various departments of our government, believing that this is the first and most essential step toward such economy.

Oppose Cancellation of Debts Owed U. S. by Foreign Nations.

Whereas: Certain interests in this country are endeavoring to create a public opinion to support legislation for the remission of the debts due by foreign nations to the United States, and

Whereas: Such remission of the debts would not be charity nor an expression of national good will, but a contribution toward the continued maintenance of the military establishments of Europe, therefore, be it

RESOLVED: That the Grain Dealers National Ass'n declares its opposition to any policy of cancellation of debts owing to the United States by foreign nations.

Deplore Class Legislation.

Whereas: The wisdom of our fathers founded here a republic in which all men should have equal rights and special privilege should be granted no man, on which principle of equality of opportunity the very foundations of our governmental structure rest; therefore, be it

RESOLVED: That we deplore the appearance of class consciousness as manifested in the formation of groups or "blooms" in our National Congress, each striving by force of numbers and practical politics to secure special legislation. Be it

RESOLVED: That in Convention assembled the Grain Dealers National Ass'n declare as a policy that it will at no time or under no circumstances request or advocate legislation benefiting the grain trade to the detriment of the rest of the body politic and that it will never lend its moral or financial support to the promotion of such legislation whatever its origin or object, and whoever may be the beneficiary.

Demand Repeal of Laws Regulating Legitimate Industry.

Whereas: The Congress of the United States is pursuing a policy of assuming regulatory and supervisory control of the commercial and industrial activities of the Nation, continually extending its authority to the inclusion of lines of business having no visible relation to government and which owe their existence to the freedom extended by our Constitution to all legitimate enterprise with assurance that the exercise of initiative and organization attended by honesty and economy of operation shall not be hampered or be denied its reward, and

Whereas: Many of these acts are not only regulatory but are inquisitorial in their nature, prying into the most intimate affairs of individuals and companies of individuals, demanding extended reports and divulgements of details of operation repugnant and distasteful to the spirit of American freedom and subversive of the accepted relations of a benevolent Government to its subjects, and

Whereas: These enactments have resulted in the creation of numerous Bureaus, Boards and Commissions with a horde of agents exercising authority of inquisition, supervision and censorship, which threaten to transform our Government from a democracy to a Bureaucracy, stifling business activity, stifling individual and corporate initiative and weakening the solid and substantial support the Government should receive from the industrial interests of the country, and

Whereas: We firmly believe that in this period of uncertainty business should not be hampered in its activities by unwarranted Governmental interference, that it should be loosed and not bound, that it should be extended a generous confidence and not treated with discouraging distrust, and that true statesmanship dictates a recognition of the strength the commerce and industry of the nation contribute to its existence and its prestige in the family of nations; therefore, be it

RESOLVED: That we demand the cessation of the assumption by the Government of regulatory and supervisory authority over the legitimate business and industrial activities of its citizens with the attendant creation of bureaus and commissions to execute such supervision and that we strongly urge the repeal of all laws now on the statute books, which have for their purpose the regulation or supervision of lines of industry that are legitimate in their nature and the operation of which is not inimical to public policy or the general welfare; and thus automatically abolish the attendant executive bureaus and commissions delivering business from political enthrallment and in a wholesome manner reducing the expense of Government, and, further, be it

RESOLVED: That this Ass'n call upon other business interests of the Nation to join in an effort to relieve ourselves and industry in general of the annoyance, discouragement and ignominy of bureaucratic control, and, be it

RESOLVED: That a copy of this resolution be mailed to the Secretary of Commerce, the Secretary of the Treasury, the Secretary of

Agriculture and to the floor leaders of both major parties in the Senate and the House of Representatives of the National Legislature.

For Repeal of Capper-Tincher Law.

Whereas: The Congress of the United States has enacted a law known as the Grain Futures Act, the purpose of which is to regulate and supervise the Grain Exchanges of the country, and

Whereas: This law seeks to rob the exchanges of their vested rights to govern themselves and maintain control of their own affairs, confiscating to a great extent the value of membership in these exchanges and breaking down the competitive system on which exchange trading is conducted, impairing the functioning of these agencies and disrupting the methods evolved by the trade during a period of over half a century as being the most efficient and economical in conducting the distribution of the great grain crops of America; therefore, be it

RESOLVED: That the Grain Dealers National Ass'n in convention assembled hereby declare opposition to this destructive and insidious piece of legislation and pledge its support to such measures as the Exchanges may deem advisable either to secure a repeal of this Act or test its constitutionality in the highest courts.

Require Wire Companies to File Tariffs.

Whereas: The Transportation Act of 1920 does not compel telephone, telegraph and cable companies to file tariffs of rates and charges, rules, regulations and practices for all classes of service with the Interstate Commerce Commissions; therefore, be it

RESOLVED: That we endorse the bill introduced May 27th by the Honorable Walter H. Newton of Minnesota in the House of Representatives and known as H. R. 11822, and request our members to request their representatives in Congress to vote for this bill.

Oppose Shrinkage Allowances Not in Accord with Docket 9009.

RESOLVED: That we oppose such portions of the rules proposed by the carriers to be placed in the tariffs covering weights and allowances for shrinkage and variations in scales which are not in accord with I. C. C. Docket 9009, and we earnestly protest that I. C. C. permit any tariffs to become effective containing such provisions.

Make Docket 9009 an Order.

Whereas: The Grain Dealers National Ass'n has instructed Chairman Goemann to request and urge the I. C. C. to issue its report and opinion in I. C. C. Docket 9009 as an order, and

Whereas: The Commissioners personally handling this whole matter at the time the investigation, report and opinion was made, which fact will necessitate extraordinary effort and work, be it

RESOLVED: That the Board of Directors of the Grain Dealers National Ass'n is hereby requested and directed to authorize and instruct Chairman Goemann to employ such assistants and counsel as in his judgment is necessary to secure such action by the Commission.

RESOLVED: It is the sense of this meeting that Mr. Henry L. Goemann in presenting this matter to the Interstate Commerce Commission recommend and urge that the report and opinion of the Commission in Docket 9009 be promulgated as an order by the Interstate Commerce Commission.

Endorse Hopper Scale Specifications.

RESOLVED: That we endorse the specifications adopted March 13, 1922, by the special Joint Meeting called by the Weighmasters Scalemen's Conference of all interested in the specifications for hand operated hopper scales when amended to meet the approval of the U. S. Bureau of Standards, and that we earnestly request the Bureau to pass upon them immediately.

Condemn County Agents' Activities.

Whereas: This Ass'n has consistently insisted on a limitation of the activities of county agents to the functions prescribed in the creation of their office, and

Whereas: The Department of Agriculture has recognized the justice of our demands and that there is apparent a marked improvement in the county agent service; therefore, be it

RESOLVED: That we express our commendation of the action of the Department of Agriculture and instruct the Secretary of this Association to convey to the Secretary of Agriculture an expression of appreciation of the spirit he has shown toward preserving the interest of the county dealer in grain and grain products.

Resolutions were adopted thanking the individuals and organizations who had contributed to making the convention a success.

Vice Pres. Watkins took the chair temporarily while A. E. Reynolds on behalf of the ass'n presented Mr. Clement with a solid silver platter as a token of appreciation of his work as president of the ass'n the past two years. Mr. Clement responded with feeling.

Mr. Green, for the com'te on nominations, presented the following list of officers and 2-year directors, and they were unanimously elected:

Pres. F. E. Watkins, Cleveland, O.; 1st v. p.,

F. G. Horner, Lawrenceville, Ill.; 2d v. p., Elmer Hutchinson, Arlington, Ind.

Directors, J. W. McCord, Columbus, O.; G. B. Wood, Buffalo, N. Y.; H. L. Shellenberger, Geneseo, Kan.; I. C. Edmonds, Marcus, Ia.; W. T. Hale, Jr., Nashville, Tenn.; H. W. Reimann, Shelbyville, Ind.; H. A. Rumsey, Chicago, Ill.; John S. Green, Louisville, Ky.; W. M. Randels, Enid, Okla.; W. W. Manning, Ft. Worth, Tex.; Reeve Harden, Hamburg, N. J.; W. Carey Cook, Fort Collins, Colo.; L. W. Forbell, New York, N. Y.; H. R. Wilber, Jamestown, N. Y.; and E. M. Wayne, Delavan, Ill.

Adjourned *sine die*.

The Boat Ride.

Notwithstanding the continuous downpour, the visiting dealers made their way to the foot of Canal Street and boarded the Steamer L. S. Thorn, already heavily laden with luscious food and refreshments. Not only was the steamer decorated, but all the boats in harbor were especially decorated for the occasion and as the visitors were taken up and down the extensive harbor they were greeted on all sides by loud whistles and waving of flags.

Music was provided for the dancers between eats and those interested in shipping were given a comprehensive survey of the extensive facilities of the second port of the land.

The weather man was denounced as a dub of the first order and the dealers enjoyed themselves in spite of his perversity.

The Smoker.

When the visiting grain dealers filed into the convention hall on the evening of Oct. 3 they found the chairs had been replaced with five long tables to bear the burden of the large platters laden with sandwiches of various kinds, rolls and cheese, as well as potato salad. A crabmeat cocktail was the first course. Bottled beer was in constant supply. The cigars were passed around.

But the guests were there not to feast but to enjoy the entertainment placed on the stage.

An orchestra made the most of all opportunities to play between the various numbers, which included a quartet, ballet dancer, soloist, negro minstrel, an Oriental dancer, several good vaudeville acts, a 4-round boxing match and a battle royal between a dozen negroes.

The Corn Belt Quartet of visiting grain dealers broke into song several times, with hits at several well known men, and informed all present "There may be flies on some of you guys, but there are no flies on Rene LeClerc."

Post Convention Gathering at Board of Trade.

The day following the convention the visitors remaining gathered at the Board of Trade building and in a meeting opened by F. E. Watkins, the new pres. of the Grain Dealers National Ass'n, expressed their appreciation of the hospitality of the local grain dealers.

John S. Green of Louisville, Ky., told of the spirit shown by the men on the various local com'tes. He quoted the Grain Dealers Journal as having said New Orleans has enough facilities to handle all of the grain that goes out of the country.

Geo. A. Wells, of Des Moines, Ia., added a voice of appreciation.

Henry L. Goemann of Mansfield, O., told of opportunities to develop the port.

Mr. Schultz urged the development of the waterway because of what it means to St. Louis as well as to New Orleans.

W. L. Richeson of New Orleans reviewed the history of port development, telling of the floating elevator and barges of 25 years ago, and stated that a million bushel annex to the new Public Elevator is contemplated.

R. C. Jordan, of New Orleans, said the city has facilities for handling corn that are unequalled, with sufficient drying facilities to take care of any off grades in the winter and spring and dry it down to No. 2.

Entertainment of the Ladies.

New Orleans hospitality in 1913 resulted in the visiting ladies having such a splendid holiday that the number of ladies in attendance this year was unusually large. The reception committee which established headquarters in parlor E of the Hotel Grunewald was composed of Mrs. W. L. Richeson, chairman; Mrs. E. Nathan, Mrs. R. W. Warriner, Miss J. Warriner, Mrs. R. C. Jordan, Mrs. M. E. Whitehead, Mrs. James Thoma, Miss Inez Herring, Mrs. Chas. Sanford, Mrs. Jonn Sanford, Mrs. George Colby, Miss Ruth Colby, Mrs. G. W. Anderson, Mrs. W. H. Hendren, Mrs. W. H. Cowley, Mrs. M. L. Costley, Mrs. H. L. Daunoy, Mrs. J. Finke, Mrs. G. P. Gaiennie, Mrs. J. S. Waterman.

The program of entertainment included an automobile trip for ladies to many points of interest about the city, on the afternoon of the first day.

On the evening of the first day the ladies were given a theater party at the Strand.

The evening of the second day they were taken to the Orpheum.

On the morning of the third day they were taken on a walking trip through the historical French quarter of New Orleans in charge of the famous lecturer, Miss Flo Fields, who has established a national reputation for her many interesting articles on New Orleans and its early inhabitants.

The morning walk ended at the Patio Royale where a splendid luncheon was served in the court of the old Paul Morphy home. Miss Fields acted as toastmaster and after several stories introduced Mrs. Christian Schertz, who has also written much regarding the old French quarter and creole days.

On the evening of the last day, after the convention adjourned, the dealers and their ladies were tendered a Bouffet Dance at the Southern Yacht Club. As was usual at every turn the grain dealers were given more eats and more refreshments. The dancers remained until long after midnight and voted the three days continual round of pleasure a delightful success.

The ladies present included Mrs. F. G. Horner, Lawrenceville, Ill.; Mrs. Frank Ware, Butler, Ill.; Mrs. B. B. Bishopp, Sheldon, Ill.; Mrs. H. W. Reimann, Shelbyville, Ind.;

Mrs. M. King, Sioux City, Ia.; Mrs. Geo. A. Wells, Des Moines, Ia.; Mrs. T. W. Swift, Detroit, Mich.; Mrs. T. W. Hall, Minneapolis, Minn.; Mrs. F. R. Durant, Minneapolis, Minn.; Mrs. T. A. Bryant, St. Louis, Mo.; Mrs. C. A. Wilson, St. Louis, Mo.; Mrs. J. A. Linderholm, Omaha, Neb.; Mrs. F. H. Brown, Omaha, Neb.; Mrs. J. J. Rammacher and daughter, Buffalo, N. Y.; Mrs. P. C. Miller, Celina, O.; Mrs. F. A. Miller, Chicago, Ill.; Mrs. S. S. Kerr, Nashville, Tenn.; Mrs. C. M. Carter, Fort Worth, Tex.; Mrs. B. K. Smith, Fort Worth, Tex.; Mrs. J. Jockusch, Galveston, Tex. Mrs. J. W. Winchester, Galveston, Tex.; Mrs. A. Breed, Milwaukee, Wis., and Mrs. H. H. Peterson, Milwaukee, Wis.

Convention Notes.

Four hundred and fifty-two registered at headquarters.

The Corn Belt Quartet were omnipresent and ever welcome.

The New Orleans lodge of Elks extended the courtesies of its home to the visitors.

H. W. Donnan, supt. of the mill and elevator department, represented the Hartford Fire Ins. Co.

Continuous market quotations were posted by the Fulton Bag Co. in a room adjacent to the convention headquarters.

The generous occupant of Room 1265, who welcomed all visitors with open arms every time they chose to call has moved.

State Ass'n Sec'ys in attendance included J. W. McCord of the Ohio Ass'n, Geo. A. Wells of the Iowa Ass'n and H. B. Dorsey of the Texas Ass'n.

ELEVATOR SUPPLY firms were represented by J. F. Bassett, Chicago; M. A. Cook, Moine; A. C. Carpenter, St. Louis; J. W. Vaughan, Memphis; C. F. Egan, Fort Worth.

SOUVENIRS distributed at the convention were pencils by B. C. Christopher & Co. and Hodgson-Davis Grain Co. An interesting booklet entitled "Our Competitors," by But-ton-Simmons Grain Co., and a Flying Whistle by the Fulton Bag Co.

Grain Exchange Sec'ys in attendance included E. D. Bigelow of the Kansas City Board of Trade, Eugene Smith of the St.

Louis Merchants' Exchange, F. P. Manchester of Omaha Grain Exchange, Jno. R. Lofgren of the Peoria Board of Trade and H. S. Herring of the New Orleans Board of Trade.

GRAIN INSPECTORS present from the various markets were: A. E. Hawn and J. A. Noble, Chicago; A. A. Breed, Milwaukee; F. B. Tompkins, Peoria; S. A. Holder, Indianapolis; Paul Larson, Sioux City; L. D. Irving, Louisville; D. H. Larkin, Baltimore; M. C. Fears, Kansas City; P. D. Connors, Buffalo; Laurel Duval, New York; H. A. Wickstrom, Galveston; C. F. Rake, Texas City.

Dealers in Attendance.

From Jacksonville, Fla., came J. W. Snyder.

From Enid, Okla., came A. R. Hacker of the Enid Milling Co.

From Buffalo, N. Y., came Geo. B. Wood and J. J. Rammacher.

From Georgia came M. H. Haym, Atlanta, and W. J. Crosswell, Atlanta.

From Alabama came E. Wilkinson, Birmingham; E. J. Donahue, Mobile; R. M. Hobbie, Montgomery.

From Baltimore, Md., came W. S. Burton, C. P. Blackburn and J. A. Peterson.

From Kansas came P. M. Clarke, Arkansas City; F. J. Hipple, Hutchinson; E. Adams, Topeka; A. F. Baker and S. P. Wallingford of Wichita.

Kentucky was represented by the following from Louisville: J. B. Green and H. A. McLean.

From Milwaukee, Wis., came A. A. Breed, M. H. Ladd and H. H. Peterson.

From Minneapolis, Minn., came F. R. Durant, T. W. Hall and E. F. Huber of the McCaull-Dinsmore Co.

From Michigan came T. W. Swift, Swift Grain Co., Detroit; R. C. and Geo. Smith, Lake Odessa; and Jack O'Brien, Penobscot.

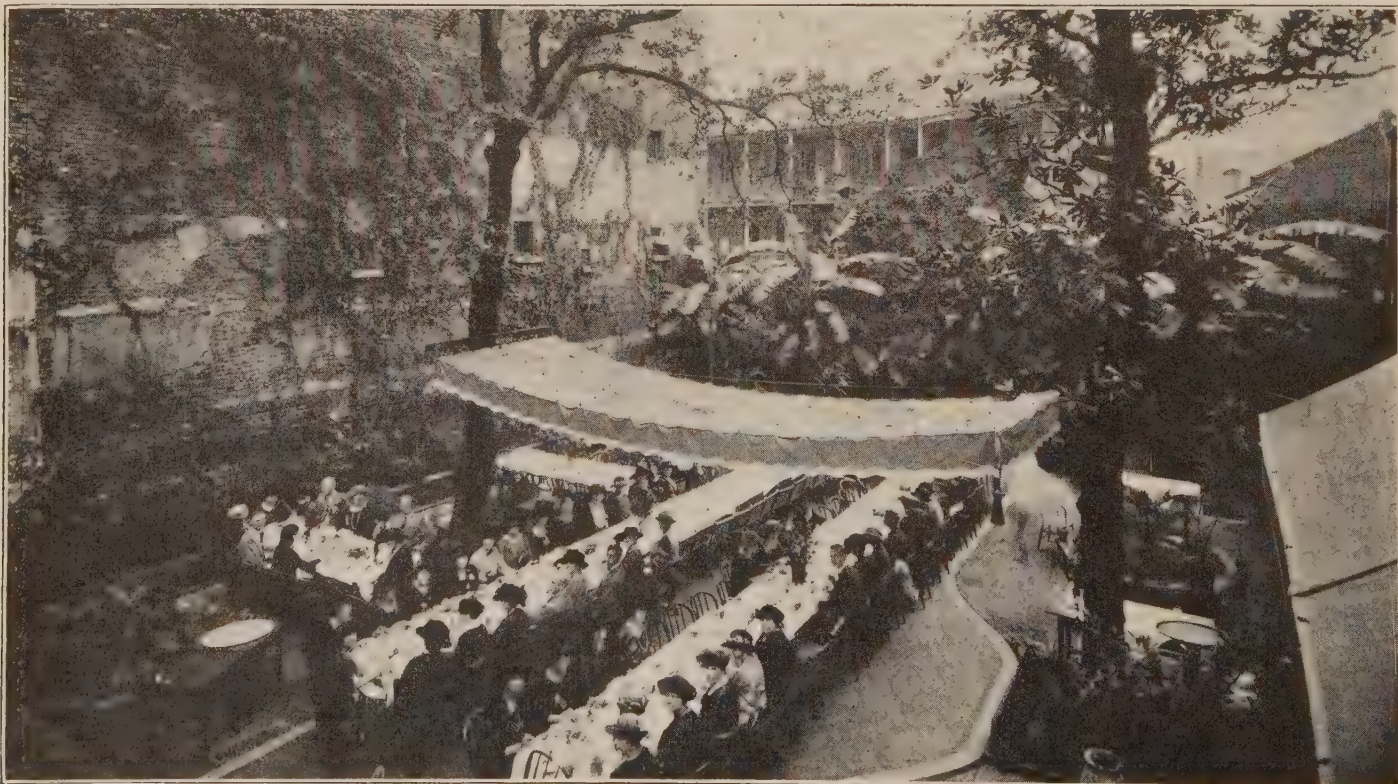
From Philadelphia, Pa., came F. Graff, H. J. Horan, G. M. Richardson, James J. Rodgers and W. B. Stites.

From Tennessee came C. G. Robinson of Scrugg-Robinson Co., Memphis; R. W. Hale, C. D. Jones, S. S. Kerr and H. Williams, Nashville; E. C. Huffman, Shelbyville.

Cairo, Ill., was represented by J. B. Gillespie, Jr., of the Halliday Elevator Co., O. B. Hastings of Sam'l Hastings Co. and W. F. Hartmann.

Chicago's delegation included Geo. E. Booth, Lamson Bros. & Co.; E. M. Coombs, J. C. Shaffer Grain Co.; E. A. James, Armour Grain Co.; H. A. Rumsey, Rumsey & Co.; H. M.

[continued on page 474.]



Luncheon Was Served the Ladies in the Patio Royale Courtyard, after a Trip thru the Historical French Quarter.

Grain Trade News

Reports of new firms, changes, deaths, casualties and failures; new elevators, improvements, fires and accidents are welcome. Let us hear from you.

ARKANSAS

Eldorado, Ark.—I have been out of business since last winter.—Elbert Rider.

CALIFORNIA

Turlock, Cal.—The A. B. Shoemaker Co. of Modesto has opened a branch office at this point which will deal in grain, feeds and beans. The new office will be under the management of C. J. Stanley, who is also general mgr. of the company.

CANADA

Daysland, Alta.—The new plant of the Maple Leaf Mfg. Co. has been completed.

Kilwinning, Sask.—An elvtr. here of the National Elvtr. Co. Ltd., of Winnipeg has been opened.

Vegreville, Alta.—The National Elvtr. Co. Ltd. of Winnipeg, opened an elvtr. at this point last month.

Alliance, Alta.—C. L. Hall, who has been in charge of the elvtr. of the United Grain Growers, is now located in Vancouver, B. C.

Kandahar, Sask.—The elvtr. at this point of the National Elvtr. Co., Ltd., of Winnipeg, was burned a short time ago. It will be rebuilt.

Fort William, Ont.—Work has been completed on the 2,000,000-bu. annex to the plant of the Northwestern Elvtr. Co., and the machinery is now being installed.

Mervin, Sask.—The plant of the Brooks Elvtr. Co. was burned last month, together with about 7,000 bus. of oats. The loss is insured and the company is planning immediate rebuilding.

Winnipeg, Man.—James Stewart, pres., of the Maple Leaf Mfg. Co. of this city, will open an office in New York City in connection with the newly formed grain company to be known as the James Stewart Grain Corporation.

Mennon, Sask.—P. P. Abrams, paymaster for the National Elvtr. Co. Ltd. of Winnipeg, was held up recently, and relieved of \$1,400. Thieves, probably the same ones, also blew their way into the safe of the Quaker Oats Co. at this point and were successful in finding about \$3,000.

Fort William, Ont.—Archie Bruce has been appointed superintendent of the Gillespie Terminal Elvtr. Co. Ltd., to succeed Neil Black, who died in August. Mr. Bruce was formerly on the staff of the Government Terminal Elvtr. here; he has been spending the last two years in South America.

Carmangay, Alta.—The elvtr. of the Terwilliger Grain Co., containing about 3,000 bus. of wheat and 1,200 bus. of rye, was burned Sept. 30. The plant of the National Elvtr. Co. which adjoins the Terwilliger property, was also burned. It contained 6,000 bus. of wheat. Losses are insured.

Edmonton, Alta.—Jas. Richardson & Sons, Ltd., of Kingston, Ont., have opened a branch office at this place which is located in the C. P. R. Bldg., and will be conducted under the management of W. C. McLean. Richardson & Sons also have offices at Toronto, Fort William, Winnipeg, Calgary and Vancouver.

COLORADO

Denver, Colo.—The Colorado Mfg. & Elvtr. Co. has increased its capital stock from \$6,000,000 to \$10,000,000.

Wellington, Colo.—The elvtr. being erected for the Fort Collins Flour Mill will be covered with corrugated galvanized iron.

Holyoke, Colo.—G. E. Schoonover will be in charge of the 42,000-bu. elvtr. of the Colorado Mfg. & Elvtr. Co. recently completed.

IDAHO

Soda Springs, Idaho.—The Idaho Grain Co. will establish an elvtr. at this point.

Malad City, Idaho.—There has been one new elvtr. completed here this year belonging to the South Idaho Wheat Growers Ass'n. Steve Staynor is mgr.—E. R. Williams, mgr. Inter-Mountain Mfg. Co.

Idaho Falls, Idaho.—The plant of the Idaho Falls Mfg. & Elvtr. Co. was burned Sept. 26 at a loss of \$100,000 which is covered with about \$65,000 insurance. The warehouses were only slightly damaged but the mill and elvtr. were completely destroyed. It is believed that the plant will be rebuilt.

Paul, Idaho.—The main floor of the elvtr. of the Paul Mill & Elvtr. Co., of which F. H. Adams is mgr., caved in last month causing about 5,000 tons of wheat to fall to the basement. Because cars for shipment were unobtainable, the great quantity of grain was stored loose in every available space, causing the disaster. The damage is estimated at \$3,000.

ILLINOIS

Ocoya, Ill.—Rufus Mutchler's elvtr. burned Sept. 28.

Chatsworth, Ill.—The Delaney Grain & Lumber Co. has been dissolved. The company sold its elvtr. in 1919.

Niantic, Ill.—The Niantic Farmers Co-op. Co. is planning to sell its elvtr. The company was reorganized last year.

Alton, Ill.—The mill and elvtr. of the Sparks Mfg. Co. was slightly damaged by fire starting from a hot bearing Oct. 3.

St. Anne, Ill.—Joseph W. Bettendorf's corn crib was burned Sept. 25, slightly damaging the elvtr. with which it was connected.

Hindsboro, Ill.—The Wheatly Grain Co. which recently purchased the elvtr. of Porterfield & Son, will hereafter be known as Carlos Wheatly & Son.

Cissna Park, Ill.—We took possession of the grain and coal business of the Consolidated Grain Co. at this point on Oct. 2.—E. W. Davis, Davis Bros., Galesburg.

Elkhart, Ill.—There is no such company at this station as the E. B. Conover Co., as reported. The elvtr. formerly owned by E. B. Conover is being operated by me personally.—J. A. Havey.

McKays Landing (Glasford p. o.), Ill.—The Peoria Grain & Barging Co. of Springfield and Havana, writes that the report that the elvtr. of the company at this point would be taken down is untrue.

Tuscola, Ill.—Two walls of the elvtr. of the Tuscola Grain Co. gave way Sept. 26 causing about 6,000 bus. of corn to be spilled on the ground. Ernest Orndorff of Mattoon and Wilbur Goodspeed of this city, are the owners.

Wapella, Ill.—I am moving to Monmouth, but at present have nothing definite in view. I do not think I will enter the grain business again for at least a year.—J. C. Boyer.—The Delaney Grain & Lumber Co. recently bot Mr. Boyer's elvtr.

Kemp, Ill.—S. Munson, senior member of the firm Munson & Moss whose elvtr. was burned Sept. 7 at a loss of \$20,000, has decided to retire from the grain business. It is believed that Mr. Moss will continue the business, but plans for rebuilding the plant have not, as yet, been made.

Ohlman, Ill.—M. I. Virden, at present mgr. of the Ohlman Co-op. Co. where he has acted in that capacity since 1906 for Jas. F. Umpleby and his successors, the Ohlman Co., has resigned his position effective Nov. 1. Mr. Virden and his brother, Ira Virden of the Pana Hay Co. of Pana are retiring from the hay and grain business and will enter the hardware business at Columbus, Ind.—Ohlman Co-op. Co.

Fairmount, Ill.—A. W. Cast of Danville has purchased a fifth interest in the Fairmount Grain Elvtr. Co. and will act as mgr. of the elvtr. here to succeed Z. A. Terry. His family will also make its home here. The company is reopening its elvtr. at Bennett Station (Oakwood p. o.) shortly.

Galesville, Ill.—John Hays, a partner in the grain firm of Hays Bros., died last month after an operation for cancer of the stomach, at the age of 50 years. Mr. Hays was born in Dayton, O., but had lived in Illinois since he was ten years old. He is survived by three boys, two brothers and a sister.

Palestine, Ill.—Our fire of Aug. 20 was in the office, the only damage being one desk, part of the floor and one partition and damage by smoke and water to the walls and some of our records. The loss was fully covered by insurance. We contemplate installing an oil engine this winter, also additional drives and feed grinding machinery.—Miesenhelder Bros.

Ottawa, Ill.—The South Ottawa Co-op. Grain & Supply Co. of South Ottawa, has brot suit for \$10,000 against the New Amsterdam Casualty Co. to collect on a bond given to prevent any defaulting on the part of Supt. A. G. Zenor, who is alleged to have speculated on the Board of Trade, incurring a \$30,000 loss. As the bond is for \$10,000, the grain company cannot collect more than this amount. No action is being taken against Mr. Zenor personally.

CHICAGO NOTES.

Memberships in the Board of Trade are selling for \$5,500 and \$5,600.

The firm, Mayfield & Co., will hereafter be known as Mayfield & Co., Inc.

Fred F. Breckenridge has succeeded L. N. Perrin as first vice-pres. of the Nye & Jenks Grain Co.

A new blackboard has been added to the facilities of the smoking room of the Board of Trade, giving the future delivery quotations of Winnipeg, Minneapolis and Kansas City.

The following memberships in the Board of Trade have been transferred: Howard H. Logan, J. B. Donahoe, E. O. Lloyd and the estates of I. P. Rumsey and A. J. White.

The following have been elected to membership in the Board of Trade: David M. Minton and Martin B. Jones of New York; Harold O. Hunt of Indianapolis, and George A. Chapman of this city.

Thomas Chandler Edwards, founder of the firm Chapin & Edwards in 1877, died the latter part of September. Mr. Edwards was an active member and a director of the Board of Trade until 1913 when he retired.

The Uhlmann Grain Co. has been incorporated for \$250,000 to deal in grains, cereals and agricultural products. Incorporators: Richard, Fred and Hattie Uhlmann. This company will operate the M. K. & T. elvtr. being erected at Kansas City.

Creditors of E. W. Wagner & Co. have been ordered by the U. S. District Court to appear in court at New York Oct. 27 to show cause why an order should not be entered ordering all holders of securities or claimants to file their claims before Dec. 1.

John Gillies, partner and head of the office forces of Hulburd, Warren & Chandler, and well known on the Board of Trade, died Oct. 5 after an illness of ten days. He was 60 years old and had been with the company for more than thirty years. His widow and son survive him.

James A. Noble, chief of the grain sampling and seed inspection department of the Board of Trade, has had added to his duties that of giving the final decision on what is new or old corn, relieving the com'te of that work. Last fall some shippers who sold old corn, mixed in new with greater moisture content, giving rise to many disputes. This is not a question that can come before the state inspectors or the federal supervisors; but is a trade term. The to-arrive grain com'te announced "In case of dispute on contracts wherein the terms demand the delivery of old corn the decision of James A. Noble, chief grain sampler, shall be final."

INDIANA

Morocco, Ind.—M. Duffey is out of business.—Citizens State Bank.

Lapaz, Ind.—W. D. Wilson is not mgr. of this company.—Lapaz Grain Co., Chester Hill, mgr.

Union City, Ind.—The Union City Elvtr. Co. has authorized the issuance of \$10,700 preferred stock.

Pence, Ind.—Fred Crane has purchased the elvtr., operated as the J. O. Crane Co., from his brother, J. O. Crane.

Chase, Ind.—E. A. Benedict has resigned his position as mgr. of the Farmers Co-op. Co., and has located at Oxford.

Bentonville, Ind.—We are planning to do some remodeling on our feed room here.—Milton Grain Co., R. O. Clark, mgr., Milton.

Sullivan, Ind.—Ned Crowder, prop. of the Crowder elvtr., died, as reported. The elvtr. is for sale.—Miesenhelder Bros., Palestine, Ill.

Mt. Vernon, Ind.—A. W. Mackey, our mgr., has been appointed postmaster, as reported. We have not appointed a new mgr.—Farmers Elvtr. Company.

Oxford, Ind.—Please send my Journal to this place from now on as I am taking the management of the Oxford Grain Co.—E. A. Benedict, formerly of Chase.

Indianapolis, Ind.—The Farmers Grain & Supply Co. of Modoc and the Mill Creek Elvtr. Co. of Mill Creek, have been elected to membership in the Indiana Grain Dealers Ass'n.

Martinsburg, Ind.—I took charge as mgr. of the Farmers Ass'n, Oct. 2. We handle lumber, coal, grain and building materials. The former mgr. was George Gebeaut.—M. O. Stone.

Nortonburg, (Hope, p. o.), Ind.—The elvtr. here formerly owned by the late H. Griffith has been sold to Martin A. Holder who took possession Oct. 2.—Stafford Grain Co., Hope, Ind.

Frankton, Ind.—Harry J. Nading has taken charge at Lapel as reported, and I am acting as temporary mgr. until a permanent man can be obtained.—W. A. Reynolds, Farmers Grain & Coal Co.

Martins Station, Ind.—Iglehart Bros. have awarded contract to the Polk, Genung, Polk Co. to rebuild their elvtr. at this point which was struck by lightning in August and destroyed by the resultant fire.

Claypool, Ind.—We notice in several trade papers that it has been reported that we have bot the elvtr. here. This is an error and we do not know how the rumor started.—Mayer Grain Co., South Whitley.

Stewart (Tab p. o.), Ind.—The Miller Elvtr. Co., whose plant was formerly operated by the Seeger, Betts & Ward Grain Co. and purchased by Earl Goodwine and Jesse Weaver, has been incorporated for \$20,000 by Alonzo J. Miller, Joseph O. Crane and Mr. Weaver. Glen Crane is in charge of the elvtr.

Converse, Ind.—The elvtr. and feed mill of Knox & Kepner has been purchased by Hirsch Bros., grain dealers and bankers of Grass Creek and Kewanna, who will move to this place and take possession Oct. 25. Knox & Kepner are contemplating the purchase of another site of which Mr. Kepner will have charge. Mr. Knox will retire from active business. Frank Myers will be in charge of the Hirsch elvtr. at Grass Creek.

Indianapolis, Ind.—Regarding the fire on Sept. 19, the loss on building was \$21,000, grain, \$5,000; machinery, \$10,000; supplies and office equipment, \$4,000. Total insurance, \$30,000. The fire started around the machinery from combustion or dormant spark from motors. Plans for rebuilding have not been completed, we having made arrangements to care for our business from our other elvtrs.—Farmers Terminal Grain & Feed Co.

IOWA

Rockwell, Ia.—James Murphy has sold his elvtr. to Henry Hoensee.

Goodell, Ia.—Mgr. Meyers of Worthington, Minn., is now in charge of the Farmers Elvtr. Co.

New Sharon, Ia.—Arthur Briggs has succeeded Jeff McMain as mgr. of the farmers' elvtr. here.

Columbus Junction, Ia.—A 24-inch motor driven attrition mill has been installed in the elvtr. of Weber & Houston.

Webster City, Ia.—C. B. Martin has given up his position as mgr. of the branch office of the Donahue-Stratton Co. here.

Boyden, Ia.—The Farmers Co-op. Ass'n is going to put in a 12,000-lb. scale, three lifts and dumps.—John C. Pryde, agt. Quaker Oats Co.

Gladbrook, Ia.—The Gladbrook Grain & Lumber Co. has completed the erection of a warehouse to be used for feed storage.

Ringsted, Ia.—The Ringsted Farmers Elvtr. Co. is planning the erection of a new building. Work will be started immediately.

Callender, Ia.—The 35,000-bu. elvtr. of the Callender Grain Co. has been completed. The Younglove Construction Co. had the contract.

Cedar Rapids, Ia.—The St. Louis office of the National Oats Co. will be removed to this place, where the main plant of the company is located.

Rock Valley, Ia.—We have put in a new leg and will do some repairing. Jake Vander Stouwe has become mgr., as reported.—Farmers Elvtr. Co.

Granville, Ia.—The Granville Co-op. Grain Co., incorporated for \$18,500, has requested the appointment of a receiver. Liabilities are listed at \$36,808.

Eagle Grove, Ia.—Michael Gary of Elmore, Minn., will be in charge of the plant of the Independent Grain & Lumber Co., recently purchased by Gary Bros. of Ledyard, Ia.

Riverton, Ia.—We are confirming the report of the purchase of the Sherman-Chambers elvtr. here, where we are represented by Roy Fleming.—Good Bros. Seed & Grain Co., Hamburg.

Modale, Ia.—W. Maher is now agt. for Nye, Schneider & Fowler Co., and I am at Rushville, Neb., and also superintend ten other stations.—S. C. Beebe.—Mr. Maher succeeds Mr. Beebe at Modale.

Thornton, Ia.—Jacob Schleisman of the Moore Grain Co., whose headquarters are located at Belmont, has assumed charge of the elvtr. of the company at this place, succeeding Fred Christensen.

Spencer, Ia.—C. B. Martin is now at this place for the Cargill Grain Co.—Donahue-Stratton Co., Milwaukee.—Mr. Martin was formerly in charge of the Donahue-Stratton office at Webster City.

Storm Lake, Ia.—J. E. Bennett & Co. of Chicago have opened a branch office here as reported, with Joe Hunt, formerly of Iowa City, in charge. S. Stoensen, road man, is helping organize territory.—I. B. Hunt.

Denison, Ia.—We have discarded our steam boiler and installed a 100-h.p. F-M Type Y Oil Engine. We are also installing a Monarch Feed Grinder which we will have in operation within the next few weeks.—Doud Mfg. Co., H. L. Fitch.

Guernsey, Ia.—The Ray Murrel Grain Co. has purchased the elvtr. here as reported formerly owned by Gilchrist & Co., and operated by me. I am at present not engaged in anything, but am going back in the grain business in the near future.—R. E. Wenger.

Davenport, Ia.—Fred M. McCord of this place has purchased the elvtrs., offices, scales, cribs and loading platforms located at Mount Joy, Long Grove, Noel Station, Martin and Pleasant Valley, formerly owned by the Merchants Elvtr. Co. of Minneapolis, Minn.

Sioux City, Ia.—The property of the Farmers Terminal Elvtr. Co. has been transferred to Trustees Geo. H. Bliven and Orville Reed, by Receiver C. P. Downing of Ute, Ia., by order of court Sept. 21. Stockholders are hopeful the work of construction will be resumed.

Villisca, Ia.—Frank Pierson, owner of the Whitmeyer Elvtr. which was burned May 13, has awarded the contract for rebuilding to the Younglove Construction Co. The new plant will be covered with corrugated iron siding and will be equipped with a manlift, 10-h. p. motor, 1,250-bu. automatic scale and a Kewanee Spout.

Randolph, Ia.—The Farmers Elvtr. Co. has awarded contract for a 15,000-bu. reinforced concrete corn handling plant which will be equipped with two 15 h. p. electric motors, one No. 7 Barnard Moline Corn Sheller, Barnard Improved Double Screen Dustless Corn Cleaner, a 1,500-bu. Richardson Automatic Scale, dump, manlift, steel legs and head. The Younglove Construction Co. has the contract.

Pierson, Ia.—Marr & Young, formerly of Traer, are having the elvtr. which they recently purchased from the Wm. Grettenberg Grain Co. extensively repaired and improved. Contract for the work has been awarded to the Younglove Construction Co., who will raise the cupola about eight feet, install a Richardson Automatic Scale. New cement approaches will be laid at both driveways, which are being replanked. The office will also be remodeled.

Algona, Ia.—We do not run an elvtr. We are wholesalers and retailers of flour and feed.—Algona Flour & Feed Co., W. A. Dutton and D. L. Leffert.—This is a recently formed partnership and it was reported that Messrs. Dutton and Leffert would do a wholesale and retail grain business.

Oxford, Ia.—We have succeeded Mr. Jones, who has been operating as the Oxford Elvtr. Co. Mr. Jones found that he was unable to look after the elvtr. in connection with his other extensive interests for which reason he has sold the elvtr.—Gifford Grain Co., by L. W. Gifford, Cedar Rapids.

Toeterville, Ia.—I am installing a feed grinding outfit for custom grinding. I am planning to install a corn sheller and a 20-inch direct drive Monarch Attrition Mill. If the business warrants, I will add a crusher and corn cracker. The Hickok Construction Co. is putting in the milling outfit.—H. D. Mundt.

Ackley, Ia.—Henry W. Boyenga, formerly mgr. of the Farmers Co-op. Elvtr. Co. and one Mr. Oelman, a farmer, are the defendants in a suit brot against them by the elvtr. company, reported to have accused Messrs. Boyenga and Oelman of operating on the Chicago Board of Trade with funds owned by the company.

Iowa Falls, Ia.—The plant of the Farmers Co-op. Elvtr. Co. is being extensively improved and repaired by the Younglove Construction Co. The elvtr. will be overhauled and equipped with a Barnard & Leas Warehouse Sheller, a Barnard Improved Double Screen Dustless Corn Cleaner having an hourly capacity of 400 to 600 bus., corn feeder, manlift, distributors and corn crusher. Work will be done by the Younglove Construction Co.

KANSAS

Hutchinson, Kan.—The Stiles Grain Co. has discontinued business.—Cal.

Stockton, Kan.—The elvtr. of the Morrison Grain Co. was slightly burned a short time ago.

Wichita, Kan.—The Stevens-Scott Grain Co. is planning the erection of a brick building to cost about \$50,000.

Stockton, Kan.—E. F. Jones of the E. F. Jones Grain Co. is now located at Beloit.—Citizens State Bank.

Gardner, Kan.—The business of the Ward Grain Co., whose elvtr. was burned some months ago, has been sold.

Wilmore, Kan.—Mgr. Allison is now in charge of the People's Grain Co., succeeding Fred Vance, who has located at Ensign.

Marietta, Kan.—The elvtr. of the Marietta Stock & Grain Co. was slightly damaged by fire starting in the cob house Sept. 22.

Junction City, Kan.—The plant of the Brockwell Merchandise & Grain Co. was slightly burned after being struck by lightning.

Wichita, Kan.—John Harold of the Harold Grain Co. was married to Miss Genevieve Halloway, Sept. 23. The couple spent their honeymoon in Colorado.

Tonganoxie, Kan.—M. B. Rich, who had planned to reorganize a company to operate, has failed in this undertaking and is taking down his corn mill.

Dorrance, Kan.—The elvtr. of the Weber Flour Mill Corporation was burned Sept. 28. The fire is believed to have started from sparks from a passing locomotive.

Atchison, Kan.—The plant being erected for the Blair Mfg. Co. to replace the one that was burned a few months ago, is expected to be in readiness by January, 1923.

Larned, Kan.—The mill and elvtr. here recently leased by the Bowen-Oglesby Mfg. Co., is known as the Arkansas Valley Mill. It was formerly operated by the Bowen-Romer Mfg. Co.

Florence, Kan.—W. T. Hacker leased the elvtr. of the Florence Grain & Coal Co., and operates as the Hacker elvtr. We installed a motor in place of the gas engine.—H. B. Smith, mgr.

Hutchinson, Kan.—A. G. McReynolds, of the Farmers Commission Co., and who was in charge of the company's Kansas City office, has returned to Hutchinson. The Kansas City office has been closed, temporarily.—Cal.

Atchison, Kan.—The Pillsbury Flour Mills have completed the erection of thirty-two 103-foot storage bins. An 80-foot headhouse is to be erected also. This is the plant formerly owned by the Atchison Flour Mills Corp.

Concordia, Kan.—Bossemeyer Bros. are covering their elvtr. with galvanized iron and giving same a general overhauling. They are also installing a truck dump. The fire did no damage to speak of.—E. F. Carpenter, foreman of construction.

Winona, Kan.—The elvtr. recently completed for the Colorado Mlg. & Elvtr. Co., will be operated as the Wheatland Elvtr. Co., or the Hays City Mlg. Co. Manager will be B. F. Snider, formerly with Weber & Co. of Wilson.—Wheatland Elvtr. Co.

Bonner Springs, Kan.—The Tiblow Mills, now owned by the Ismert-Hincke Mlg. Co., has been repaired and improved, including the remodeling of the old building which will be used for the manufacture of flour and breakfast foods, and a new fireproof building.

Salina, Kan.—We are successors to the Hutchings-Fose Grain Co. and are doing a general wholesale grain and coal business handling carlots only. We will also have a brokerage account on produce. Business is good.—Terminal Grain Co., J. T. Hutchings.

Wichita, Kan.—James Dixon has left the Clark Burdgr Grain Co. and has opened an office for himself under the firm name of Dixon Grain Co.—R. B. Waltermire, sec'y. Board of Trade.—F. J. Kramer will close the office of the Kramer Grain Co. and will hereafter be in charge of the office of the Clark Burdgr Grain Co.

Burlington, Kan.—H. L. McIntyre has resigned as pres. and mgr. of this company. H. C. Hatch was elected pres., to succeed him and W. H. Bunge, who has been with the company for the past six years as sec'y and ass't mgr., is now mgr.—Excelsior Mlg. & Power Co.—It was reported that Mr. McIntyre would continue as pres.

Minneapolis, Kan.—G. A. Taylor, who recently purchased a half interest in our elvtrs. was previously employed by the Jackman Roller Mills of this city as bookkeeper for the past fourteen years. Our firm will be known as the Wolfersberger Elvtr. Co., as reported, with elvtrs. here, Lindsey and Brewer.—Wolfersberger Elvtr. Co.

Atwood, Kan.—The Lynn elvtr. is being operated by Jennings and Roller and Frank Janke, formerly of Stratton, Colo., is the mgr.—The Charles Miner elvtr. has been leased by the Morrison Grain Co. of Kansas City, Mo., and J. F. Blackman of Hill City is the present mgr.—E. N. Conquest is operating the William Cornell elvtr.—We recently installed a new 10-h. p. F-M Electric Motor.—Atwood Equity Exchange, J. A. Bowles, mgr.

Abilene, Kan.—R. D. Arndt of the Mid-West Mlg. Co., died last month. He had been suffering with cancer for some years and at the time of his death was confined to his home. Mr. Arndt was in the milling and grain business in this part of the country about twelve years. He was associated with the Southwestern Mlg. Co. of Kansas City, Mo., and was later interested in the Abilene Flour Mills Co. He then organized the Mid-West Mlg. Co. His widow, two sons and a daughter survive him.

Topeka, Kan.—E. J. Smiley, sec'y of the Kansas Grain Dealers Ass'n, contemplates having a bill introduced in the state legislature authorizing the public utilities commission to place a value and rental on right of way of railroads used for elvtr. sites. Members of the Ass'n are urged by him to call the attention of candidates for the legislature to the Iowa law on this subject when calling upon the grain dealers for support with a view to getting their promise to support the bill when introduced.

Topeka, Kan.—Sec'y E. J. Smiley in his recent bulletin 100 to members says: As the present law provides that the register of deeds shall keep a book in which shall be entered a minute of all mortgages of personal property, under the heading, "Time of reception," "name of mortgagor," "name of mortgagee," "date of instrument," "amount secured," "when due" and "property mortgaged" it would only be necessary for the register of deeds to refer to his record of "grain mortgage" in order to furnish the information requested by the grain dealer. We believe, however, that the better and safer plan would be that when purchasing grain of tenants, not well known to you, that you make the check for the grain purchased, payable both to the landlord and tenant, and before the bank upon which the check is drawn will honor same, it must have the endorsement of the landlord and his endorsement upon the check would relieve you of any liability.

Emporia, Kan.—H. C. Pargett, formerly at Cawker City, with the Mitchell County Farmers Union, has taken a position with the Lord Grain Co. as mgr. of the Emporia elvtr. This is a new elvtr., 25,000 bus. capacity equipped with electric power, truck dump and a 10-ton scale.—T. B. Lord.

KENTUCKY

Louisville, Ky.—The report in the Sept. 25 issue of the Journal that the entire ownership of the Bingham-Hewett Grain Co. has been acquired by Leonard A. Hewett is incorrect. There has been no change whatsoever in the name or in the stockholders or officers of this company for the past two years.—Leonard A. Hewett, Bingham-Hewett Grain Co., Louisville.

LOUISIANA

New Orleans, La.—We have removed to the Carondelet Bldg.—Anderson & Jackson, Inc., grain exporters.

New Orleans, La.—The Public Elevator is adding two shipping bins of 10,000 bus. each and four Richardson Auto Bagging Scales will be installed to care for the needs of the local distributing trade. The Port Commission plans adding storage tanks of a million bushels capacity.

New Orleans, La.—The partnership of Fenner & Beane composed of Charles E. Fenner, A. C. Beane, Frank Brown, Edmund J. Glenny and Ernest B. Norman was liquidated on Sept. 30 and the firm is now composed of the following: Charles E. Fenner, A. C. Beane, Edmund J. Glenny of this city, Frank Brown and W. M. Friedlander of New York. Special partners are J. H. Himes of Canton, O., and J. N. Carpenter Jr. of New York. The firm is now a member of the New York Stock Exchange.

MARYLAND

Hurlock, Md.—We will not have another elvtr. ready to replace the one burned June 24, before another wheat harvest.—Hurlock Mlg. Co.

Washington, D. C.—We have let contract for building our new mill to the Spencer Construction Co., but up to this time, have not contracted for the machinery for our flour and corn mills and mixing plant. The Spencer Co. will build our concrete warehouse and storage elvtrs.—Wilkins-Rogers Mlg. Co., S. H. Rogers, vice-pres.

BALTIMORE LETTER.

It has been reported that S. A. Pederson & Co. are out of business.

John Brown, who has been with Otto Stude & Co. for some years, has severed that connection and is now with H. C. Jones & Co., Inc.

Ferdinand Meyer, Jr., who is the son of Ferdinand A. Meyer, pres. of the Baltimore Grain Co., will be married to Miss Dorothy Heywood of New York City, Oct. 14. He is also associated with the Baltimore Grain Co., so the couple will make their home here after the wedding journey.

MICHIGAN

Mulliken, Mich.—McNaughton & Peabody's elvtr. was slightly burned Sept. 26.

Detroit, Mich.—The office of H. C. Carson & Co. is now located in the Equity Bldg.

Hudson, Mich.—The Hudson Mlg. Co. recently purchased the seed and grain business of James L. Moloney & Co.

Omer, Mich.—B. S. Holland and William Topp have taken over the plant and business of the East Michigan Farmers Elvtr. Co.

Watertown, Mich.—The Farm Bureau purchased the property of R. W. Johnson, whose elvtr. was burned last year and will fit up a hay shed in which to conduct business. An elvtr. will be erected in the spring which will be operated under the management of Edward Schultz.

Lachine, Mich.—The Thunder Bay Mlg. Co. has taken over the Lachine Elvtr. Co., a branch of the Martindale Grain & Bean Co., which was closed in May. The plant will be overhauled. A warehouse at Ossineke was also taken over by the company and both establishments will be operated under the management of B. B. Norton.

New Baltimore, Mich.—The Marine City Farmers Co-op. Elvtr. Co. of Marine City, has purchased the plant of the New Baltimore Elvtr. Co., which was closed some months ago. H. A. Shaw is temporarily in charge.

MINNESOTA

Owatonna, Minn.—I have gone out of business.—F. W. Wicklow.

Richmond, Minn.—E. L. Wurst, who was in charge of the plant of the Farmers Mlg. & Elvtr. Co., has given up his position and has located at St. Cloud.

Donnelly, Minn.—The old elvtr. of the Farmers Elvtr. Co. is being taken down and a new one will be erected in its stead, which will have a capacity of 30,000 bus.

Badger, Minn.—The elvtr. of the Red Lake Falls Mlg. Co., which was burned, has been rebuilt on a new site and is receiving grain again. L. O. Mounteth, agt., Greenbush.

Hampton, Minn.—The elvtr. of the Hampton Farmers Elvtr. Co., recently incorporated for \$50,000, will be operated under the management of H. E. Swanson, formerly located at Montevideo.

Ellsworth, Minn.—I am now mgr. of the Dav- enport Elvtr. Co., moving here from Badger, Ia., where I was mgr. for the Quaker Oats Co. Canute Cleveland is now mgr. there.—Earl Bowen.

Duluth, Minn.—New members of the Board of Trade are: E. J. Pierce, A. T. Master and P. K. Corcoran. Those whose memberships have been transferred are: R. J. Hamp, Percy Fuller and R. J. McNamara.—Chas. F. Macdonald, sec'y.

Savage, Minn.—The farmers own the two elvtrs. here, but the Shakopee Mill has leased one and is operating as the Savage Elvtr. Co. as reported. The other is not in use. We have installed a 5-h. p. motor and built a feed and flour shed, 18x24 feet.—M. E. Stevens, mgr.

Duluth, Minn.—The Board of Trade has elected P. K. Corcoran of the American Linseed Co. to membership. Application for the transfer of the memberships of Garfield Meyer of the Duluth-Superior Mlg. Co. to A. P. Walker, pres. of the Standard Mlg. Co. of New York, and that of D. T. Helm to R. B. Loring of Duluth, have been posted.

Walnut Grove, Minn.—Our fire started from a supposed short in electric wires. Loss, \$6,500, partly insured. About 10,000 bus. grain was burned, fully insured. A 25,000-bu. elvtr. is being erected by the Hickok Construction Co. which will be modern in every respect and cost about \$9,500. It will be equipped with an auto scale and truck dump.—Walnut Grain & Supply Co., formerly operated as Farmers Elvtr. Co., C. H. Ahlfs, mgr.

MINNEAPOLIS LETTER.

S. A. Dalton, who retired several years ago, has become connected with the McCarthy Bros. Co. as traveling representative in Southern Minnesota, South Dakota and Northern Iowa.

Louis Hanson, treas. of the Montana & Dakota Grain Co., a member of the firm, Hanson & Barzen Mlg. Co. of Thief River Falls, and also a member of the state grain board of appeals, died a short time ago at the age of 69 years. He was formerly pres. of the Ada Flour & Mlg. Co. of Ada.

The United States Grain Growers Sales Co. has been incorporated for \$25,000. Mr. Reed, Thomas E. Cashman of Owatonna and E. H. Cunningham of Cresco, Ia., sec'y of the Iowa Farm Bureau Federation. Offices will be maintained in this city which will be under the management of T. H. Hagen of Lakeville, Minn.

Thomas R. Barrett, who has been with the Wm. Dalrymple Commission Co. of this city and Duluth for some years, has severed that connection and has become associated with Becher, Barrett & Lockerby, also of Minneapolis and Duluth. Mr. Barrett will continue in the same capacity as grain solicitor, covering North Dakota.

The Chamber of Commerce held its annual election Oct. 5 which resulted as follows: F. C. Van Dusen, of the Van Dusen Harrington Co., pres.; T. W. Hall, second vice-pres.; directors: C. T. Stevenson, George P. Case, W. J. Russell, H. J. Moreton and J. F. Fraser; Board of Arbitration: C. E. Lockerby and J. D. Staddon; Board of Appeals: H. G. Dickey and J. H. Rihelddaffer.

Theodore H. Welch, formerly with E. L. Welch & Co., for whom he was appointed receiver, has become connected with the Kellogg Commission Co.

MISSOURI

St. Joseph, Mo.—The Carter-Williams Grain Co. has been adjudged a bankrupt.

Aullville, Mo.—We expect to have our elvtr. and warehouse completed by next spring.—J. S. Klingenberg & Son, Concordia.

Morehouse, Mo.—The Scott County Mlg. Co. of Sikeston has leased its warehouse at this point to the Morehouse Grain & Supply Co.

Buffalo, Mo.—James A. Bonner has awarded contract for his 10,000-bu. plant. Work has already started and is expected to be completed by Nov. 15.

Matthews, Mo.—The Missouri Sunflower Growers Ass'n will operate under lease the warehouse here of the Scott County Mlg. Co. of Sikeston.

Pacific, Mo.—The Eureka mill was taken over by the Farmers Co-op. Ass'n a short time ago. George Radeacker will be in charge of the mill and elvtr.

Cameron, Mo.—The W. H. Seaton Grain Co. has discontinued business operations at this place and Henry Seaton is planning to build an elvtr. at Lathrop.

Miner (Sikeston p. o.), Mo.—The Marshall Land & Mercantile Co. of Blodgett has leased the warehouse here and at Morley of the Scott County Mlg. Co. of Sikeston.

St. Louis, Mo.—The DeArmond Grain Co. and F. M. McClelland & Co. have been consolidated and the business will be known as the McClelland-DeArmond Grain Co.

Browns Station, Mo.—The Dover-Baker Grain Co. has leased the elvtrs. and warehouses here and at Champion. (Matthews p. o.) of the Scott County Mlg. Co. of Sikeston.

Hardin, Mo.—I am practically out of the grain business.—W. H. Trenchard.—Mr. Trenchard who operated an elvtr. at this point, had formed a partnership with J. K. Quick in June.

St. Louis, Mo.—W. L. Burton has discontinued connections with the J. H. Teasdale Commission Co., and will hereafter be in charge of the merchandising department of the Picker & Beardsley Co.

Mound City, Mo.—O. W. Long has bot the Mound City Mill & Elvtr. Co. and I am managing the business for him, as reported. We contemplate making some improvements. We have steam power and expect to change to electric power.—H. E. Combs.

The grain grading campaign inaugurated by the State Marketing Bureau of the Missouri Board of Agriculture is scheduled for lectures at the following places: Smithton and Sedalia, Oct. 12; Green Ridge and Ionia, Oct. 13; Lamonte and Hughesville, Oct. 14.

St. Louis, Mo.—The J. C. Shaffer Grain Co. has become affiliated with the Elmore-Schultz Co. We will increase the handling capacity of the elvtr. by putting in new machinery. We are negotiating for larger electric motors and larger grain cleaners.—J. J. Stream, Chicago.

Sikeston, Mo.—The Scott County Mlg. Co. which owns a large number of elvtrs. thruout Southeastern Missouri, is planning to lease all except the plants located at this point, Oran and Dexter. Those at Miner (Sikeston p. o.), Morley, Morehouse, Matthews, Browns Station and Champion (Matthews p. o.) have already been leased.

Montgomery, Mo.—I have rented my elvtr. for the coming year to the Commander Elvtr. Co. and it has engaged Matt Stanek as grain buyer. This arrangement is only temporary and I may be back in the business again next year.—John Sheehy.—Mr. Sheehy has been in business here for about 35 years. Other reports have stated that L. N. Neusman would open the elvtr.

Springfield, Mo.—We only bot merchandise in the building owned by the Interstate Mlg. Co. We rented the building from D. W. Newton of the Newton Grain Co. We have organized a company and started a new business to be known as E. B. Evans Feed & Seed Co. There is no elvtr. in connection with this business. We will not put in any machinery at this time. Will operate a feed and seed business.—E. B. Evans.

KANSAS CITY LETTER

Contract for the machinery for the new plant of the Kansas Flour Mills Co. being erected in North Kansas City has been awarded.

The Farmers Commission Co. has closed its local office and A. G. McReynolds, who was in charge as mgr., has returned to Hutchinson, Kan. The company retains its membership in the Board of Trade and it is the purpose to open the office again later.—Cal.

George K. Walton has sold his membership in the Board of Trade and retired from the grain business after sixteen years' activity. He was with Goffe & Carkener for fourteen years, then became associated with the Nelson Grain Co. for almost two years. He had reassumed connections with Goffe & Carkener at the time of his resignation.

Members of the Board of Trade will vote Oct. 16 on an amendment to the rules which will invest the directors with power to negotiate the lease or purchase of property for use of the exchange. This amendment is intended to simplify and facilitate any plans the exchange may adopt for securing new quarters for the Board or continuing in the present location. Members, however, must ratify any plans formally decided upon by the directors for leasing or purchase before it becomes effective.

MONTANA

Helena, Mont.—The T. C. Power Co. has completed its elvtr. here and has installed a feed mill.

Winifred, Mont.—The building used by the Montana-Dakota Elvtr. Co. for grain storage, collapsed a short time ago.

Wilsall, Mont.—Wilsall Grain Co. has been incorporated for \$15,000 and will deal in grain, farm products and general merchandise. Incorporators: C. K. and K. K. Liquin of this place and G. F. Sunwall of Minneapolis.

Big Sandy, Mont.—The Greely Elvtr. Co. of Great Falls is operating the elvtr. of the Farmers Produce Co. here and I am in charge as mgr. I am back at the old stand after being out of the grain business for five years and it feels like getting home after a long vacation.—W. F. Schurmann, agt. Greely Elvtr. Co.

NEBRASKA

Prosser, Neb.—The Verona Grain & Lumber Co. has sold out.

Omaha, Neb.—The Omaha Grain Exchange is planning to hold Thursday night lectures.

Princeton, Neb.—The plant of the Princeton Farmers Elvtr. Co. was slightly burned a short time ago.

Valentine, Neb.—The Farmers Union Co-op. Ass'n is planning to build a 6,000-bu. elvtr. Contract has been awarded.

Grand Island, Neb.—Please change our address from Hastings to this place as we are moving here.—McCaull-Dinsmore Co., by R. B. Hoagland.

New March, (Hastings p. o.), Neb.—Michael Smith, formerly in charge of the elvtr. of the Farmers Union at Hayland, will hereafter be mgr. of the Farmers Elvtr. & Lumber Co. here.

Garrison, Neb.—W. C. Flickinger, recently reported as having purchased an elvtr. here, has become mgr. of the Farmers Grain Co. He was formerly with the Imperial Mills of David City.

Rushville, Neb.—W. Maher is now agt. for Nye, Schneider, Fowler Co. at Modale, Ia., and I am here. I also superintend ten other stations.—S. C. Beebe.—Mr. Maher succeeded Mr. Beebe at Modale.

Mullen, Neb.—We laid a new floor in our driveway, put an extra window in our office, made some other minor repairs and painted the elevator and office buildings.—Farmers Union Co-op. Ass'n, W. B. Perkins, mgr.

Sargent, Neb.—E. G. Taylor has bot the D. T. Adams elvtr.—J. E. Werber, mgr. W. T. Barstow Grain Co.—The Currie elvtr. was bot at a sheriff's sale by Fred Seigen of Sutherland who has since sold it to Fred Dauphin, as reported recently.

Nora, Neb.—Our new 30,000-bu. iron covered cribbed house was completed Aug. 1, as reported. The plant is equipped with a cleaner, Birchard Distributor, F-M 15-h. p. Engine. We expect to install a 10-h. p. electric motor soon.—Farmers Union Ass'n.

Pierce, Neb.—A. O. Schramm has succeeded Charles Weller of Wallace, Kan., as owner of the George E. Drebert elvtr. and coal sheds at this place, trading 160 acres of land for the plant. Mr. Schramm has not decided whether he will lease or operate.

Rockford, Neb.—H. R. Rossell, formerly mgr. of the Farmers Union Co-op. Ass'n (succeeded by W. W. Topf), resigned to go in business for himself, having bot a general store here.—A. T. Burroughs.—Mr. Burroughs has leased the elvtr. of the Farmers Union, as reported.

Brunswick, Neb.—The 20,000-bu. elvtr. of the W. W. Calkins Grain Co. has been completed by the Younglove Construction Co. This replaces the plant that was burned a short time ago. The plant is 26x30x63 feet, covered with galvanized iron and has been equipped with a 100-bu. hopper scale, 10-ton dump scale, 10-h. p. engine, all manufactured by Fairbanks-Morse & Co., also a Union Iron Works plain rail dump with automatic controller and a manlift.

NEW ENGLAND

Concord, N. H.—The Merrimack County Farmers Exchange has awarded contract to the Fred Sutton Co. for the erection of an elvtr.

NEW YORK

East Aurora, N. Y.—E. E. Godfrey is having his mill and elvtr. electrically equipped.

New York, N. Y.—E. V. Phillips has been appointed mgr. of the New York office of the Hall-Baker Grain Co.

New York, N. Y.—Harry B. Signor of E. F. Leland & Co. of Chicago has posted his membership in the Stock Exchange for transfer.

Buffalo, N. Y.—I have succeeded F. A. McLellan as mgr. of the Co-op. Grange League Federation Exchange, Inc.—H. J. Hannon.—Mr. McLellan, as reported, has become associated with the George Urban Mlg. Co.

New York, N. Y.—The Brinkly-Evans-Leverich Co. is a new firm starting business Oct. 2, and will do a grain brokerage and forwarding trade. Brinkly Evans has been in the grain business for about twenty years and Mr. Leverich was formerly connected with the Hall-Baker Grain Co.

Buffalo, N. Y.—William Seligmann, vice-pres. of the George Urban Mlg. Co., died last month, following a heart attack. Mr. Seligmann had been with the company for 34 years and was a member of the New York Produce Exchange and the Buffalo Corn Exchange. He is survived by his widow and two sons.

Buffalo, N. Y.—The Pillsbury Flour Mills Co. of Minneapolis is planning the erection of a mill and elvtr. on property purchased here about a year ago. Plans are being prepared and call for a reinforced concrete building, 270x100 feet, ten stories high. The mill will have a daily capacity of 7,000 bbls. and the elvtr. 3,000,000 bu.

New York, N. Y.—James Stewart, pres. of the Maple Leaf Mlg. Co. of Winnipeg, Man., is planning to open an office in this city to be operated in connection with the newly formed grain business known as the James Stewart Grain Corporation. George Gingham, who has been associated with Webb & Kenwood of London, will have charge of the office. Mr. Gingham has been succeeded by P. S. Gallo-way.

New York, N. Y.—Charles W. Austin, who was recently appointed superintendent of the newly completed state canal barge elvtr. at Gowanus Bay, Brooklyn, is now superintendent of the terminal elvtrs. in the Metropolitan district and is also in charge of the eight barge canal terminals in New York. He will be assisted by John S. Gaynor, formerly superintendent of terminals, which position has been abolished.

Buffalo, N. Y.—William Marshall, aged 46, sales mgr. of the Churchill Grain & Seed Co., dropped dead in his office Sept. 25. He had been suffering from acute indigestion which resulted in heart failure. His sudden death was a decided shock to his many friends in the industry; he had been a visitor to the Chamber of Commerce Bldg. about a half hour previous. Mr. Marshall had been with the company here for about four years, having been located at Pittsburgh. George W. Bartlett, pres. of the company, will look after the sales department, which has been conducted under the management of Mr. Marshall for several years. He was formerly with the Quaker Oats Co. His mother and widow survive him.

NORTH DAKOTA

Glencross, N. D.—George McCabe's 25 000-bu. elvtr. burst last month.

Tyler, N. D.—The H. W. Wormer Grain Co. will install a new leg and feed grinder.—H. W. Wormer.

Webster, N. D.—We bot the Winter-Truesdell-Ames Co. out July, 1919.—Webster Co-op. Elvtr. Co.—This company's plant was slightly damaged by a stroke of lightning last month.

Mayville, N. D.—The Farmers Grain Co. has taken over the flour mill of the Farmers Grain & Mfg. Co.

Hatton, N. D.—We have purchased the Great Western elvtr. to handle potatoes in car lots.—Dean & Osling Co.

Blanchard, N. D.—James Lowmon's farm elvtr., containing about 35,000 bus. of grain, was burned last month.

Fargo, N. D.—John F. McGrann of this point has succeeded A. J. Gronna, deceased, as chairman of the mill and elvtr. commission.

Garrison, N. D.—We have installed scales and a Kewanee Truck Dump as reported, and have also made other improvements.—Occident Elvtr. Co.

Hampden, N. D.—We are now operating the elvtr. formerly operated by Winter-Truesdell-Ames Elvtr. Co.—Rosholt Elvtr. Co., H. H. Reimer.

Starkweather, N. D.—The elvtr. of the Starkweather Co-op. Elvtr. Co. containing 20,000 bus. of wheat was slightly burned last month. The loss is insured.

Linton, N. D.—J. P. Schott's elvtr. burned Aug. 31, as reported. It was formerly the Pacific Elvtr. Co.'s elvtr. Mr. Schott is now rebuilding.—Ed C. Kruger, mgr. Seeman & Kruger.

Griffin, N. D.—B. J. Murray, formerly agt. for the Davenport Elvtr. Co. at Ellsworth, Minn., is now located at this place as mgr. for the Farmers Elvtr. Co.—Earl Bowen, new mgr. Davenport Elvtr. Co., Ellsworth.

Aneta, N. D.—The elvtr. of the Quinn-Shepherdson Co. (recently purchased by the Aneta Grain Co.), was moved up from Sharon. The Cargill Elvtr. Co. now has two elvtrs. here, having bot Martin S. Lee's elvtr.—Oscar Otterson, agt., Cargill Elvtr. Co.

Amenia, N. D.—The Amenias Seed & Grain Co. which was recently incorporated for \$100,000, was formed to take over the grain, feed, seed and lumber business handled by the Amenias & Sharon Land Co. We are the company's commission house. Carrie T. Chaffee, pres., S. M. Higgins, vice-pres., E. W. Chaffee, sec'y-treas., Adele C. Higgins and Lillian B. Chaffee, who are also the directors, are the larger holders in the Amenias & Sharon Land Co. and this new organization was formed on account of the Land Co. distributing its farm land to its stockholders with a view to a change from grain farming to general or diversified lines. The new concern is starting off with the full plant and equipment clear of all incumbrance and paid-up capital of \$100,000.—The John Miller Co., P. E. Stroud, vice-pres. and treas., Minneapolis, Minn.

OHIO

Circleville, O.—Our successor is the Soyco Mills Co.—Circleville Mfg. Co.

Portsmouth, O.—T. G. Jewett & Sons' elvtr. and warehouse was burned Sept. 25. Loss, \$100,000, partly insured.

The Fall meeting of the Ohio Grain Dealers Ass'n to discuss the handling of new corn will be held at Lima the last of the month.

Toledo, O.—Lester W. Howard has become connected with C. A. King & Co., and will have charge of the cash grain end of the business.

Pleasant Bend, O.—J. W. Deckman is the new mgr. of the Clover Leaf Grain Co., formerly known as the Pleasant Bend Grain Co.

Westville, O.—The Farmers Exchange Co. was damaged by fire Sept. 22, starting in the mill, believed to have originated from a spark in the siding.

Hartland, O.—Mead & Woodward of Norwalk are again the owners of the elvtr. at this point. They sold it to a farmers company several years ago.

London, O.—A 12,000-bu. cribbed addition was erected to the elvtr. of Wood Bros., increasing the capital stock to 40,000 bus. A 15-ton Fairbanks Wagon Scale was also installed.

Celina, O.—W. T. Palmer of the Palmer-Miller Grain Co., dropped dead recently while working on his automobile. Mr. Palmer was forced to discontinue business activity last November because of illness.

Orrville, O.—Henry Monter has purchased an elvtr. which he has rented to George Fladung and William Justice. Mr. Monter plans to enter the coal business, but Messrs. Justice and Fladung will continue the grain business.

Dayton, O.—Plans for the erection of a \$50,000 elvtr. to be erected in North Dayton for the Farmers Co-op. Elvtr. Co. have been discontinued. A final decision on the question is yet to be made, but the majority of stockholders is in favor of giving up the project.

South Solon, O.—We bot the elvtr. of the South Solon Co-op. Grain & Live Stock Co. at public auction in June, as reported, and will continue the grain and elvtr. business under the name J. M. Diffendal Elvtr. Co. which is owned and controlled by individuals.—J. M. Diffendal Elvtr. Co.

CINCINNATI LETTER.

Cincinnati, O.—The Van Leunen Co. has gone into the hands of a receiver.

The Hay and Grain Exchange will be located in new quarters after Oct. 25.—W.

Cincinnati, O.—Albert Hess has resigned his position as mgr. of the hay department of the A. C. Gale Grain Co. and will hereafter be connected with the Early & Daniel Co., assisting Robert Early in that capacity.

B. J. Drummond, traffic mgr. of the Hay and Grain Exchange, has tendered his resignation to take effect Nov. 1. E. B. Walker, who assisted Mr. Drummond, will take charge of the rate quotations and sundry matters.—W.

OKLAHOMA

Jefferson, Okla.—We are covering our elvtr. with iron and building a fireproof engine room.—Enid Mfg. Co.

Afton, Okla.—The plant of the Afton Flour Mills containing 5,000 bus. of wheat, was burned a short time ago. Loss, \$10,000.

Fairview, Okla.—It is reported that the Home Builders Protective & Shippers Ass'n which operated an elvtr. here, has been dissolved.

Enid, Okla.—The McKnight Grain & Feed Co. has been incorporated for \$10,000 by J. B. McKnight, B. B. Brown and J. H. Degrange.

Edmond, Okla.—The Edmond Grain Co. is the defendant in a suit for \$890 brot against it by P. J. Lang of Oklahoma City, who is reported to have charged that a truck of the grain company damaged his motor car.

Bartlesville, Okla.—We are contemplating the erection of a small mill and elvtr., but plans have not yet matured. The idea underlying the project is to furnish a market for such wheat as is raised in this vicinity. If the plan proves feasible, it will mean an increased production of wheat in this section of the state. We will advise you of further developments.—Bartlesville Chamber of Commerce, E. L. George, sec'y.

OREGON

Haines, Ore.—The firm of Dodd & Ashum is now known as the Haines Feed Mill. John Hammond is mgr.—N. E. Dodd.

Portland, Ore.—The Sperry Flour Co. of San Francisco is contemplating purchasing the Portland Flouring Mills Co. of this city. Such a merger would increase the storage capacity of the Sperry Co. to 15,000 bus. and will give a mill capacity of 27,500 bbls. daily, making the company one of the largest milling companies in the United States. The Portland Flouring Mills Co. operates ten cereal and feed mills and 160 warehouses and elvtrs. thruout this state and Washington.

PENNSYLVANIA

Shamrock, Pa.—Claude D. Trexler, grain and coal dealer, is having a warehouse erected.

Philadelphia, Pa.—The Commercial Exchange has elected Geyelin & Co., Inc., to membership.

Freighters, Pa.—The Mauser Mill Co. is erecting an additional elvtr. to its main plant which will be used for storage purposes.

Sinking Springs, Pa.—The Sinking Springs Mfg. Co. has been organized and incorporated for \$20,000 to deal in grain, flour and feed. Incorporators: Edward J. Bolender, Wyomissing, Pa., A. M. Ruth and Eva Schounour of Sinking Springs, Pa.

SOUTH DAKOTA

Wetonka, S. D.—Lorber & Lynch have sold out to Geisler & McIntyre.—Joseph Lorber.

Kennebec, S. D.—The erection of an elvtr. at this point is being contemplated by the Farmers Union.

Madison, S. D.—The elvtr. of E. W. Ketcham & Son was slightly damaged by a stroke of lightning last month.

Manchester, S. D.—I am not in the grain business now.—G. B. Bjornson.—Mr. Bjornson's elvtr. was burned last year.

Langford, S. D.—R. E. Shoemaker is in charge of the elvtr. which the National Elvtr. Co. took over from Jacob Palmquist.

Hecla, S. D.—The J. E. Scott & Co. elvtr. at this station is closed. Their business is in the hands of a receiver.—Hecla Co-op. Elvtr. Co.

Tripp, S. D.—The 25,000-bu. elvtr. which was being erected for the Farmers Elvtr. Co. by the Younglove Construction Co. has been completed.

Spottswood, S. D.—John Kingdon of Hitchcock has leased the plant of the Farmers Elvtr. Co., which went into the hands of the receiver about a year ago.

Parker, S. D.—The firm name now is the Independent Grain Co.—G. H. Howdle, pres.—Mr. Howdle recently purchased the plant of the Shanard Elvtr. Co.

Miller, S. D.—The local flour mills of the Huron Mfg. Co., also operator of an elvtr., have been leased by J. W. Johnson and Ray Howard, who have already taken possession.

Scotland, S. D.—C. F. Kludt has succeeded E. B. Kast as mgr. of the Farmers Grain & Stock Co. Mr. Kast, whose elvtr. at Lester-ville was burned in August, is now located at Mankato, Minn.

Langford, S. D.—I installed all new machinery as reported, including a F-M 15-h. p. type "Z" Engine, a simplex cleaner and in addition, have repainted both my Langford and Spain, S. D., elvtrs.—Reuben Olson, Olson Grain Co.

Frankfort, S. D.—We have bot the Ed. Lindgren Elvtr. at a trustee sale and have incorporated for \$25,000, as reported, as the James Valley Elvtr. Co.—Hans Realren, mgr.—Ed Lindgren is now agt. for the Frankfort Mill & Elvtr. Co.

Wessington, S. D.—In 1921, the Equity Exchange went bankrupt. This spring a number of the old stockholders reorganized and bot the elvtr. back, but did not organize to operate it themselves. They have leased it to A. C. Ruddy for one year as reported.—H. A. Osterich, agt. Eagle Roller Mills Co.

De Smet, S. D.—The W. J. Thompson elvtr. has an old engine room made of stone and covered with a wooden roof. The wind raised the roof recently and at the same time a wagon loaded with grain stood waiting to unload. The roof hit the driver, doubling him up and broke his back. He was a boy of 18 years and has been taken to Rochester, Minn., where they operated on him. He is getting along as well as he could. Whether or not he will walk, the doctors do not know. At the same time, a corner of the roof struck Mr. Murry as he started from the elvtr. and broke his hip. Had it happened one minute later, no one would have been hurt.—Atlas Elvtr. Co., B. A. Rice, agt.

TENNESSEE

Nashville, Tenn.—A brick building has been completed for Cowser & Cowser, grain dealers.

Tullahoma, Tenn.—We have not completed our elvtrs. yet. Expect to soon. Middle Tennessee Mfg. Co.

Nashville, Tenn.—I have been identified with the grain trade here for some 12 years excepting the time that I was doing my stunt in France. I have been doing a track and shipping grain business until recently when I engaged in the grain and flour brokerage business. Our market has long been an established grain and flour distributing point for the Southeast and owing to a favorable adjustment of transit rates, we expect to have quite a boom in the business during the coming season.—W. P. Bell.

TEXAS

Floydada, Tex.—The Farmers Exchange Elvtr. is a new plant.

Tulia, Tex.—The W. C. Cowan Grain Co. has a new 15,000-bu. elvtr.

Sherman, Tex.—The headquarters of the Kimball Mfg. Co. are located at this place, having been moved from Whiteright.

Ft. Worth, Tex.—The Simons Hay & Grain Co., the Chadwick Grain Co. and T. F. West & Co., have discontinued business.

Amarillo, Tex.—Arthur E. Stinson of the Amarillo Grain Exchange disappeared a short time ago. His automobile, with a blood stained seat, was found abandoned, which has added to the mystery.

UTAH

Ogden, Utah—The city of Ogden is planning to have an annual grain show at which prizes will be awarded.

WASHINGTON

Seattle, Wash.—The firm Stephens-Smith Grain Co. has been dissolved.

Odessa, Wash.—The Seattle Grain Co. has closed its warehouse at this point.

Seattle, Wash.—James McFarlane & Co. has been incorporated for \$100,000 by James McFarlane, M. Thompson and M. J. Droppelman.

Seattle, Wash.—The Seattle Port Commission elvtr., which is operated by the Gray-Rosenbaum Grain Co., is being improved and repaired.

Seattle, Wash.—George L. Neil, formerly with the Seattle offices of the Pacific Grain Co., and the Neil Bros. Grain Co., has become associated with W. J. Lake & Co., corn and food products brokers.

La Crosse, Wash.—M. E. Stansell, formerly with the Pacific Coast Elvtr. Co., has been appointed local representative of the Gray-Rosenbaum Grain Co. at this point. Mr. Stansell is also mgr. of the La Crosse Elvtr. & Produce Co.

WISCONSIN

Grafton, Wis.—The plant of Kroehne Bros. was slightly damaged by fire last month.

Bancroft, Wis.—Fire starting from a locomotive engine spark did slight damage Sept. 22 to the elvtr. of the A. M. Penny Co.

Park Falls, Wis.—The mid-winter grain show of Northern Wisconsin will be held at this place Jan. 3, 4, and 5. George Briggs is sec'y.

Wisconsin Rapids, Wis.—A. C. Madsen, formerly with the Grand Rapids Mfg. Co., is now connected with the Jackson Mfg. Co. as sales mgr.

Watertown, Wis.—The Globe Mfg. Co. has amended its articles of incorporation and now has \$100,000 common stock and \$20,000 preferred.

Oconto Falls, Wis.—Please send my Journal to me here as I have accepted a position as branch mgr. for the Northern Mfg. Co. of Wausau.—C. R. Waterman.—Mr. Waterman was formerly in charge of the Chili Co-op. Produce Co. of Chili.

Byron, Wis.—Charles Schmeling of Brownsville has been elected to succeed Oscar Blohm, deceased, as mgr. of the plant of the South Byron Shipping and Trading Co. The company recently purchased the elvtr. of the Manitowoc Malting Co. in which an electric feed grinder will be installed.

Casco, Wis.—Nine creditors of the Casco Mfg. & Elvtr. Co. have formed the Casco Mfg. Co. and have purchased the property of the elvtr. company and will operate on the same site. A. J. Villers will be in charge of the plant and F. J. Fameree will act as pres., J. J. Defnet, sec'y-treas. and M. W. Burke, vice-pres. The company is incorporated for \$40,000 and the plant, which has been closed, is now operating.

MILWAUKEE LETTER.

Herman Jahns, Jr., has again become connected with Runkel & Dadmun.

The finance com'te of the Chamber of Commerce has determined the October rate of interest to be 6%.

Jackson Bros. of Chicago have opened a branch office at this place which will be under the management of William Young, Jr.

Feedstuffs

DEL RIO, TEX.—C. D. Brown opened a new feed store here.

DREXEL, MO.—Robert Steele has entered the feed business here.

ASHLAND, KY.—R. D. Taylor has purchased the feed store of D. L. Bush.

LIBERTY, MO.—The Cowherd Feed & Fuel Co. opened for business Oct. 1.

MARION, ILL.—A new feed mill is being erected here by A. J. Kuykendall.

ERWIN, TENN.—The Acme Feed & Milling Co. is now located in its new building.

KITTITAS, WASH.—The Kittitas Feed Mills, Inc., have filed a petition for dissolution.

ELKHORN, WIS.—The Elkhorn Lumber Co. has installed a new plant for grinding and mixing feeds.

INDIANAPOLIS, IND.—John G. Brown and F. W. Boyle have purchased the feed business of the A. M. Nowlin Feed Co.

SMITHVILLE, ARK.—The plant of J. A. Reed, feed miller here, burned recently. A stock of corn and meal was also consumed.

ATLANTA, GA.—The Sensation Flour & Feed Co. of Memphis, Tenn., has opened a branch office here with J. K. McDowell in charge.

MOUND CITY, KANS.—J. A. and J. H. Wilkinson have opened a feed store here and will handle all kinds of feed, and will also buy hay and grain in carlots.

EDGEWATER, N. J.—The Corn Products Refining Co. has authorized the expenditure of \$3,000,000 for improvements on its plants at Argo and Pekin, Ill., and here.

MADERA, CAL.—The Valley Feed & Fuel Co. incorporated for \$50,000 to succeed the firm of the same name. Incorporators are G. M. Shedd, B. A. Harrington and H. M. Davis.

MINNEAPOLIS, MINN.—The Reliance Feed Co. has been incorporated here for \$10,000, to deal in flour, feed, etc. Incorporators are L. C. Newsome, J. R. Smart, A. R. Taylor.

CHICAGO, ILL.—The American Hominy Co. of Indianapolis, Ind., and the Purity Oats Co. of Keokuk and Davenport, Ia., have established their headquarters here.

SCOTDALE, PA.—J. E. Pritts will rebuild his flour and feed mill which burned recently. The new building will be two and one-half times larger than the old one, with a capacity of 75 cars.

ASHLAND, KY.—The buildings and stocks of the Vanhooose-Adams Co., feed dealers, burned here Sept. 27, together with buildings of a wholesale grocery company. The combined loss is estimated at \$500,000.

MADERA, PA.—W. F. Rector has purchased the flour and feed mill of the Liberty Trading Co. and will operate it as W. F. Rector Co. He was formerly connected with H. S. Allen & Co. flour and feed merchants of Pittsburgh.

PORTLAND, ORE.—The East Side Feed Co. warehouse burned Sept. 21, here, with a loss of \$10,000. The three story wooden structure was entirely gutted and a stock of grain and hay lost. Johnson Bros. managed the company.

FARIBAULT, MINN.—We are building an addition to our elevator, 30x32 feet, which will be used for a feed mill. We are installing a 24 inch, 2-20 h. p. motor direct driven Unique Attrition mill, Triumph Corn Sheller and a Bowsher Cob Crusher. The mill will be modern in every respect and will handle 100 sacks of ground feed per hour.—E. B. Murphy, mgr., Farmers Co-op. Elevator Co.

GRANITE CITY, ILL.—W. S. Thomas has been chosen as the head of the Best-Clymer Co., which has resulted in the reorganization of the bankrupt Best-Clymer Mfg. Co., a subsidiary of the Temtor Corn & Fruit Products Co.

Feed Distributors' Annual Meeting.

About 35 feed dealers gathered at the Hotel Sherman, Chicago, Sept. 27, to attend the two-day meeting of the U. S. Feed Distributors Ass'n.

Due to the illness of his wife, E. C. Dreyer of St. Louis, president of the ass'n, was unable to attend so the meeting was held in charge of W. O. Fehling of Philadelphia.

One of the first steps taken was to ask the co-operation of all grain exchanges in helping to obtain lower reconsignment charges on carloads of feed. The charges have been from \$7 to \$10 per car, with some talk of lowering the charges to from \$3 to \$5.

It was decided to insert in all telegraphic code books pertaining to the feed trade, a word, which when inserted into a telegram, would mean that the sender was buying or selling according to the National Feed Trade Rules.

The remainder of the day was spent in open discussion regarding changes which should be made in the National Feed Trade Rules which were adopted at the convention of the G. D. N. A. last year. A com'te was then appointed to draw up whatever changes they saw fit and submit them to the meetings here and at New Orleans, Oct. 2, 3 and 4 for approval and adoption. Those appointed on the com'te were W. O. Fehling, Philadelphia; C. S. Wehmann, Minneapolis; L. W. Ervine, Chicago; C. W. Wagar, Philadelphia; E. W. Elmore, Oneonta, N. Y., and J. Canfield, Minneapolis.

The session opened Sept. 28, about 10:30 a. m., and as the com'te had not as yet gotten the changes in the rules into form, it was decided that such changes would be left to the discretion of the com'te and they could draft them later and present them to the feed dealers for adoption at New Orleans in connection with the G. D. N. A. meeting.

Election of officers was then started, and altho Sec'y Beatty told the members in attendance that Pres. Dreyer emphatically stated he would not serve again, he was re-elected as pres. and it was voted that he should have more assistance in carrying out his duties than heretofore.

J. W. Juneau of Milwaukee and E. W. Elmore of Oneonta, N. Y., were elected as vice-presidents.

Five new directors were elected to replace those who had retired or become inactive. They are J. Canfield, Minneapolis; Max Cohn, Buffalo; M. G. Rankin, Milwaukee; L. W. Ervine, Chicago, and Monroe A. Smith of Philadelphia.

By unanimous vote C. S. Wehmann, of Minneapolis, became sec'y of the organization in addition to being treasurer, for the ensuing year. He succeeds R. T. Beatty, who has served the ass'n well in the past.

Just before the adjournment Louis J. Weitzman of the Weitzman Milling Co., Chicago, tendered his application for membership and became a member.

Feed Movement in September.

Receipts and shipments of feedstuffs at the various markets during September, compared with September, 1921, were as follows:

| | Receipts | | Shipments | |
|----------------------------|------------|-------------|------------|---------|
| | 1922. | 1921. | 1922. | 1921. |
| Baltimore, tons | 1,820 | 1,840 | ..... | ..... |
| Cincinnati, tons | 1,890 | 1,050 | ..... | ..... |
| Chicago, lbs., 35,285,000 | 28,982,000 | 107,055,000 | 72,491,000 | ..... |
| Kansas City, tons, bran.. | 5,900 | 5,440 | 18,240 | 16,720 |
| Mil'kee, tons. | 9,902 | 16,420 | 17,999 | 22,991 |
| New York, tons bran.. | 143 | ..... | 80 | ..... |
| Peoria, tons.. | 19,660 | 4,620 | 19,615 | 16,020 |
| San Francisco, tons bran.. | 264 | 275 | ..... | ..... |
| St. Louis, sacks, bran | 95,660 | 154,740 | 115,600 | 164,435 |

Grain Carriers

THE B. & O. is now free from all embargoes and is prepared to handle all freight.

THE C. R. I. & P. Ry. has placed orders for \$5,000,000 worth of new equipment. Five hundred box cars are among the 2,500 cars ordered.

THE STRIKE of lake vessel men will not be a cause for worry for grain shippers, states a vessel man, as most of the ships are not unionized.

THE LOUISVILLE Board of Trade has been permitted to intervene in I. C. C. No. 14020, J. D. Hollingshead Co. v. Aberdeen & Rockfish, et al.

WESTERN railroads have tightened up in regard to allowing their cars to leave their own lines. The move is intended to relieve the car situation.

THE EQUITY Co-op. Ass'n of Hilger, Mont., has been permitted to intervene in I. C. C. No. 13910, Denton Independent Elevator Co. v. C. M. & St. P. Ry.

TO REPRESENT the grain trade in the St. Lawrence deep sea waterway project, the Chicago Board of Trade, thru its president, has appointed a committee.

THE INTERSTATE Commerce Commission suspended until Jan. 23, 1923, certain schedules which proposed to increase domestic rates on grain and grain products from various points in southwestern Indiana and Kentucky to eastern points.

SUSPENSION of proportional rates on grain and grain products from Sioux City, Ia., to Arkansas and Missouri has been ordered by the Interstate Commerce Commission upon a petition by the Illinois Central and St. Louis Southwestern railroads.

THE FLANLEY GRAIN Co., Sioux City, Ia. v. Director-General, in I. C. C. No. 14176 claims unlawful reconignment charges on grain from points in Iowa, South Dakota and Minnesota to Sioux City and Council Bluffs, Ia. Complainant asks reparation.

The general managers of Kansas railroads held a meeting recently to determine what could be done regarding the car situation. The Santa Fe reports that it can fill but 10 per cent of its car orders for grain, and other roads are in the same predicament.

AT A MEETING held recently in Kansas City, 30 delegates from 13 states upheld the Kansas Utilities Commission in its fight to obtain reduced freight rates. The commission recently filed a petition with the Interstate Commerce Commission to obtain a 10% reduction.

THE PENNSYLVANIA put into effect from Oct. 2 to 11, limited freight embargoes to facilitate the movement of grain and coal. Since Sept. 1 the Pennsylvania has handled an average of 173,000 cars per week, compared with 134,000 the corresponding weeks in 1921.

CARRIERS, by order of the I. C. C., have re-adjusted interstate rates on grain, grain products, hay and straw from points in the Western Illinois group, advises J. S. Brown of the Chicago Board of Trade transportation department. The readjustment becomes effective Oct. 16. Under the original decision the carriers made reductions on rates of 14½ per cent on wheat and 21½ per cent on corn, oats, rye and barley in the Western Illinois group. A recently issued supplementary decision states that there will be substituted a 10 per cent reduction on the interstate rates as given in the first decision. This will again give uniformity in the state and interstate rates from most points in Illinois and there will also be restored the former relationship in the rates to Chicago, Milwaukee, St. Louis and the Ohio river.

THE KANSAS Utilities Commission issued an order Sept. 23 which cut the car supply to mills to one-half of the capacity of the various plants. This move was made to enable wheat shippers in Kansas to receive more cars. The order will be in effect about seven days.

THE STRIKE of shopmen on the railroads is practically ended, states the Ass'n of Railway Executives. At the close of business, Sept. 26, the larger railroads of the country had 363,417 employees, which is about 90 per cent of normal. On July 10 there were but 155,685 men employed.

DONALD B. CONN, formerly head of the transportation division of the joint congressional commission of agricultural inquiry, has been made manager of the public relations section of the American Railway Ass'n. He will make a special study of the movements of grain and coal.

CONGESTION on railroads in the East is preventing a free movement of Canadian and United States wheat to the seaboard on a lake and rail basis. This tends to make exporters obtain wheat from the West on an all rail basis for both Gulf and Atlantic seaboard shipments.

EASTERN railroads have been ordered to release grain cars belonging to western roads, whether they are sent back loaded or empty, states a report from the American Railway Ass'n. This is to facilitate the movement of wheat tied up in country elevators thruout the western states.

THE PENNSYLVANIA Ry. has placed an embargo on wheat originating in Pennsylvania, New Jersey, Maryland and Delaware when destined to the Girard Point Elevator at Philadelphia. The embargo also covers wheat from the Keystone Elevator at Philadelphia to the Girard Point Elevator.

ATTACKS by Boston and New England interests on the freight rate differentials in favor of Baltimore opened at a hearing before the Interstate Commerce Commission examiner recently. The Baltimore interests were represented by former Director of Railroads, W. D. Hines. Additional hearings will be heard.

THE MINNESOTA Railroad and Warehouse Commission has decided to issue an order giving potato shipments preference over grain in Minnesota and other states are expected to follow suit. The action is being taken so that the crop may be moved before freezing weather sets in.

A UNIQUE situation has arisen at Fort William, Ont., due to the blockade of the lower lake ports. For the first time for this season of the year, grain is being shipped to Montreal via the all-rail route, due to export demand. The Canadian National Ry. is loading 200,000 bus. of wheat for Montreal via the all-rail route.

THE ALBERS BROS. Mfg. Co. in case No. 14158, v. Oregon-Washington Ry. & Nav. Co. before the Interstate Commerce Commission claims excessive and unjust rates on grain, especially wheat, from points in Idaho and eastern Washington to Portland. Complainant asks cease and desist order and establishment of reasonable rates.

THE TURNER GRAIN Co., Viborg, S. D. v. Director-General, as agent, Great Northern, in I. C. C. No. 14175, claims unjust and unreasonable rates on grain from Viborg and Irene, S. D., to Willmar, Minn., for inspection and then reconignment, because of \$5 reconignment fee without tariff authority. Complainant asks reparation.

HECLA, S. D.—The Hecla Co-op. Elevator Co. has installed a radio receiving outfit.

CLARKSON, NEB.—The Clarkson Milling & Grain Co. has installed a radio receiving set.

WINTERSET, IA.—M. Young & Son have installed a radio receiving outfit in their elevator.

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× ×

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A. B. C. Improved 5th Edition, contains a complete set of five letter code words for every expression in the former edition. Any two of these words may be combined and sent as one word, reducing telegraph tolls 50 per cent. Price in English, \$18.00.

Miller's Code (1917), for milling and flour trades, 3¼x6 inches, 77 pages, \$2.00.

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Grain Dealers Journal

309 S. La Salle Street,

Chicago, Ill.

Supreme Court Decisions

Damages on Market Price on Day of Final Failure to Deliver.—In estimating damages for a seller's failure to deliver, the market price on the day of the final failure to deliver is controlling, and the buyer cannot select a remote date during a rapidly rising market as a standard of measurement.—*National Wholesale Grocery Co. v. Simon Rice Milling Co.* Supreme Court of Louisiana. 92 South. 713.

Privilege of Canceling Contract.—An agreement to do something reserving the right to cancel at will, is no contract. Altho buyer retains privilege of canceling order before shipment, a right in seller to ship any time within three months, existing for however short a time after acceptance, carries at least the right, which buyer cannot nullify, to ship at time of acceptance, and this constitutes sufficient consideration to establish a valid contract.—*Gurfein v. Werbelovsky.* Supreme Court of Errors of Connecticut. 118 Atl. 32.

Measure of Damages for Breach of Warranty.—That the cotton seed meal was sold to plaintiffs as merchants for resale and that a portion thereof was resold without loss, and that for the balance unsold was substituted a like quantity of the quality warranted in the sale, is no defense to an action for breach of warranty as to quality. The measure of damages for breach of warranty of quality is the difference between the actual value of the goods at the time of delivery to buyer or to carrier for his account and the value if the goods had corresponded to the warranty; the price received by the buyer on resale being immaterial.—*Attalla Oil & Fertilizer Co. vs. Goddard.* Supreme Court of Alabama. 92 South. 794.

Bank's Lien on Shipment for which it Holds B/L.—Where the drawer of a draft indorses it to a bank and transfers to the bank an accompanying B/L and receives credit on his account with the bank for the amount of the draft, the bank has a right as a matter of law to charge back to the account of the drawer the amount of the draft if not paid, and if it was the intention of the parties at the time the draft was deposited and the B/L assigned that the bank was acting only as the collecting agent of the drawer of the draft, such bank, by reason of the assignment of the B/L, would have a lien on the property described in the bill, and such property would not be subject to attachment until the bank had received the money advanced.—*First and Old Detroit National Bank v. Holloman.* Supreme Court of Oklahoma. 208 Pac. 791.

Embargo Does Not Excuse Failure to Deliver.—A shortage of railroad freight cars did not justify or excuse the seller's failure to make delivery of rice under a contract of sale f. o. b. cars at point of shipment, it not constituting inevitable accident or irresistible force within Civ. Code, art. 2120, and not justifying the application of the doctrine of commercial frustration. Where a contract of sale of rice f. o. b. cars at point of shipment required the buyer to pay the freight, and contained no stipulation for shipment by ocean steamer, it was the duty of the seller to ship by the most available route, regardless of slight differences in freight rates, and an embargo on transportation by water was not a legal excuse for his failure to deliver.—*C. F. Bonsor & Co. Simon Rice Milling Co.* Supreme Court of Louisiana. 92 Southern 711.

A sale of wheat already shocked in the field is a transaction concerning a sale of property such as is not usually accompanied by actual delivery of possession, and with respect to which Rev. St. 1919 § 2282, allows a reasonable time for delivery, and only after the lapse of a reasonable time for delivery is the sale void as to vendor's execution creditors. The seller of wheat in the shock was a lessee, and under the lease had to put one-third of the threshed wheat in the landlord's granary as rent, so that, when the buyer purchased, he could not remove the wheat without the landlord's consent. An execution was levied on the wheat 14 days after the purchase, while the buyer was waiting for threshers. Held, that whether a reasonable time for delivery had elapsed after the sale was for the jury.—*Edwards v. Wilkins.* St. Louis Court of Appeals, Missouri. 242 S. W. 995.

Passage of Title with B/L.—One who purchases certain cars of wheat on the railroad track, procures shippers' orders Bs/L to himself as consignor and consignee, and draws drafts on certain grain companies for the price of the wheat, and indorses such Bs/L and attaches them to the drafts and deposits the same in the bank, and receives credit therefor subject to check, thereby passes the title of the wheat to the bank. The fact that such indorser of the Bs/L with the drafts attached was a regular customer of such bank and had been frequently overdrawn for some time, and that the bank knew or had reason to believe that he was insolvent, would not make the bank guilty of bad faith in accepting such paper. In order to preclude a bank in the situation indicated by the foregoing paragraph from being a bona fide holder of such Bs/L and drafts attached, it must, in the transaction by which it received such paper, have acted in bad faith.—*Clark Burd v. V. C. Scott and National Bank of Commerce of Wichita.* Supreme Court of Kansas. 208 Pac. 668.

New Coal Commission Established.

A bill, No. 12377, to establish United States Coal Commission was enacted by Congress Sept. 22.

The commission will be established immediately and will consist of not more than seven members, to be appointed by the President and approved by the Senate. Each member will receive a salary of \$7,500 per year, the commission existing only one year.

The duties of the commission will be to investigate fully conditions, ascertain facts and study questions relating to the coal industry, for the purpose of aiding and assisting Congress in the enactment of legislation for the purpose of insuring a supply of coal for industries and the public generally. A separate investigation will be made of the anthracite industry, aside from the general investigation. This report will be made on or before July 1, 1923.

Recommendations will be required of the Commission on the standardizing of the mines; ascertainment of the cost of living for miners; standardizing of methods of computing overhead expenses of mines, and the advisability of legislation having to do with government of private ownership, regulation or control of the coal industry. The first report on the above matter will be made not later than Jan. 15, 1923.

The Commission is authorized to examine the records of the business of the industry. For willful failure or refusal to furnish information to the Commission by those concerned, the bill provides a penalty of \$5,000 fine or imprisonment for one year, or both.

An appropriation of \$200,000 has been made by Congress for the expenses of the Commission.

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References: Any bank, mercantile agency, commission firm or the editor of this publication.

The Railroad to Sue and the Railroad With Which to File Your Claim.

By OWEN L. COON.

Shippers should realize that delay and shortage claims can be presented in the first instance against a carrier other than the one that should be sued when litigation becomes necessary.

The carrier issuing the bill of lading at the point of origin is always the railroad that should be sued. The Cummins amendment to the Interstate Commerce Act made such initial carrier liable for "all loss, damage or injury" to the shipment, regardless of whether it occurred on its own line or that of some connecting carrier. At the trial of any such suit against an initial carrier, it is only necessary to show that the goods when delivered at destination were in a damaged condition, other than when delivered to the carrier at point of origin. It is *not* necessary to show which one of the carriers handling the shipment caused the damage.

If, however, you should sue some connecting carrier, or the delivering carrier, it is then necessary for the shipper to show that the particular loss and damage complained of occurred on the lines of the carrier that is sued. This is a much more difficult problem when the shipper does not accompany the car to destination and should be avoided wherever possible.

While it is always wise to sue the bill of lading carrier, it is better in many cases to file your claim in the beginning against the railroad delivering the shipment at destination. And for this reason. Policies of freight claim departments vary exactly as do opinions of individuals. One freight claim department will consider and settle a claim which the freight claim officials of a different railroad would promptly decline. Under bill of lading provisions, claims may be filed against either the originating or delivering carrier.

Under American Railway Ass'n rules, Section 7, the delivering carrier may adjust many claims and charge out against the other lines involved their due proportion without even consulting them. If the originating line does not give you fair consideration on your claims, try the delivering carrier. You may have better success. If a certain delivering line does not give you a fair deal, route your traffic so that it is delivered by some railroad that will give you reasonable consideration. Railroads must follow your directions as to the routing of shipments, and you can select any delivering carrier that is available.

Some freight claim officers are square enough to give claimants a fair deal; some of them are not. The ones that are against giving you fair consideration on your claims are the ones whose railroads you want to short haul every time you ship. This policy consistently followed will sooner or later bring the proper consideration from the railroad that is so penalized.

If the claim is filed against the delivering railroad, and suit does become necessary, it can be instituted against the initial carrier which will call for the claim file for the other road and handle to a conclusion.

THE WAVE length of the Omaha radio sending station has been increased from 360 to 385 meters. The hours at which the market reports are sent are 9:35, 10:35 and 11:35 a. m. and 12:35, 1:20 and 8:00 p. m.

GRANITE CITY, ILL.—The Temtor Corn & Fruit Products Co. is defendant in a suit filed by the Irving National Bank of New York on a charge of conspiracy to defraud. The plaintiff claims the stocks of the company were manipulated on the St. Louis Stock Exchange so as to create false values, also that members of the company misrepresented facts of financial conditions when borrowing money. The bank seeks to recover \$175,000 of the \$300,000 it loaned to the company.

Changes in Rates

As shown by tariffs recently filed with the Interstate Commerce Com'n the carriers have made the following changes in rates:

W. J. Kelley in 305-A, and 306-A, gives transit privileges on feterita, kafir, and milo at Milwaukee, effective Oct. 5.

E. B. Boyd, agent Western Trunk Lines, in Supplement 35 to Circular 1-P gives rules, regulations and exceptions to classifications, effective Nov. 1.

C. & A. in tariff No. 1661-F gives joint freight rates on grain from stations in Illinois and starch from Peoria, Ill., to Decatur, Ill., effective Nov. 1.

C. & A. in tariff No. 1653-E gives joint freight rates on grain from stations in Illinois, St. Louis and Louisiana, Mo., to Sheldon, Ill., effective Nov. 1.

E. B. Boyd, agent Western Trunk Lines, in Supplement 34 to Circular 1-P gives rules, regulations and exceptions to classifications, effective Oct. 15.

Monon in Supplement 1 to 505-C gives milling in transit rates on grain from C. I. & L. Ry. stations to be milled in transit at Monticello, Ind., effective Oct. 3.

C. M. & St. P. 504280, 500908, 61042, **C. B. & Q.** 108894, **M. & St. L.** 19310, **U. P.** 135362, **S. L.** 141012 were in a wreck on the C. M. & St. P. between Ferguson and Haverhill, Ia., Sept. 25. The only car showing any leak was C. M. & St. P. 504280.—**O. K. Morrison**, mgr., Farmers Elevator Co., Laurel, Ia.

A. T. & S. F. in Supplement 5 to 5588-M gives local, joint and proportional rates on grain and grain products from points in Kansas, Colorado, Missouri and Oklahoma, also Superior, Neb., and Joplin, Mo., to points in Illinois, Indiana, Iowa, Kansas, Michigan, Minnesota, Missouri, Nebraska, Oklahoma and Wisconsin, effective Oct. 26.

C. R. I. & P. in No. 31408-C gives, local, joint and proportional rates on grain, grain products, seeds and broom corn from stations in Colorado, Kansas, Missouri, Nebraska, New Mexico and Oklahoma, also Council Bluffs, Ia., and Memphis, Tenn., to Memphis, Tenn., New Orleans, La., and stations in Alabama, Arkansas, Louisiana, Mississippi and Oklahoma, effective Oct. 25.

C. & N. W. in Supplement 1 to 8300-E establishes increased rates of one-half to one and one-half cents over present rates on grain and grain products between Chicago, Ill., and Milwaukee, Wis., also between stations in northern Illinois, southern Wisconsin and Milwaukee. The new rates between Chicago and Milwaukee will be 5 cents per hundred pounds and on grain products 6½ cents, effective Oct. 16.

B. T. Jones in Supplement 9 to 104-J gives the proportional rates on grain, grain products etc., from Chicago to Ohio River Crossings, Evansville, Ind., Cincinnati, O., when destined to southeastern and Carolina territories. The rates will be increased approximately one cent per 100 pounds. On shipments originating in Trans-Mississippi territory, rate to Cincinnati, O., and New Albany, Ind., will be 11 cents, from Illinois and northern Iowa, 13 cents, from Northwestern territory, 10 and 13 cents and from all territories to Cairo, Ill., Evansville, Ind., etc., 10 cents, effective Oct. 16.

THE PROTEIN laboratory in connection with the Kansas State Grain Inspection Dept. at Hutchinson, Kan., started operation Sept. 27.

Books Received

THE BROAD-NOSED GRAIN WEEVIL is the subject treated by Richard T. Cotton in United States Dept. of Agriculture Bulletin No. 1085. The 10-page pamphlet gives the origin and economic history of the weevil; the present known distribution, food, life history, parasites, control measures, and technical descriptions of immature stages. Government Printing Office, Washington, D. C.

I. C. Ry. Makes Car Shortage Worse.

Complaint was filed with the Illinois Commerce Commission Sept. 28, by J. S. Brown of the Transportation Dept. of the Chicago Board of Trade, against the Illinois Central Ry. stating that the Illinois Central Ry. was bringing into Chicago empty box cars that had been shipped from Chicago to points in Illinois with merchandise. The cars should be used for grain that is now stored at country points awaiting cars to move it. To uphold its statement the complaint said that only 11 cars of grain were brot into Chicago on the I. C. Sept. 27 and 14 on Sept. 28, out of a total of 600 cars brot in by all railroads.

Upon being called before Chairman F. L. Smith of the Commission, officials of the road promised that the practice would be rectified and shippers of grain given more cars.

New Rule on Transferring Freight Bills.

In Supplement 1 to C. & N. W. 14,000-G, effective Oct. 2, the following rule has been established regarding transfer of freight bills covering grain and grain products at Milwaukee, Wis.:

"When whole grain is sold and transferred at a transit station, representative freight bills may be transferred, without additional charge, in accordance with the form of assignment shown below.

When commodities covered by this tariff other than whole grain are sold and transferred at the transit station, representative freight bills (Freight Bills covering the original commodity from which the products were manufactured will be considered as representative) may be transferred, at an additional charge of 1.5 cents per 100 pounds, in accordance with the form of assignment shown below.

The following form of assignment must be endorsed on each freight bill:

"The tonnage represented by this freight bill has been sold to, and the transit privileges thereon, if any, transferred to

(Signed)

Any additional switching performed at transit stations will be charged for in accordance with tariffs lawfully on file with the Interstate Commerce Commission."

Loadings of Cars.

Freight cars loaded with revenue freight on United States railroads during the week ended Sept. 23, according to the American Railway Ass'n was within 96 per cent of the greatest loading in history. The total for the week was 973,291 cars, or an increase of 27,372 over the previous week. This was the heaviest loading for any one week since Oct. 29, 1920.

Compared with the corresponding week last year, it was a gain of 99,650 cars, but a loss of 34,818 cars from the total loaded the corresponding week two years ago.

Grain and grain products totaled 52,379 cars, this being 289 above the previous week, a decrease of 527 under the total of the corresponding week of a year ago, and an increase of 7,610 cars over the total of the corresponding week two years ago.

Railroads in the Northwest district loaded for the week ended Sept. 16, 18,928 cars with grain. This was an increase of 4,186 over the corresponding week a year ago.

The number of cars awaiting repairs on Sept. 15 was 304,548, compared with 324,583 the corresponding date in 1921, showing that the shopmen's strike did not affect the rolling stock greatly.

ROSALIE, NEB.—We have put in a radio receiving outfit and are receiving the market in great shape each hour.—G. V. Patrick, mgr., Farmers Grain Co.

THE FORMAL opening of the new export and domestic tidewater terminal for flour at Philadelphia will be held Oct. 21. A special train will leave Chicago Oct. 20 after the meeting of Millers National Federation, containing directors and delegates of the Federation who will attend the opening.

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GRAIN DEALERS JOURNAL

309 So. La Salle St.

CHICAGO, ILL

Patents Granted

1,428,093. Grain Cleaning Machine. Francis William Humphreys and Lorne Edwin Mackenzie, Carman, Man., Can. The grain cleaning machine has in combination with an endless conveyor, a suction flue presenting an inlet disposed beneath the discharge end of the conveyor, and a scraper carried by the flue adjacent to the inlet and engaging the conveyor.

1,430,293. Grain and Seed Grader and Separator. Edgar D. Eddy, Ottawa, Ont., Can. The grader and separator comprises a casing, a rotor rotatably mounted in the casing, peripheral vanes on the rotor, means for delivering stock to be treated axially to the rotor, means for delivering liquid to the casing and curvilinear baffle walls eccentrically located between the vanes.

1,429,006. Dust Collector. Adolph J. Wilhelm, Ashland, Wis. The dust collector has a casing having an outlet and comprising oppositely disposed foraminous walls; means for discharging a blast of dust-laden air into the casing; a brush; means located between the walls for mounting the brush for movement in an orbit along the walls; and means for flexing one of the walls to co-operate with the brush.

1,428,926. Grain Separator. Patrick H. Vaughn, Morris, Minn. The grain separator has in combination with a feed hopper, a sieve equipped shoe, means for vibrating the shoe, a screening sieve mounted in the shoe between the upper sieve of said shoe and the hopper and means for vibrating the screening sieve from the shoe either in the same direction therewith or in an opposite direction in respect thereto.

1,429,258. Disk Mill. Stefan Steinmetz, Berlin, Germany. The disk mill is for grinding grain, meal, bran and feed and embodies throw wings on the outer circumference of one of the turning mill disks, the wings being tangentially arranged and extending across both disks, a spirally formed grinding chamber enclosing the wings and provided with a curved sieve so that the wings lift the ground material leaving the disks and throws it against the sieve.

1,428,878. Conveyor Belt. Edward J. Brennan, Chicago, Ill. The conveyor belt comprises a foundation of flexible sheet material and mutually connected wear plates upon the surface of the foundation, certain of the plates being resilient and having their intermediate portions outwardly bulged to provide transverse ribs capable of expansion and contraction thru the inherent resiliency of the foundation and to serve also as means for accumulating and retaining the material to be handled upon the belt.

1,427,915. Car Door. Axel Sellgren, Winnipeg, Man., Can. The car door has a plurality of leaves hingedly connected, channeled sectional metal strips cover the ends of the leaves and a portion of the sides. The strips are hingedly connected, certain leaves having notches in the opposite ends, the strips being adjacent to said notches and perforated, with rollers journaled in the side portions of the strips and disposed in the notches. Means are provided for lowering and raising the door on the rollers.

1,428,763. Grain Chute. Harry W. Davisson, Kansas City, Mo., assignor of one-half to James C. Patrick, Kansas City, Mo. The grain chute has a grain delivery belt, a casing into which the delivery of the belt discharges, delivery chutes in spaced relation in the bottom of the casing and a sliding hopper in the casing of greater length than the width of the belt and provided with a discharge opening adapted to register with either of the delivery chutes when the hopper slides in either of two directions.

1,428,710. Grain Crib. Geo. B. Read, Bloomington, Ill. The concrete stave crib has, in combination, a corn crib section formed by sections of super-posed stave members having ventilating openings, a grain bin section mounted thereabove and formed by super-posed sections of solid stave members a roof for the grain bin section, having a cupola provided with ventilating openings therein, transverse partition members between the corn and grain bin sections, and a vertically disposed stack open at its upper end, having in its lower portion within the corn crib section, ventilating openings.

1,429,732. Belt Conveyor Tripper. Oscar B. Hanson, Tiffin, O. The belt conveyor tripper has a carriage having side frames, shafts fastened in the side frames, pulleys mounted idle on the shafts and adapted to be driven in opposite directions, a friction clutch for each pulley also idling on the same shafts and adapted to be shifted to engage one or the other of the pulleys, wheels on which the carriage is mounted, a pinion connected with the clutch mechanism of each pulley, an intermediate gear wheel meshing with the pinions, and means to transmit motion from the gear wheel to the carriage wheels to cause the tripper to run back and forth as desired in accordance with which pulley is clutched for the time being.

1,429,985. Grain Door. Geo. M. Waddy, Buffalo, N. Y. The sliding door includes a plurality of horizontally disposed boards, vertical bars disposed adjacent to the side edges of the door for connecting the horizontally disposed boards, a central bar, bracing rods having their ends extended thru openings in the vertical bars and overlying the central bar, plates secured to the horizontally disposed boards at the side edges thereof, the door having openings formed adjacent to the side edges under the side plates, sliding keys positioned under the side plates and disposed within the openings, the keys having piercing ends adapted to embed themselves into the door frame, in which the door is positioned, the keys having right angled ends providing means to operate the keys.

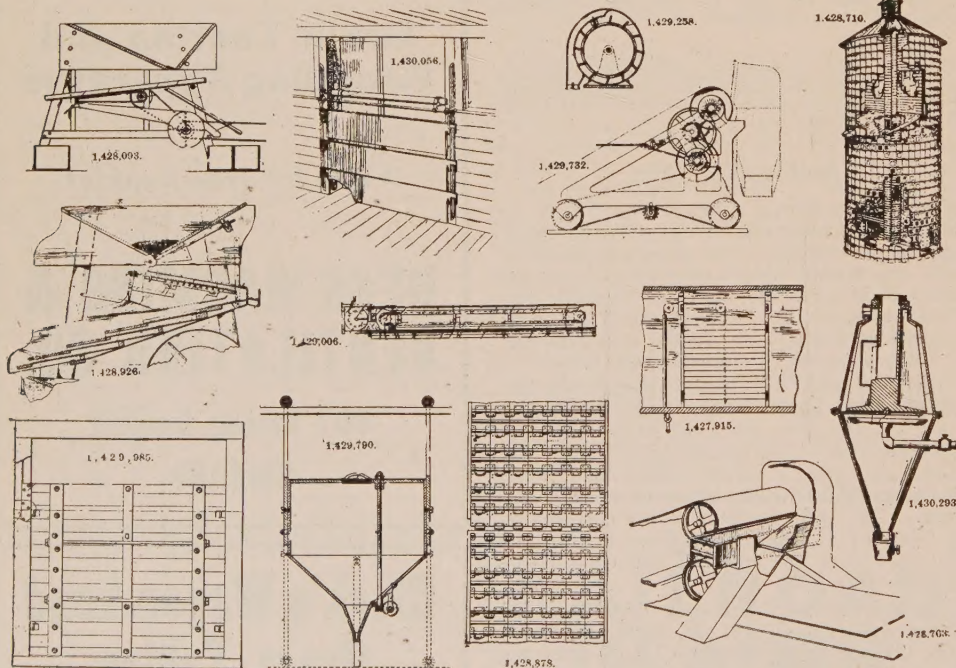
HAVANA, CUBA.—Jose Ortega, Peralta, former grain commission merchant, is reported as having failed in business and will hereafter be connected with E. M. Carason S. en C.

CORN in Guerrero and corn and beans in Chihuahua, Mexico, are suffering from lack of rain. In Jalisco the corn, wheat and sugar cane crops are greatly increased over last year.

RUSSIAN crops are ample to feed its own people, but there will be no grain for export, states Dr. L. Hutchinson of Berkeley, Calif., who has just come from Russia. While there may be a shortage in some sections, surplus from others will make it equal.

Insurance Notes.

DAVID J. PRICE, Engineer in Charge of Development Work in the Bureau of Chemistry, has been designated by Sec'y Wallace to represent the U. S. Dept. of Agriculture and co-operate with the Chamber of Commerce of the United States in the nation-wide fire-prevention movement now being undertaken by that organization. In this campaign considerable attention will be given to industrial dust explosions and fires, and Mr. Price will serve on the National Fire Waste Council.



1,430,056. Car Door. Ernst Christenson, Stockton, Kan., assignor of one-fourth to Wm. I. Smith, Stockton, Kan. The car door has a plurality of door hangers arranged in vertical rows, a plurality of door sections, means for supporting the door sections and means for bringing the door section supporting means in proximity to the door hangers whereby the door sections may be transferred from the door supporting means to the door hangers.

1,429,790. Feed Mixer. John G. Steel, Amarillo, Texas. The feed mixer includes a tank having an outlet, means for directing the contents of the tank thru the outlet, hangers adjustably angularly upon opposed portions of the tank, support engaging wheels connected to the hangers and means for securing the hangers in either upwardly or downwardly extending positions upon the tank for engagement with an overhead or an underneath support.

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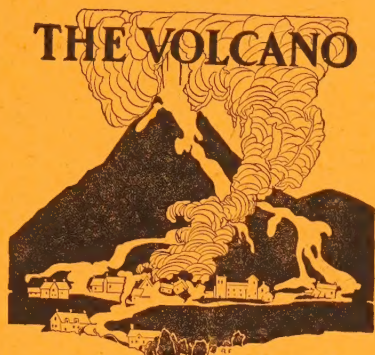


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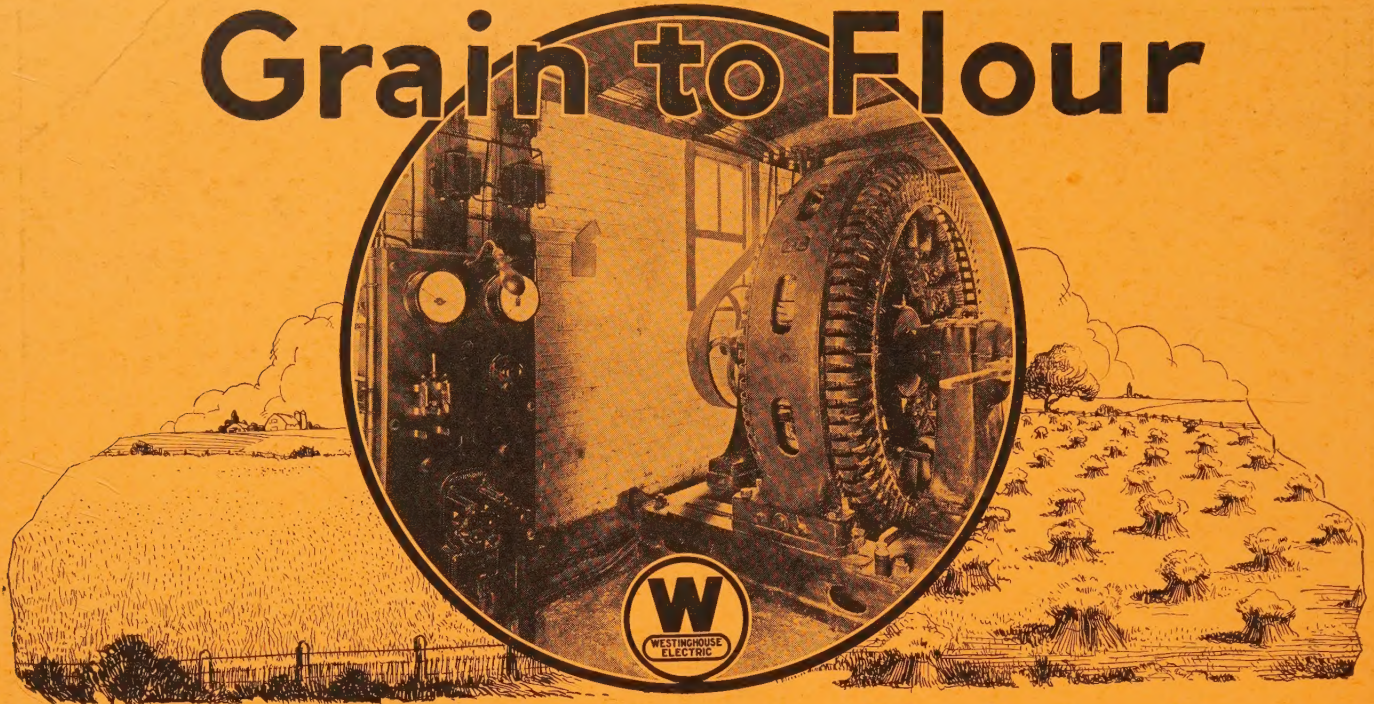
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The **GRAIN** DEALERS JOURNAL.

Grain to Flour



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